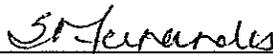


I, SYBIL FERNANDES, Deputy City Clerk of the Corporation of the City of Vaughan, in the Regional Municipality of York, do hereby certify that attached is a true copy of Amendment Number 597 to the Official Plan of the Vaughan Planning Area, which was approved by the Regional Municipality of York, without modification, on the 31st day of May, 2006. (Note: Section 2.7- Special Policy Areas of OPA 597, is deferred).



Sybil Fernandes
Deputy City Clerk
City of Vaughan

SYBIL FERNANDES
DEPUTY CITY CLERK
CITY OF VAUGHAN

DATED at the City of Vaughan
this 17th day of July, 2006.

Certificate of Approval

AMENDMENT No. 597

TO THE

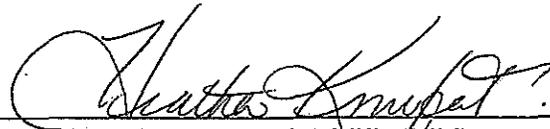
OFFICIAL PLAN FOR THE

CITY OF VAUGHAN PLANNING AREA

This official plan document which was adopted by the Council of the City of Vaughan is approved in part, pursuant to Sections 17 and 21 of the Planning Act and came into force on May 31, 2006. Section 2.7 of OPA 597, which deals with Special Policy Areas, is deferred.

Date:

June 8, 2006



Heather Konefat, M.C.I.P., R.P.P.
Director of Community Planning
The Regional Municipality of York

THE CITY OF VAUGHAN

BY-LAW

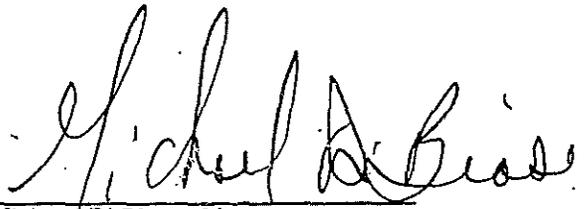
BY-LAW NUMBER 187-2003

A By-law to adopt Amendment Number 597 to the Official Plan of the Vaughan Planning Area.

NOW THEREFORE the Council of the Corporation of the City of Vaughan ENACTS AS FOLLOWS:

1. THAT the attached Amendment Number 597 to the Official Plan of the Vaughan Planning Area, consisting of the attached text and Schedule(s) "A" and "B" is hereby adopted.
2. AND THAT the City Clerk is hereby authorized and directed to make application to the Regional Municipality of York for approval of the aforementioned Amendment Number 597 to the Official Plan of the Vaughan Planning Area.
3. AND THAT this By-law shall come into force and take effect on the day of the final passing thereof.

READ a FIRST, SECOND and THIRD time and finally passed this 23rd day of June, 2003.



Michael Di Biase, Mayor



J. D. Leach, City Clerk

**AMENDMENT
NUMBER 597
TO THE OFFICIAL PLAN
OF THE VAUGHAN PLANNING AREA**

The part of this Amendment entitled "Part B- Islington Avenue Corridor Secondary Plan" and Schedule B-Special Policy Areas" constitute Amendment No. 597 to the Official Plan of the Vaughan Planning Area.

Also attached hereto but not constituting part of the Amendment are "Part A-The Preamble" and Appendix "A".

PART A – THE PREAMBLE

I TITLE

This is Amendment No. 597 to the Official Plan of the Vaughan Planning Area.

The part of this Amendment entitled “Part B – Islington Avenue Corridor Secondary Plan” and those schedules entitled “Schedule A - Land Use” and “Schedule B - Special Policy Areas” constitute Amendment No. 597 to the Official Plan of the Vaughan Planning Area.

II PURPOSE

The purpose of the Islington Avenue Corridor Secondary Plan is to provide an overall framework for the future orderly growth of the Islington Avenue Corridor in accordance with sound and reasonable land use planning policies and urban design principles. These policies should be responsive to the needs of the area residents while having regard for the provincial and regional policy objectives. The Secondary Plan should serve to maintain and strengthen the existing community structure through appropriate and contemporary planning policies and design guidelines.

The Islington Avenue Corridor Secondary Plan is intended to guide development within the Islington Avenue Corridor by establishing a framework for future land use planning decisions. The Plan is intended to reduce the uncertainty for both the public and private sectors with respect to future land use redevelopment alternatives. To this end, the land use planning principles and policies enunciated in this Plan are designed to achieve compatibility between land uses and their associated activities. The intent of the policies is to minimize potential land use conflicts and reinforce and enhance the existing community structure.

III PLANNING AREA

The City of Vaughan is located immediately north of the City of Toronto, within the Regional Municipality of York. The City is a mosaic of historic communities that have evolved into a comprehensive urban community. The Woodbridge Community dates back to the early 1800's and was officially incorporated in 1882 and later amalgamated with the Township of Vaughan in 1971. Small settlement areas within Woodbridge including Pine Grove, provide the historic link for this community. As the City of Vaughan grew, a number of comprehensive Community Plans were approved to guide development and future growth. Most recently, in 1987, the Ontario Municipal Board approved Official Plan Amendment No. 240, the Woodbridge Community Plan.

The Secondary Plan Area is centred on the historic settlement of Pine Grove and comprises approximately 38 hectares of land fronting onto Islington Avenue as well as some properties that have access onto Islington Avenue. The Secondary Plan Area is consistent with the Study Area contained within the Islington Avenue Land Use Study, dated September 2002. Generally, the Secondary Plan Area consists of the lands along the Islington Avenue corridor between Langstaff Road and Woodbridge Avenue. Schedule A to this Amendment depicts the Islington Avenue Corridor Secondary Plan Area.

IV BASIS

In December 2001, Council for the City of Vaughan, released the Terms of Reference for the conduct of the Islington Avenue Land Use Study. The Terms of Reference stated the study was "to examine existing and appropriate future land uses for lands with frontage on the west and east sides of Islington, to the branch of the Humber River and Open Space corridor to the east, located between Woodbridge Avenue on the south and Langstaff on the north". The Study commenced in February 2002 and concluded in September 2002 with the "Islington Avenue Land Use Study – Final Report". City of Vaughan Council received and endorsed the Final Report at a Council Meeting on October 21, 2002.

It is the opinion of Council that, based on the findings of the Islington Avenue Land Use Study, dated September 2002, that limited residential intensification within the Secondary Planning Area is appropriate. Following an extensive Public Consultation process, as part of the Land Use Study, the Statutory Public Meeting, held on June 2, 2003, and input from Municipal Staff and various commenting agencies, the Municipality has determined that the Amendment is appropriate for a variety of reasons, including the following:

- 1) The Lands are located within the Woodbridge Community Plan Area (OPA 240). It is a historic corridor that both radiates out of the Woodbridge core and links the mature areas of Woodbridge to the newer development areas to the north.
- 2) Moderate levels of residential intensification would be appropriate and consistent with the Provincial Policy Statement, relevant Provincial Guidelines as well as the Region of York Official Plan.
- 3) There is an adequate level of social service infrastructure in the local area to accommodate a moderate level of residential intensification.

- 4) The Land Use Plan respects the existing scale and residential character of the Islington Avenue corridor by maintaining most existing land uses and identifying “pockets” for intensification. These pockets are sensitive to and compatible with both the natural and built environment.
- 5) The Land Use Plan, reinforced by Urban Design policies, Urban Design Guidelines, and Streetscape Plans will respect the existing built form, landscape and pedestrian character of the area.
- 6) The existing and future road network can accommodate a moderate level of intensification. All major residential intensification must assess the on-going changes to this transportation system to ensure a satisfactory level of traffic service prior to being approved for development.
- 7) The Land Use Plan was thoroughly reviewed and commented on by the Public through a comprehensive public consultation process and Stakeholder Meetings.
- 8) The findings of the Islington Land Use Study support this amendment.

Islington Land Use Study

The primary focus of the Islington Avenue Land Use Study was “to comprehensively define appropriate land uses within the Study Area” and consider the appropriateness of future development within the Secondary Plan Area. The Study addressed the following key planning components:

- Land use compatibility;
- Transportation impacts;
- Environmental impacts;
- Community impacts; and
- Urban design principles.

As part of the Study, four capacity options were developed covering a range of intensification possibilities. They ranged from a lowest level of intensification (severing and subdividing single-family lots to build additional single-family houses) to a highest level involving redevelopment in a variety of locations at the scale of apartment form intensification that was proposed by the three development applications. Each capacity option was reviewed against a set of factors that incorporated the above-mentioned key planning components. Following the review, and with

stakeholder input, a Preferred Land Use Plan was developed, together with a set of recommendations to guide the future of the Islington Avenue Study Area.

The Preferred Land Use Plan provided direction for the entire Study Area, regardless of individual land ownership. The impacts on the larger community were considered for this assignment, but the Preferred Land Use Plan and recommendations relate specifically to the lands within the Study Area boundaries, as determined by the City of Vaughan. The focus and scope of the Study was not to evaluate any specific development applications beyond the level of any of the other lands included in the scope of the Study.

In conjunction with the Preferred Land Use Plan, the Islington Avenue Land Use Study recommended the following key implementation measures:

1. That the Preferred Land Use Plan, based on the policy recommendations contained within the Study, be implemented by an amendment to the Woodbridge Community Plan (OPA 240).
2. The Official Plan should include urban design policies that provide for preparation and implementation of Urban Design Guidelines.
3. Maintain existing zoning in the Study Area. Any new development or redevelopment identified for intensification should be done by way of site specific zoning amendments.
4. In the event that industrial operations cease at the Hayhoe Mill site, redevelopment should occur by way of a Secondary Plan. At the time of the secondary plan consideration shall be made for public access via recreational trails abutting the west side of the East Humber River.

Ontario Municipal Board

During the Islington Avenue Land Use Study, there were three applications for development in the Study Area. All three applications were for high-density condominium projects, ranging in height from 3.5 to 5 storeys, each requiring an Official Plan and Zoning By-law Amendments. Following the completion of the Land Use Study, but prior to the approval of this Amendment, these applications were considered by the Ontario Municipal Board. Accordingly, the respective Ontario Municipal Board Decisions govern the three parcels of land and implementing planning documents. Each parcel has been identified on

Schedule A - Land Use to this Amendment including the Official Plan Amendment governing each site.

- Lanada Investments Limited, Decision Date: December 17, 2002, Decision Order #1714, Official Plan Amendment No. 586.
- Pinegrove on the Humber Inc, Decision Date: December 17, 2002, Decision Order #1715, Official Plan Amendment No. 587.
- Robert Fredrick Good et al, Decision Date: May 8, 2003, Decision Order #0594, Official Plan Amendment No. 596

PART B – ISLINGTON AVENUE CORRIDOR SECONDARY PLAN

I TITLE

All of this part of the Amendment entitled “Part B - Islington Avenue Corridor Secondary Plan” consisting of the following text and the attached Schedules A and B, constitute Amendment No. 597 to the Official Plan of the Vaughan Planning Area.

II OVERVIEW

As is apparent from Part A of this document, there is a need to provide detailed planning policies to ensure the future orderly redevelopment of the Islington Avenue Corridor. Schedule A to the approved Official Plan Amendment No. 240 for the City of Vaughan must be amended insofar as it is necessary to delete all designations applicable to the Islington Avenue Corridor Secondary Plan area, and, to clearly identify thereon the boundary of the lands affected by this Secondary Plan.

III DETAILS OF THE AMENDMENT AND POLICIES RELATIVE THERETO

1. THAT Schedule A, the Land Use Plan, forming part of the approved Official Plan Amendment No. 240 for the City of Vaughan is hereby amended by deleting all land use designations as generally apply to the Islington Avenue Corridor Secondary Plan Area and by delineating the boundaries of the lands affected by the Islington Avenue Corridor Secondary Plan in accordance with the Land Use Plan for the City of Vaughan, attached hereto as Schedule A and by this reference forming part of this Amendment to the City of Vaughan Official Plan.
2. THAT the policies and designations contained in the Islington Avenue Corridor Secondary Plan, as set out below, shall hereinafter apply to the future development and use of lands within the area affected by the Islington Avenue Corridor Secondary Plan as delineated on Schedule A, the Land Use Plan, forming part of the Official Plan for the City of Vaughan, attached hereto and by this reference forming part of this Amendment. Those lands identified on Schedule A, as subject to site specific Official Plan Amendments #586, #587 and #596, shall be subject to the provisions of the respective site specific OPA.

ISLINGTON AVENUE CORRIDOR SECONDARY PLAN

1.0 GOALS

1.1 GENERAL

- a) To establish a planning area boundary which provides the most appropriate interface with the surrounding community.
- b) To augment and complement the existing residential community within, and surrounding, the Secondary Plan area in a manner which respects the existing scale and residential character of the area.
- c) To respond to the development pressures by identifying suitable opportunities for redevelopment, taking into consideration the environmental, traffic, urban design, parks and planning environment.
- d) To provide an opportunity for phasing of new development through the monitoring of the surrounding road system to ensure the system can accommodate the level of redevelopment.

1.2 RESIDENTIAL

- a) To implement the intent of the Provincial Policy Statement and Regional Official Plan, as related to residential intensification.
- b) To identify suitable locations within the Secondary Plan Area to accommodate residential intensification.
- c) To identify a range of suitable residential densities and built forms that would provide a compatible integration to the existing community and respect the area's existing scale and residential character.

1.3 URBAN DESIGN

- a) To respect and protect the existing scale and character of the Secondary Plan Area.
- b) To recognize and protect the existing landscape character of the Secondary Plan Area, including vegetation.
- c) To provide a comprehensive Streetscape and Linear Park System.

- d) To identify specific locations within the Secondary Plan Area for community enhancement.
- e) To establish general urban design guidelines to address the form and massing of any new development and the organization and relationship of new development to public and private open spaces.

1.4 INDUSTRIAL

- a) To recognize the long-standing industrial operation of Hayhoe Mills and its continued importance and influence in the area.

1.5 ENVIRONMENTAL

- a) To protect, enhance, conserve and recognize the physical resources, such as environmentally sensitive areas, hazard lands and woodlots.
- b) To encourage the use of energy conservation techniques in matters such as site plan design, building orientation and building design in new development.
- c) To protect and manage the features of the East Humber River, and minimize the risk to life and property through implementation of applicable TRCA policy and Ontario Regulation 158.
- d) To recognize the East Humber River as a community amenity and to create opportunities within new development for views and vistas of this feature, and public access.

1.6 OPEN SPACE

- a) To develop an integrated and, where appropriate, continuous system of open space comprising of community parkland, pedestrian and bicycle paths, hiking trails, flood and valley lands and major wooded areas to serve the active and passive recreational needs of the Community.
- b) To provide an off-street pedestrian and bicycle path system linking the Secondary Plan area to the surrounding community, including the Woodbridge Core.
- c) To enhance the open space resources in the Secondary Plan Area and integrate them into the larger, citywide trail system.

1.7 TRANSPORTATION AND SERVICING

- a) To integrate the transportation and transit system with the City and Region wide network in a manner conducive to the efficient and convenient routing of traffic and transit.
- b) To encourage development that supports increased transit opportunities along the Islington Corridor.
- c) To ensure that all new development occurs on the basis of full services including sanitary sewers, storm sewers and piped water supply.
- d) To ensure that municipal services improvements, including transportation improvements required to accommodate new development, are in place or will be in place, prior to development proceeding.

2.0 LAND USE POLICIES

2.1 GENERAL POLICIES

- a) The intent of this Plan is to provide for the future orderly development and growth of the Islington Avenue Corridor in keeping with the established character of the area. Lands shall be developed in accordance with the land use pattern delineated on Schedule "A" and the policies contained in the text of this Plan.
- b) The Secondary Plan Area is intended to accommodate limited residential intensification in keeping with the general overall low-density character of the area.
- c) All new development may be permitted in accordance with the policies of the Plan only when the following are, or will be made available to the satisfaction of the Council
 - a) Full municipal servicing;
 - b) Adequate school facilities;
 - c) Adequate road facilities.

The holding provision outlined in Section 6.2 may be utilized until Council is satisfied that the above services are available.

- d) In areas where slopes exceed 10%, and are determined not to form part of the East Humber River Valley Wall, development will be permitted only if the erosion and siltation control measures are satisfactory to the City of Vaughan and the TRCA.

2.2 RESIDENTIAL POLICIES

- a) Residential development close to the Islington Avenue Corridor shall be permitted only if appropriate noise abatement measures are provided. A noise report describing noise abatement measures and buffering/screening shall be submitted to the satisfaction of the City, Region of York and the Ministry of the Environment.
- b) All new residential development within the potential influence area, as defined by the Ministry of Environment, of the Hayhoe Mills industrial operation shall be required to submit all necessary studies to the satisfaction of the City, the Ministry of Environment and any other agency having jurisdiction detailing recommendations with respect to suitability and/or appropriate abatement or control measures, buffers or mitigation measures. All study recommendations shall be implemented to the satisfaction of Council through the appropriate development approval process.
- c) All new residential development within the potential influence area, as defined by Ministry of Environment, of the Hayhoe Mill industrial operation, shall register on title an acknowledgement of the long-standing industrial operation and that its generation of off-site noise and air quality emissions will continue.
- d) All new residential development within the potential influence area, as defined by Ministry of Environment, of the Hayhoe Mills industrial operation shall be required to submit environmental assessment reports in accordance with Provincial Guidelines. Where a site is determined to be contaminated, development will not be permitted until the site has been cleaned-up in accordance with Provincial criteria and City specifications and policies.
- e) All new residential within the potential influence area, as defined by the Ministry of Environment, of an existing or former service station shall be required to submit environmental assessment reports in accordance with Provincial Guidelines. Where a site is determined to be contaminated development will not be permitted

until the site has been cleaned-up in accordance with Provincial criteria and City specifications and policies.

- e) In Low Density Residential areas, the permitted uses shall be single detached and semi-detached dwelling units.
- f) Any development, redevelopment or intensification within the low-density designation shall be considered within the context of the surrounding lot frontages, natural and built environment, traffic and compatibility with the surrounding land uses. The new development must comply with the urban design policies contained in Section 3.0 and be consistent with the Urban Design Guidelines, attached as Appendix A.
- g) In Medium Density Residential areas, the permitted uses shall be en-bloc, stacked or street townhouses, garden-court or low-rise apartments. Any new residential development within the Medium Density Residential designation, as depicted on Schedule A, shall not exceed 3.5 storeys.
- h) In High Density Residential areas, the permitted uses shall be stacked townhouses or apartment units. Any new residential development within the High Density Residential designation, as depicted on Schedule "A", shall not exceed 5 storeys.
- i) In all residential areas, institutional and public open space uses (other than those prohibited in Section 2.7e) shall be permitted in accordance with the policies of this Plan, and provided these are suitably designed and landscaped in a manner consistent with that of the surrounding neighbourhood and the Urban Design Guidelines attached as Appendix "A".
- j) For the purposes of calculating density in the low density designation, a gross hectare shall include the net residential land area plus traversing streets, alleys and drives, and one-half of boundary streets and one-quarter of boundary street intersections.
- k) For the purposes of calculating density in the medium and high-density designations, the net area includes the residential land area, plus one-half of any local or collector roads.

- l) Floor Space Index (FSI) means the ratio of gross floor area to the net area. The gross floor area means the total of all floors in a building above or below grade, measured from the outside of the exterior walls but excluding vehicular parking areas within the building.
- m) The maximum densities permitted for development in each designation shall be as follows:
- i) Low Density: 8.6 units per gross hectare
 - ii) Medium Density: 0.5 FSI
 - iii) High Density: 1.0 FSI
- n) Notwithstanding the provisions of paragraph 2.2(m) above, the maximum permitted density for the Low Density Residential area identified as Area 1, on Schedule A shall be 17 units per gross hectare.
- o) Notwithstanding the provisions of paragraph 2.2(m) above, the maximum permitted density for new street townhouse and block townhouse development within the Medium Density Residential areas shall be calculated on a 35 units per net hectare basis.
- p) New Residential development within the Medium Density Residential areas may contain a mixture of street/block townhouses and other permitted housing forms. The density should be based upon the proportionate amount of land area for each housing group.
- q) Notwithstanding the maximum densities permitted on lands designated High or Medium Residential Density on Schedule "A", additional bonus density of up to 10% of the maximum may be permitted. Council shall determine the level, appropriateness and type of density bonus subject to the provision of one or more of the following criteria identified below. The community benefit that is received by the City in order to grant this bonus shall be used within the Secondary Plan Area.

i) Landscaping

- 1) Provision of linkages from public streets to open space and parkland areas by way of interconnecting walkways and trails.
 - 2) Landscape areas in excess of the normal minimum required and landscape material in excess of the City's standard requirements.
 - 3) Provision of Public Facilities.
 - 4) Provision of additional Parkland or cash-in-lieu of parkland in excess of the required parkland dedication.
 - 5) Provision of Senior Citizen Housing, where the development includes a central dining facility, with limited en-suite kitchen facilities, and unit size is appropriate for seniors oriented housing.
- r) All new development and redevelopment (within low, medium and high density designations) shall be subject to site plan control which will ensure, among other things, compliance with the urban design guidelines, attached as Appendix "A".
- s) Modified lot configurations, lot dimensions and setbacks, landscaping and other accepted measures of energy conservation will be considered when evaluating site specific applications for energy conservation.
- t) Prior to the development of any part of a medium or high-density residential block, a site specific Zoning By-law Amendment must be approved pursuant to the Planning Act.
- u) Development of any medium or high-density residential block will be by Draft Plan of Subdivision/Condominium and/or Site Plan Approval pursuant to the Planning Act.
- v) Prior to Site Plan approval or any other development approval, the City and TRCA shall have approved a stormwater management plan for the development and a sediment and erosion plan.
- w) Prior to Site Plan approval or any other development approval within or adjacent to the Regional Greenland System or Environmentally Sensitive area, an Environmental Impact Statement shall be completed to the satisfaction of the Region, City and TRCA.

- x) Prior to Site Plan approval or any other development approval, the City shall have approved a tree preservation plan for the site.

2.3 COMMERCIAL POLICIES

2.3.1 General Provisions

- a) For the purpose of this Secondary Plan, the Commercial Areas are categorized into 3 types:
 - a. General Commercial
 - b. Local Convenience Commercial
 - c. Service Station
- b) The development, redevelopment or maintenance of all Commercial Areas shall be subject to the following:
 - a. Open Storage shall not be permitted.
 - b. Building Setbacks, compatible lighting, visual screening, planting and/or fencing shall be provided between commercial uses and any adjacent residential uses.
 - c. Development Plans shall be considered within the context of the Urban Design Guidelines, attached as Appendix "A".
- c) Any new commercial development shall be by amendment to this Plan. A Planning justification report, which, shall address such matters as the commercial need, traffic impact and land use compatibility, shall support any such amendment.

2.3.2 General Commercial

- a) Permitted Uses within "General Commercial" designation are existing commercial uses, retail stores for the buying, leasing, and exchanging of goods and services, restaurants, banks and business and professional offices.
- b) Development of a General Commercial area shall be in accordance with the Zoning By-law and comprehensive Site Plan approved by the City of Vaughan. The Site Plan shall provide for the co-ordination of the development or

redevelopment of the area and shall lay out the buildings and structures, parking areas, access points, traffic circulation patterns, pedestrian circulation patterns and overall landscaping and buffering.

- c) Uses that require outdoor storage and display of merchandise shall not be permitted.

2.3.3 Local Convenience Commercial

- a) Permitted uses within the "Local Convenience Commercial" designation shall be one or more retail stores offering convenience goods and personal services for the residents of the immediate area.
- b) Such site shall be planned and developed as a single unit and shall be a maximum of 0.5 ha (1.2 acres). However, existing sites of greater size shall be deemed to conform.
- c) The maximum permissible gross floor area of each local convenience centre shall be 1000 sq. m. (10,700 sq ft), with the exception of any existing local convenience centres.
- d) Any additional Local Convenience Commercial centres in the Secondary Plan areas may be permitted only by amendment to this plan. Such centres shall be located at intersections and shall be in harmony with the character of the surrounding area.

2.3.4 Service Station

- a) Within Service Station areas, individual sites shall not exceed 0.4 ha (1 acre) and the permitted uses shall be the sale of petrol, oil and lubricant and other related products and may include repair and maintenance of vehicles. A Service Station may include an accessory use of a local convenience commercial store to a maximum permissible gross floor area of 1000 sq. m. (10,700 sq ft).
- b) Existing service stations are recognized in this Plan. New service stations may be provided only by amendment to this Plan.

2.4 INDUSTRIAL POLICIES

- a) The lands designated "Industrial" recognize the long standing Hayhoe Mill industrial operation.

- b) Permitted uses shall be manufacturing, processing and packaging operations, repairing, construction, warehousing and wholesaling, data processing, and storage operations, parks and open space, and offices associated with an industrial use.
- c) Adequate off-street parking and loading areas shall be available on industrial sites, or adjacent to the industrial operation in accordance with the zoning by-law. Such areas shall be screened to minimize adverse visual effects.
- d) Notwithstanding section 3.4(c) above, this Plan recognizes the unique truck parking and waiting conditions associated with the existing Hayhoe Mills industrial operation. Council shall encourage this operation to provide adequate parking and truck waiting areas on or adjacent to its existing facilities.

2.5 OPEN SPACE POLICIES

- a) The Open Space designation permits Neighbourhood Parks, Community Parks, Linear Parks and bicycle/bicycle pathways.
- b) The Inter-Regional Trail is conceptually shown on the Land Use Plan, Schedule "A". The final location, size and facilities etc. will be determined through consultation with the Region of York and Toronto Region Conservation Authority.
- c) Public recreational buildings may be developed within the active parkland system at the discretion of the City of Vaughan, subject to TRCA approval as appropriate.
- d) Land for park purposes shall be provided at a ratio of 5% of the gross land area or 1 hectare for each 300 units, or a combination of the 5% and 1:300, whichever yields the greater provided for in Section 42(1) of the Planning Act. Commercial development shall require conveyance of 2% of the land for park purposes as provided for in Section 42 (1) of the Planning Act.
- e) The City shall require dedication of parkland rather than cash-in-lieu unless it can conclusively be demonstrated that no significant requirement for such parkland will be presented for the foreseeable future.
- f) Linear Parks shall be incorporated as an acceptable parkland form. The following are policies related to this particular parkland category:

- i. Providing long linear public spaces which act as linking corridors between parks, open spaces, environmental features and other significant landforms;
- ii. Ideally should be a minimum of 25m wide;
- iii. Shall utilize existing significant tree hedgerows, natural features and land form character;
- iv. Incorporate ecosystem based corridor linkages between neighbourhood and community parks, woodlots and other environmentally sensitive areas;
- v. Include public trails/bicycle paths, seating areas, signage and lighting; and,
- vi. Should be integrated with public greenway systems to form expansive community open space links.

2.6 ENVIRONMENTAL PROTECTION AREA POLICIES

- a) The Environmental Protection Area designation permits only passive use parks, valleylands, woodlots and other environmentally protected features.
- b) Where lands designated as Environmental Protection Area are under private ownership, this Plan does not indicate that these lands will necessarily remain so designated as such indefinitely, nor shall it be construed as implying that such areas are free and open to the general public, nor that such lands will be purchased by the municipality or by any other public agency.
- c) No building shall be constructed on lands designated Environmental Protection Area except where such building is intended for flood and erosion control and is approved by the TRCA.
- d) Any development or redevelopment adjacent to the Environmental Protection Area may require an Environmental Impact Statement to the satisfaction of the City and TRCA. Further, any development or redevelopment adjacent to the EPA shall ensure that there is an adequate vegetative buffer strip along the area, to be maintained in a naturally vegetated, undisturbed state, to the satisfaction of the City and TRCA.

- e) The limits of development for any development or redevelopment adjacent to the Environmental Protection Area shall be staked to the satisfaction of the City and the TRCA. The valleyland portion shall include the slopes/stable slope limit, valley and floodplain lands and lands below the top-of-bank of the Humber River and its tributaries as well as any additional lands as deemed appropriate in Section 3.6(d) as determined by the City and TRCA.
- f) There will be no development or placing of fill or alterations to any watercourse in areas regulated by the TRCA without permission of the TRCA.

2.7 SPECIAL POLICY AREA

- a) The Provincial Flood Plain Planning Policy generally prohibits development or redevelopment within the Regulatory Storm Floodplain as determined by the TRCA.
- b) However, the Provincial Flood Plain Planning Policy also recognizes that parts of certain urban areas have historically developed within floodplains. In accordance with the Special Policy Area provisions of the Provincial Flood Plain Planning Policy, certain lands within the Regulatory Floodplain of the Humber River in the Woodbridge Community have been identified as "Special Policy Area" on Schedule B. The continued viability of these areas depends on a reasoned application of the Provincial standards for flood plain management.
- c) The Provincial Flood Plain Planning Policy recognizes the concept of Special Policy Area status as a possible option for flood prone communities or portions thereof where the Province, the TRCA and the City agree to accept a higher level of risk to floodplain management. Prior to development, technical information to the satisfaction of the TRCA may be required to demonstrate the feasibility of the site for the proposed use. Further, the implementation of flood proofing measures will be a condition of development approval by the City, subject to TRCA approval.
 - i. The "Special Policy Area" includes all lands designated SPA on Schedule B. The policies for land use designations as shown on Schedule A of Official Plan Amendment 597 located within the "Special Policy Area" and related policies continue to apply.

- ii. Within the limits of the "Special Policy Area", in order to reduce the risk to life and property, new development or redevelopment may require design considerations that may be inconsistent with those outlined in Appendix A, Urban Design Guidelines. Should an inconsistency occur, the design consideration outlined by the TRCA shall take precedence.

- iii. Within the limits of the "Special Policy Area", new development, redevelopment, rehabilitation of and additions to existing buildings and structures, shall only be permitted subject to the following policies:
 - 1) The proposed development is flood protected to the Regulatory Flood, as defined by regulations made under Section 28 of the Conservation Authorities Act, and to the satisfaction of the TRCA.
 - 2) No buildings or structures other than for conservation or flood control projects will be permitted within the "floodway" of the Humber River as defined by the TRCA.
 - 3) Notwithstanding the provision of 2.7 c)(i) above, where it is technically impractical to flood-protect a building or structure, or an addition thereto, to the level of the Regulatory Flood, the TRCA may permit a level of flood protection not lower than the 1:350 year flood elevation.
 - 4) The specific level of flood protection to be imposed, and any flood protection measures to be implemented relative to individual development applications, shall be determined by the TRCA in consultation with the City. The level of protection to be required shall be the highest level determined to be technically feasible or practical.
 - 5) All applications for development approval on lands designated Special Policy Area shall be accompanied by engineering studies, prepared by a qualified professional, detailing such matters as flood frequency, the velocity and depth of storm flows, proposed flood damage reduction details, storm water management techniques and other information and studies as may be required by the TRCA and the City.

- 6) Prior to the development proceeding, the TRCA and the City shall approve any proposed flood damage reduction measures including such matters as setbacks from the floodway, columns or design modifications to elevate openings in buildings and structures above the regulatory flood level, the use of water tight doors, waterproof seals at structural joints, strengthened foundation walls, the installation of backwater valves and sump pumps, etc.
- 7) For all types of development, dry, passive floodproofing measures shall be implemented to the extent technically and/or practically feasible. Where dry passive floodproofing may not be achieved or practical, the TRCA and the City may consider wet floodproofing and/or dry, active floodproofing measures. The placement of fill as a method of flood damage reduction shall generally not be permitted.
- 8) Upon completion of the foundation of any building or structure, the TRCA and the City may require a letter from an OLS or Professional Engineer verifying the required floodproofing levels.
- 9) Ingress and egress for all new development should be "safe", pursuant to the Provincial floodproofing standards.

d) The following uses shall be prohibited on lands designated Special Policy Area:

- i. public or private elementary school, day care centre, hospital, nursing home, senior citizens housing, a home for the physically or mentally handicapped; and
- ii. an automobile service station or any development which includes the storage, handling, production, disposal or use of a chemical, flammable, explosive, toxic, corrosive, or other dangerous material which would pose an unacceptable threat to public safety if they were to escape their normal containment and the treatment, collection or disposal of sewage; and
- iii. a building or structure directly related to the distribution and delivery of an essential or emergency public service including police, fire, ambulance and electric power transmission.

- e) Minor adjustments to the boundaries of the Special Policy Area boundary, as approved by the TRCA, as shown on Schedule "B" may be considered without an amendment to the Official Plan.
- f) The Zoning By-law may be amended to show lands designated Special Policy Area on Schedule "B" and to include the necessary provisions to implement the policies of Section 2.7 (c) of the Official Plan.
- g) Any new residential restricted area zoning by-laws within the Special Policy Area shall contain provisions, where appropriate, relating to minimum building or structure setbacks, maximum lot coverage, minimum height of any building or structure opening, and other such matters as determined by the T.R.C.A and the City.

2.8 INSTITUTIONAL POLICIES

- a) Institutional uses shall include public and separate elementary and secondary schools, private schools, religious institutions, day-care centres, community centres, service clubs, branch colleges, libraries, police stations, nursing homes and fire halls.
- b) New institutional uses shall be compatible with existing adjacent uses and shall front on Islington Avenue.
- c) All new institutional development shall be considered within the context of the Urban Design Guidelines, attached as Appendix A.
- d) All new institutional uses shall be addressed by an amendment to this Plan.

3.0 URBAN DESIGN POLICIES

- a) The Urban Design policies contained within this section provide design direction for any new development. Urban Design Guidelines have been created as Appendix A in order to provide further detail to guide new development. All new development shall be reviewed and considered within the context of these policies and the Urban Design Guidelines, attached as Appendix A.
- b) Any new development is to be designed in a manner that is complementary to the overall character of the area.

c) Any new residential development shall have regard to the following design criteria:

1. Buildings should be sited to promote positive building-to-street relationships;
2. Reverse lot development should be discouraged along Islington Avenue;
3. In areas where "high-density" or "medium-density" is introduced adjacent to single-family housing, there must be an appropriate transition in scale and building form;
4. Buildings should generally be oriented east-west to avoid creation of "building walls" parallel to the Humber River Valley or Islington Avenue. Where lot sizes do not allow this, length of buildings should be limited with visual breaks in between;
5. Entrances should be located along Islington Avenue or close to the Islington Avenue frontage to maintain "eyes on the street";
6. Building massing and volumes should respect the existing single-family character predominant in the Secondary Plan Area;
7. Buildings should be arranged to create pedestrian comfort in courtyards and private/shared open spaces;
8. Common areas such as courtyards or gazebos visible from Islington Avenue should be encouraged;
9. Roofing types and pitches should be consistent with those existing in the Secondary Plan Area;
10. Above-ground parking should not be visible from the street or be located within the front yard setbacks;
11. Under-ground parking shall be encouraged for the following housing types: en-bloc, stacked or street townhouses, garden-court or apartments. Entrances to underground garages should be located away from the street;
12. Landscaping for above-ground parking areas should screen parking from Islington Avenue;

13. A maximum driveway width for low, medium and high-density developments should be established and controlled at the time of Site Plan application;
14. Service areas should not be located along Islington Avenue or be visible from the street;
15. Service areas should be integrated into the building or be screened with fencing and landscaping;
16. Preservation of existing trees should be enforced through By-law and/or Site Plan agreements;
17. Private amenity space or publicly accessible amenity space along Islington Avenue should be encouraged; and
18. Coordinated improvements to the streetscape should be promoted with:
 - Coordinated street tree planting with native species;
 - Coordinated paving patterns/detail strips that are permeable and allow for groundwater infiltration;
 - Continuous tree canopy where possible to visually narrow the width of the corridor;
 - Lighting; and
 - Street furniture.

4.0 SERVICING AND TRANSPORTATION

4.1 SERVICING

- a) All new development shall be serviced by sanitary and storm sewers, piped municipal water, hydro and other utilities. The design of these services shall comply with the current standards of the City, to the satisfaction of the City Engineer and to the standards of the various public and private utilities concerned.
- b) Prior to the City of Vaughan approving any development application, a stormwater drainage plan for the drainage areas in which the proposed development is located shall be prepared to the satisfaction of the City of Vaughan and the TRCA. Such stormwater management plan shall be required to meet current criteria of the City and TRCA.

- c) Where significant development is proposed within the same catchment area, the proponent(s) may be required to undertake a more comprehensive servicing study for the broader area, as determined by the City and TRCA.
- d) New development will be dependent upon the availability of sufficient water and sanitary supply. The City and/or Region of York must confirm capacity and allocation prior to the approval of any new development.

4.2 TRANSPORTATION

- a) Road Classifications are shown in Official Plan Amendment 240. Road design shall be in accordance with current City standards.
- b) Prior to Site Plan approval or any other development approval, a site-specific traffic impact study is required to be submitted to the satisfaction of the City of Vaughan and Region of York. The City of Vaughan shall monitor the overall traffic capacity of the surrounding road network to ensure sufficient capacity exists prior to approving any development.
- c) Parking and sidewalks on all streets shall be controlled and regulated in accordance with the design classification.
- e) Signalization, channelization and illumination will be required at any new intersections along Islington Avenue at no cost to the Region.
- f) Individual driveways serving individual residences should, generally, be discouraged onto Islington Avenue.
- g) A system of pedestrian walkways/bicycle paths linking the park system and other public uses and providing a connection from the Secondary Plan Area to the Woodbridge Core shall be developed.
- h) Integration of the Secondary Plan Area into the City and Region-wide transit system shall be encouraged.

5.0 PHASING

- a) Prior to the approval of any Zoning By-law Amendment for the lands designated High-Density, the City shall be satisfied through a needs assessment study that the full residential capacity of the Woodbridge Core has been reached.

6.0 IMPLEMENTATION

6.1 EXISTING NON-CONFORMING USES

- a) Notwithstanding any other provision of this Plan to the contrary, this Plan is not intended to necessarily prevent the continuation, expansion or enlargement of existing uses, which do not conform, to the designations or provisions of this Plan. It shall be the policy of this Plan that where an existing land use is not designated on Schedule A to this Plan, Council may recognize the use of such land, building or structure for the purpose for which it was legally used at the date of adoption of this Plan.

6.2 HOLDING PROVISIONS

- a) Council may, in a By-law passed pursuant to the provisions of the Planning Act, zone lands for their intended purpose and further impose a holding provision by use of the "H" symbol, in accordance with the following provisions, namely:
 - i. Where the lands are designated Medium or High Density Residential and a plan of condominium has received draft approval or where a proposal has been received which conforms to the intent of this Plan and development is to proceed by means of a Site Plan Agreement.
- b) No development of those lands affected by a holding provision shall be permitted and Council shall not remove the Holding Symbol until such time as the proposal has been approved by all relevant Municipal, Regional and Provincial agencies and bodies and the necessary agreements have been registered on title to ensure that all the provisions of this Plan, and, as may be applicable, the conditions of draft approval, have been complied with.

6.3 TEMPORARY USE BYLAW

- a) Council may, in a By-law pursuant to the provisions of the Planning Act, pass by-laws to permit the temporary use of lands, buildings or structures, provided that the general use conforms with the intent of this Plan, the use is compatible with surrounding land uses, and that such temporary use does not result in a hazardous condition for traffic.

6.4 SITE PLAN CONTROL

- a) It is the policy of this Plan that all the lands within the Secondary Plan Area shall be designated as an area of proposed Site Plan control in accordance with the provisions of The Planning Act, R.S.O. 1990.
- b) It is further the policy of this Plan that single detached dwelling units on lots exceeding 12m in frontage may be exempt from Site Plan Control.

6.5 PLANS OF SUBDIVISION AND CONDOMINIUM

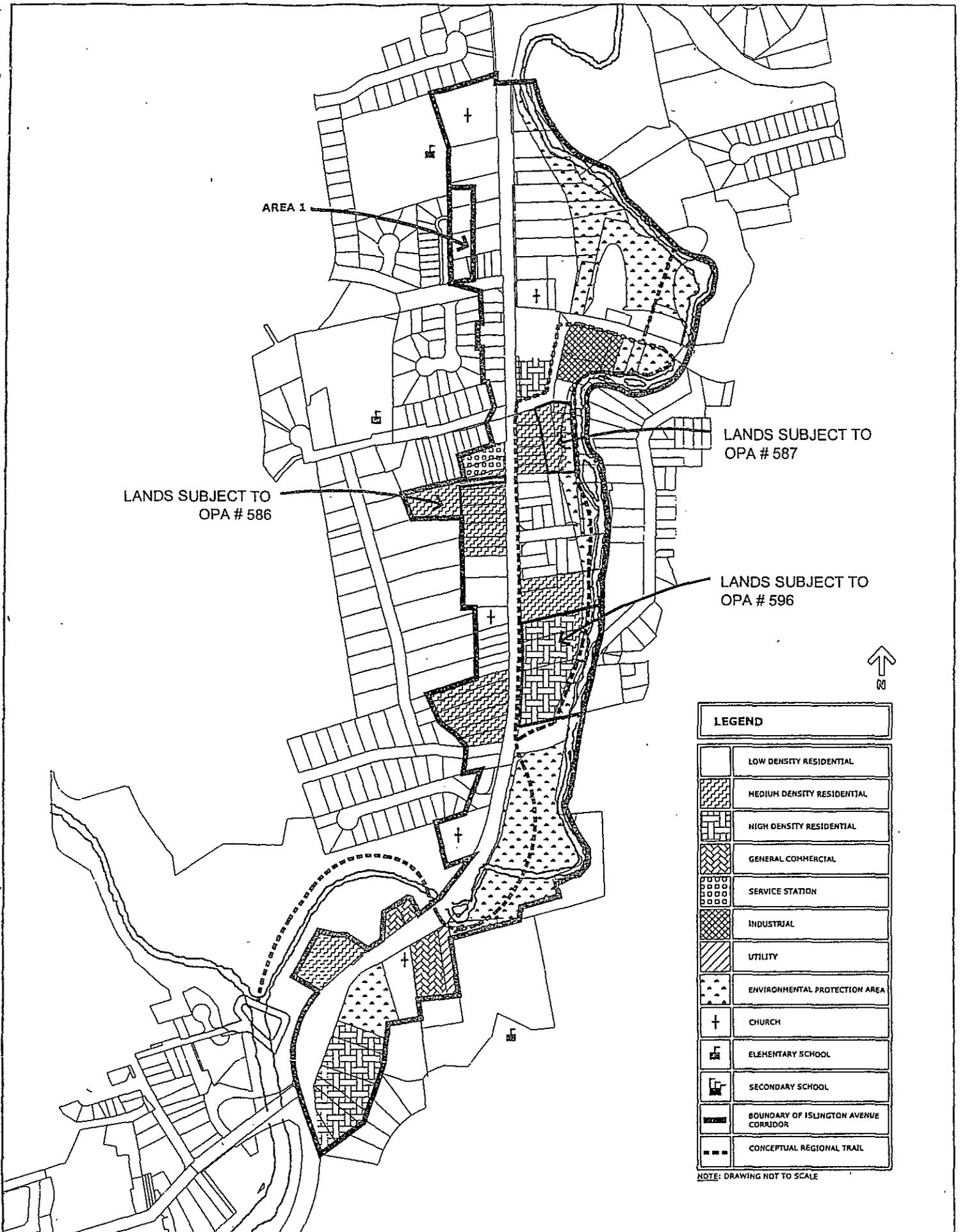
- a) It shall be the policy of this Plan to approve only those plans of subdivision and condominium which comply with the designations and policies of this Plan, and which, to the satisfaction of Council, can be supplied with adequate municipal servicing and community facilities such as schools, fire protection and roads.

IV IMPLEMENTATION

The Amendment to the Official Plan of the City of Vaughan shall be implemented in accordance with the provisions of the Islington Avenue Corridor Secondary Plan; the Implementation policies of the approved Official Plan of the City of Vaughan and the provisions of the Planning Act, R.S.O., 1990, as amended.

V INTERPRETATION

1. The boundaries between the land use designations shown on Schedules "A" and "B", are approximate, except where they coincide with roads and valleylands or other clearly defined physical features. Minor adjustments will not require an amendment to this Plan as long as the intent of its policies is maintained.
2. The provisions of the Official Plan of the Vaughan Planning Area, as amended from time to time regarding the interpretation of that Plan shall apply with respect to this Amendment.



L A N D U S E S C H E D U L E

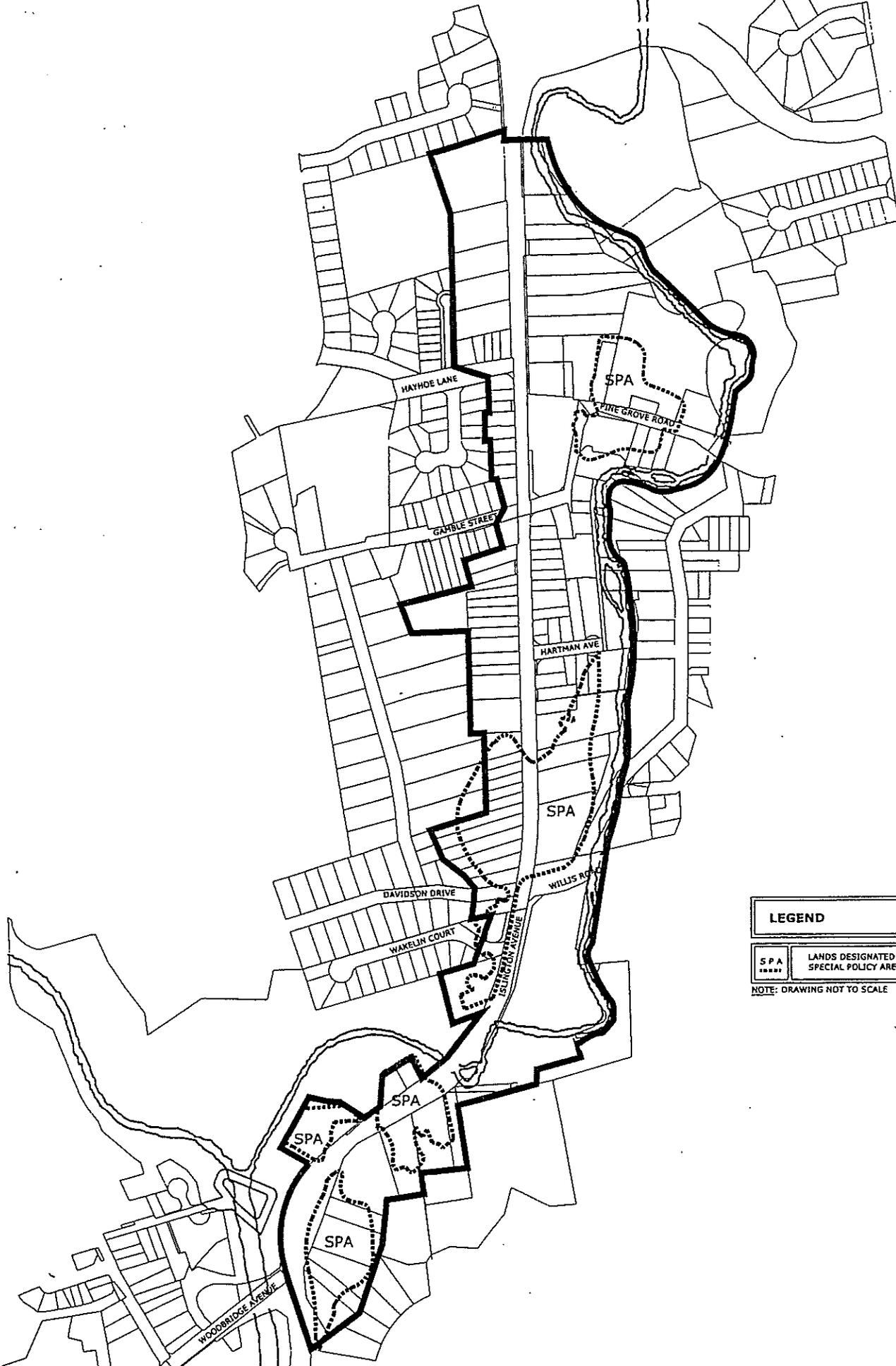
THIS IS SCHEDULE "A"
 TO AMENDMENT No. 597
 ADOPTED THE 23RD DAY OF June, 2003

ISLINGTON AVENUE CORRIDOR SECONDARY PLAN
 CITY OF VAUGHAN

SIGNING OFFICERS

 MAYOR

 CLERK



SPECIAL POLICY AREAS

THIS IS SCHEDULE "B"
 TO AMENDMENT No. 597
 ADOPTED THE 23RD DAY OF June, 2003

ISLINGTON AVENUE CORRIDOR SECONDARY PLAN
 CITY OF VAUGHAN

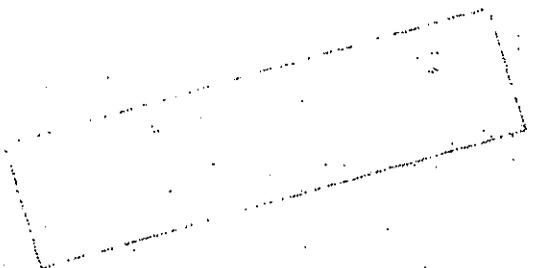
SIGNING OFFICERS


 MAYOR

 CLERK

APPENDIX "A"

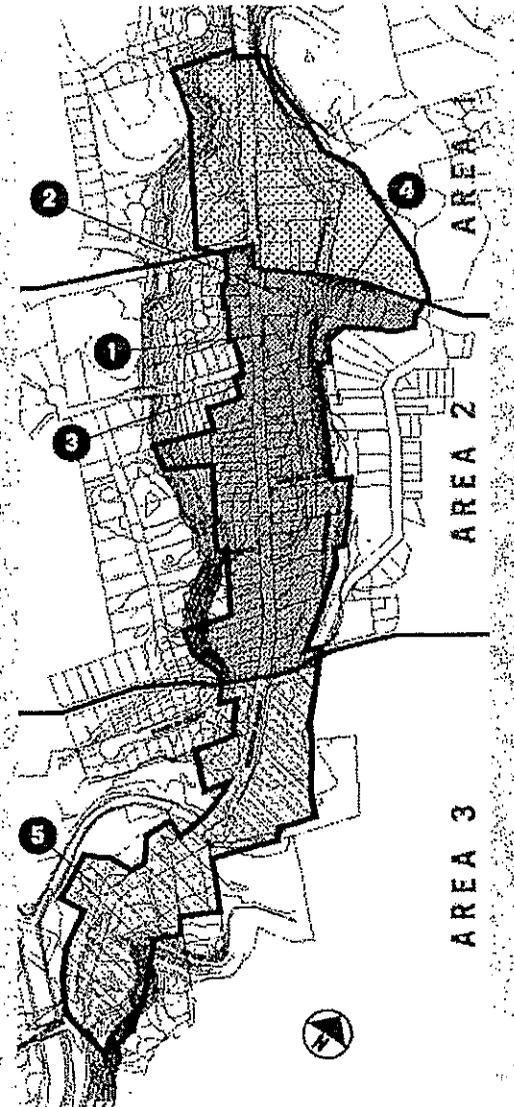
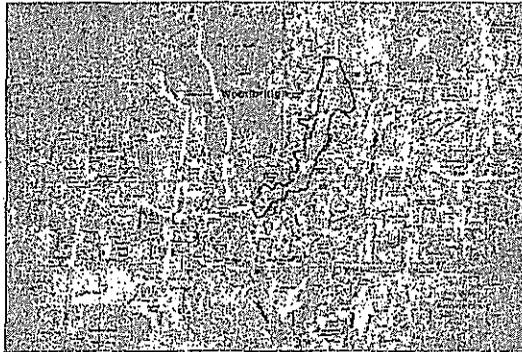
1. ISLINGTON AVENUE LAND USE STUDY AREA	1
2. BACKGROUND	2
2.1 Overview	2
3. ROLE OF THE GUIDELINES TO DIRECT PUBLIC AND PRIVATE DEVELOPMENT	4
4. ENFORCEMENT OF THE GUIDELINES	5
5. GENERAL URBAN DESIGN PRINCIPLES AND OBJECTIVES	6
5.1 Community Wide Urban Design Principles	6
5.2 Community Wide Urban Design Objectives	9
6. SITE SPECIFIC URBAN DESIGN GUIDELINES	12
6.1 Site Design	12
6.1.1 Small Lot Residential Condition	12
6.1.2 Large Lot Residential Condition	13
6.1.3 Areas of Intensification Condition	13
6.1.4 Institutional and Commercial Condition	14
6.1.5 Public Space Condition	15
6.2 Built Form and Site Organization	15
6.2.1 Building Siting and Orientation	15
6.2.2 Building Height and Massing	17
6.3 Building Design	18



6.3.1	<i>Materials</i>	18
6.3.2	<i>Treatment of Doors and Entries</i>	19
6.3.3	<i>Use of Colour</i>	19
6.3.4	<i>Building Lighting</i>	19
6.3.5	<i>Building Servicing</i>	19
6.3.6	<i>Location and Treatment of Surface Parking</i>	20
6.3.7	<i>Environmental Considerations</i>	20
6.4	Private Space Landscape Design	22
6.4.1	<i>Guiding Landscape Principles</i>	22
6.4.2	<i>Landscape Design / Small & Large Lot Residential</i>	22
6.4.3	<i>Landscape Design / Areas of Intensification</i>	23
6.4.4	<i>Landscape Design / Institutional and Commercial</i>	23
6.4.5	<i>Landscape Design / Public Space</i>	24
6.5	Streetscape Design	26
6.5.1	<i>Hamlet Identity</i>	27
6.5.2	<i>Street Tree Planting</i>	28
6.5.3	<i>Streetscape Paving</i>	30
6.5.4	<i>Streetscape Furniture</i>	30

1: ISLINGTON AVENUE LAND USE STUDY AREA

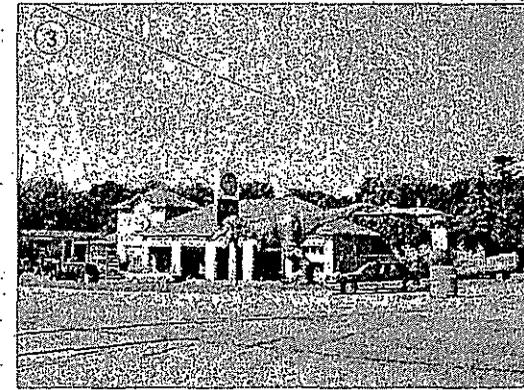
The Islington Avenue Study Area is approximately two kilometres in length, extending from Woodbridge Avenue at the south to Langstaff Road at the north.



In the northern section (Area 1), from Langstaff Road to where Hayhoe Lane intersects with Islington Avenue, the community consists of a mixture of larger single family detached homes, and a number of church properties with large lots and generous setbacks. This section of Islington Avenue typically has generous natural landscape character, and a "rural" setting. The architectural character is varied and diverse, as is the natural topography both east and west of the road.



In the central section (Area 2), from Hayhoe



Lane south to Willis Road, the community consists of a diverse mixture of uses including single family homes, both old and new, churches, a Montessori School, a seniors' nursing home, a condominium apartment building, service station and convenience commercial development. On the block east of Islington Avenue, off Pine Grove Road,

exists the Hayhoe Mill, an industrial use.

Although not a dominant building as seen from the street, the mill is located one block east and is visible from

Islington Avenue. The overall character of this intersection is diverse, and there is no one dominant theme, architectural style or character.

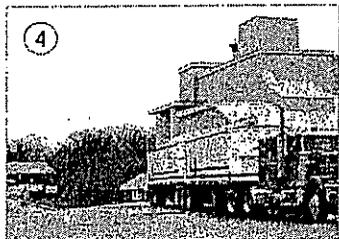
Further south, Hartman Avenue is a smaller street with a T-intersection, which extends only to the east of Islington Avenue. There, the lots are defined by the Humber River on the east. The front yard setbacks are generous, often with mature trees in the front yard.

The built form and architectural character is varied and includes older cottage-type dwellings and more modern brick and stone dwellings.

In the southern section (Area 3), from Willis Road south to Woodbridge Avenue, there is a predominance of natural landscape, open space, and mature trees. Where the East Humber River crosses Islington Avenue, there is a sense of entrance into the "village" and a sense of departure from the scale of the Woodbridge Core.

Within this area the land uses include open

space, commercial, convenience retail, institutional (both modern and historic churches) and residential. The built form varies greatly, from townhouse clusters and single family homes to a large office building located just north of where Islington meets the Woodbridge Core.

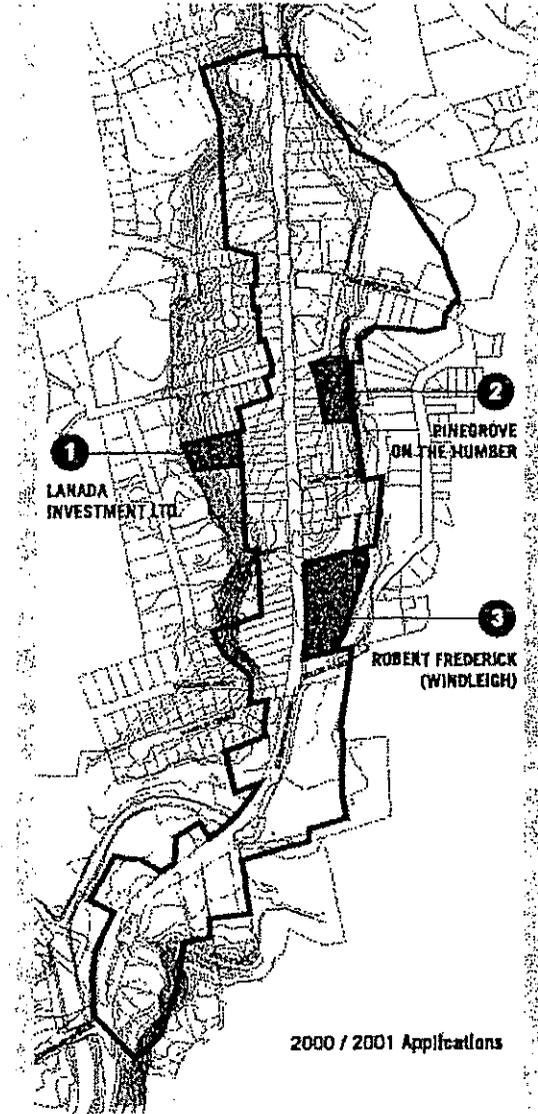


2. BACKGROUND

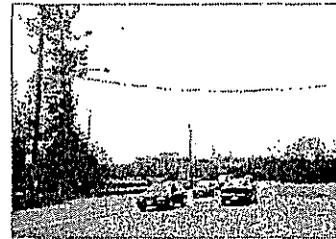
2.1 Overview

In 2000 and 2001, the City of Vaughan received three separate applications for high-density residential developments within the Study Area, which raised considerable concern from the residents within the area. Aside from traffic and strain on existing infrastructure, the concerns included Urban Design and Built Form issues, including height, visual impact and compatibility with existing built form.

On October 15, 2001 a Special Committee of the Whole Meeting was held to deal with the applications and Council adopted recommendations to carry out a Comprehensive Land Use Study of the Islington Avenue Core.



In December 2001, the City of Vaughan released an RFP and the study was awarded to IBI Group in February 2002. After an extensive public consultation process and series of meetings, public presentations and open houses, the Islington Avenue Land Use Study – Final Report was submitted to the City and its recommendations adopted on October 15, 2002.



The Study recommendations addressed Implementation, Public Connections, Land Use, Environment and Transportation. With regard to Urban Design, it was recommended that a series of Urban Design Principles be incorporated into a set of Urban Design Guidelines to be adopted under new Official Plan Amendments for the Study Area. IBI Group was retained to prepare these guidelines.

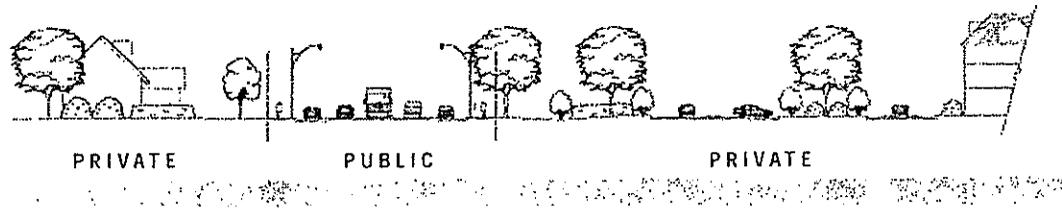
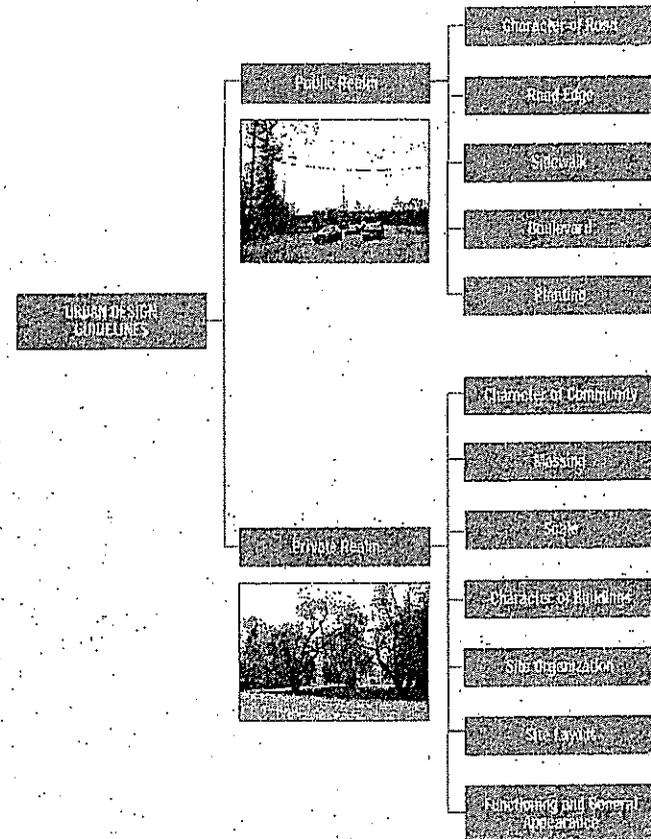
3. ROLE OF THE GUIDELINES TO DIRECT PUBLIC AND PRIVATE DEVELOPMENT

The role of Urban Design Guidelines within the Islington Avenue Corridor is to provide direction on how the Community evolves as development applications are made.

Within the road right-of-way the guidelines prescribe the character of the road improvements, the road edge, the sidewalk, the boulevard, any planting within the road right-of-way and any special features that make the streetscape special and unique from other roadways.

Beyond the road right-of-way, in the private realm, the guidelines prescribe the character of the community as private applications are made to change properties within the community. These guidelines give guidance to the location, massing, scale and character of buildings, to site organization, and to the general appearance.

The work which will be undertaken in the public realm including various streetscape improvements, will occur over time as funds are made available as they become available. Portions of the work may be undertaken by the public sector or by various grant programs as they become available. Other improvements may be undertaken by private developers on both private and public lands, as required and negotiated through site plan agreements.



4. ENFORCEMENT OF THE GUIDELINES

The Design Guidelines will be enforced through a site specific Zoning By-law that applies to the lands that fall within the Study Area. Although traditionally single family and semi-detached homes do not require site plan approval, it is recommended that in this case, all new development within the Study Area should be subject to Site Plan Review.

The implementation of the Islington Avenue Secondary Plan is through site specific Zoning By-law Amendments. As such, the intent of the Design Guidelines will be enforced through the site specific Zoning By-law Amendments for all lands that fall within the Islington Avenue Secondary Planning Area. Further, the detailed review and implementation of the Design Guidelines will be through the appropriate Site Plan Approval process.

The City of Vaughan Urban Design Department shall be the reviewing authority

for the enforcement of these Guidelines.

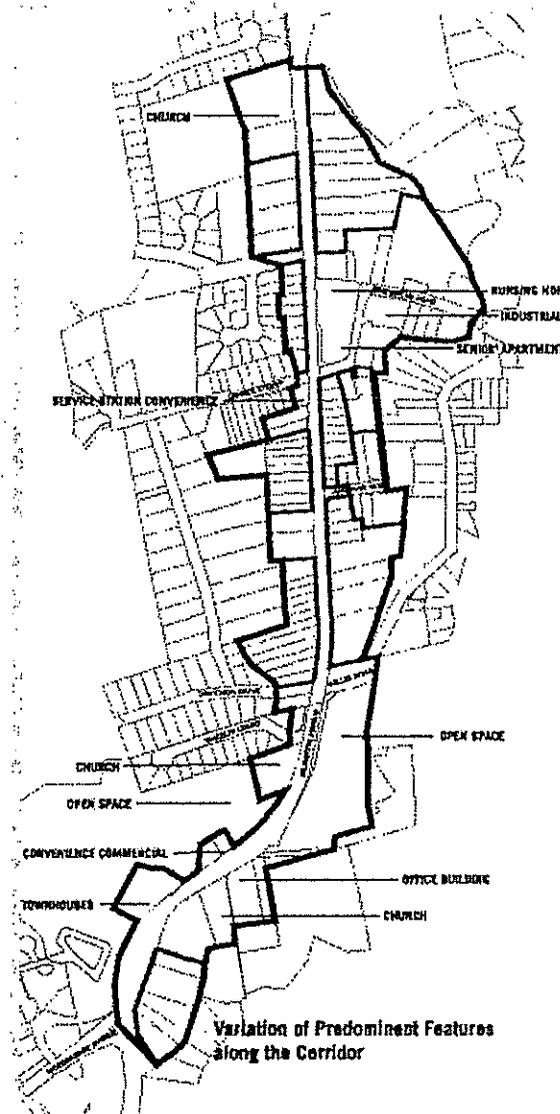
Should a dispute occur regarding the compliance to these guidelines between an applicant and the Urban Design Department, the Commissioner of Planning for the City of Vaughan shall be the final authority to determine compliance of the Guidelines.

5. GENERAL URBAN DESIGN PRINCIPLES AND OBJECTIVES

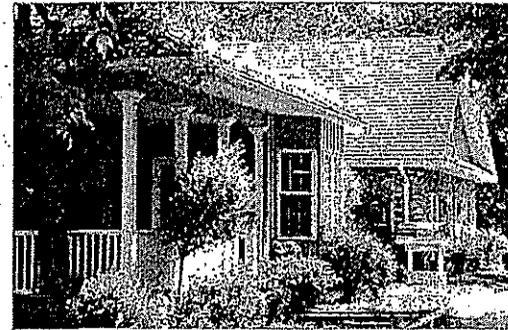
5.1 Community Wide Urban Design Principles

There are a number of general urban design principles which apply at a "community" level to all sites within the Study Area. These have been established through the public consultation process, and reflect the uniqueness of this Study Area. The overall character is unique as compared to most arterial corridors because of the predominance of the natural environment, the established built form, some of which dates back to the turn of the century, the topography and the presence of mature trees that give this an old village or hamlet character.

The East Humber River is a major natural feature which runs parallel to Islington Avenue along the east edge of the study area. It creates a natural boundary to the lots fronting onto the east side of Islington Avenue from



Langstaff Road south to Pine Grove and further south to where it crosses Willis Road. Further south, the River bends to the west and crosses Islington Avenue where the flood plain's natural beauty stretches across both sides of the road.



Over the past few years, the area has been undergoing change, including the introduction of new developments that are beginning to change the built form and the overall character of the corridor.

The character of the corridor will also change with the reconstruction of the road from its current width to a three to five lane cross-section to address current traffic problems. The challenge will be to improve the road



network for the residents and the regional neighbours, without losing the character and appeal of what exists today.

The following principles should be followed at a community-wide level to retain the character of the village that exists today.

5.1.1 Respect and protect the existing scale and character of the Islington Avenue Corridor that exists today.

Criteria required to achieve the principle:

- A. Respect and preserve the predominant built form of the single family neighbour-

hood that exists today;

- B. Allow intensification of development in pockets where the visual and physical impact can be minimized through coordinated design solutions consistent with the intent of this report;

- C. Respect and maintain existing height and setback requirements along the Islington Avenue frontage, except where lots have been identified for intensification of built form.

5.1.2 Protect the existing landscape character of the Islington Avenue Corridor.

Criteria required to achieve the principle:



- A. Respect and preserve the existing setbacks that exist along Islington Avenue today;
- B. Protect and preserve existing trees along the corridor;
- C. Limit the site coverage requirements and restrict front yard parking along the corridor;
- D. Coordinate driveway connections and curb cuts to minimize pedestrian / vehicular conflict;
- E. Replace trees that must be removed with large caliper replacement trees at a higher replacement ratio, as defined by the York Regional Forestry Department or the City of Vaughan.

Depending on the caliper, species, age



and character of the tree, at a minimum, the minimum tree-replacement ratio would be 3:1. This should be subject to the discretion of York Regional Forestry approval.

5.1.3 Enhance the pedestrian environment along Islington Avenue

Criteria required to achieve the principle:

- A. Coordinate driveway connections and curb cuts to minimize pedestrian/vehicular conflict;
- B. Prepare a planting plan to create a continuous overhead canopy along the sides of Islington Avenue, with a variety of complementary street trees;
- C. Encourage additional planting of street trees and private front yard trees and a coordinated sidewalk and street furniture program;
- D. Discourage front yard parking and prohibit garages that protrude beyond the front line

of the house;

- E. Apply and enforce application of principles from the City of Vaughan Design Standards Review, where lot sizes are applicable.



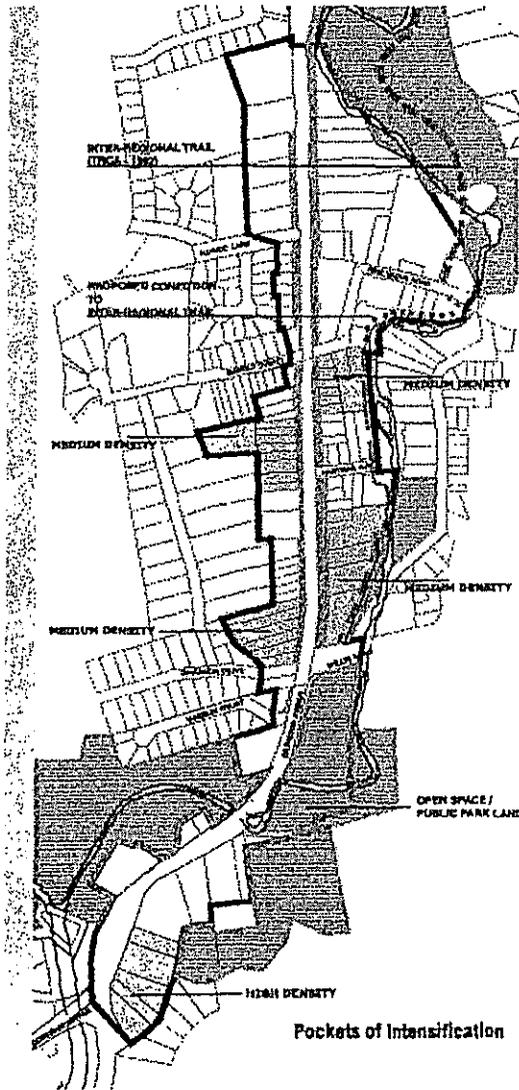
5.1.4 Provide a Comprehensive Streetscape and Linear Park System with Linkages to the Adjacent Open Space System

Criteria required to achieve the principle:

- A. Identify open space linkage opportunities in both the public and private ownership and work towards providing public access to that system;

- B. Identify park dedication opportunities in locations where they will most benefit the public realm;
- C. Identify setbacks within private ownership that visually contribute to the open space system;
- D. Promote links to special feature areas including the Woodbridge Core, heritage buildings, churches and open spaces.





5.1.5 Only allow for Intensification in Pockets Identified in the Land Use Study

Criteria required to achieve the principle:

- A. The Land use Study identifies three pockets of medium-density development and one pocket of high-density development. These three areas are to be examined on a site-specific basis, following the guidelines put forward in the document.
- B. Evaluate applications based on their design merits and the extent to which they enhance the public realm and the natural environment;
- C. Prepare site specific zoning for these areas of intensification that are based on built form, compatibility with adjacent properties, and minimal impact to the public realm and the natural environment.

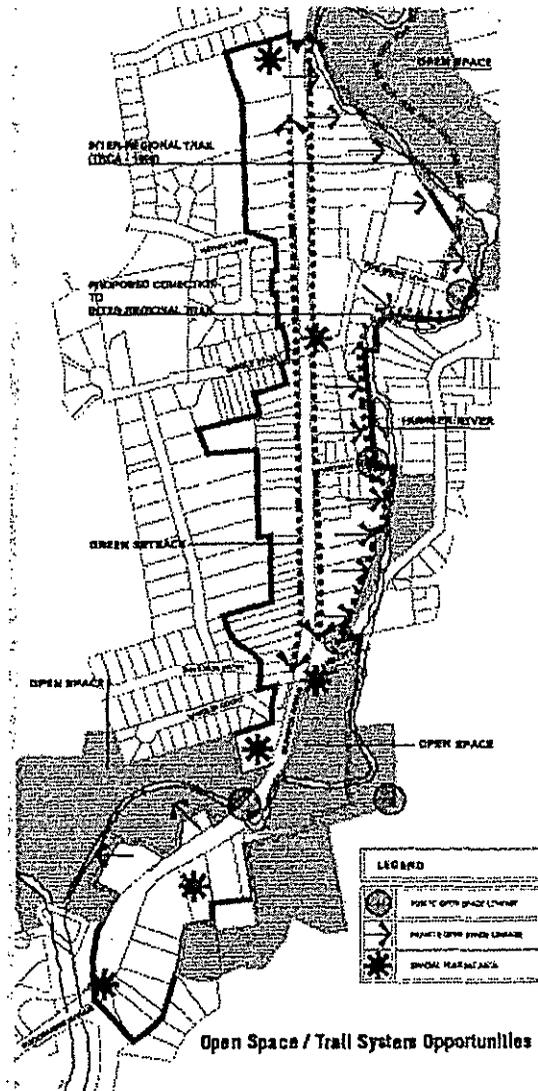
5.2 Community-Wide Urban Design Objectives

Community-wide Urban Design Objectives have been drafted to help guide development along the Islington Avenue Land Use Corridor:

- 5.2.1** Public access and visibility to natural areas should be encouraged and preserved through the location of built form. The siting of any new development should have regard for views to the East Humber River, the vegetated valley slope bank located west of Islington Avenue from Gamble Street to Davidson Drive and the

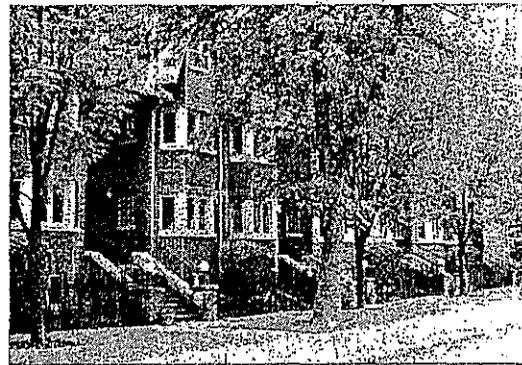


mature trees that exist along the corridor;



5.2.2 Any new developments should be sited in a positive orientation to Islington Avenue, with front doors and animated portions of buildings related to the street. In no case should buildings be back-lotted onto Islington Avenue or any major public open space;

5.2.3 Any new development should contribute to the creation of a pedestrian-friendly environment along Islington Avenue. This should be enhanced through coordinated street tree planting, street paving, sidewalk enhancements, street furniture, signage and lighting;



5.2.4 A variety of dwelling types; characters and setbacks is encouraged to maintain the diversity that exists today. Front yard setbacks are to be established based on where built form exists today, the location of mature trees, and alignment with similarly sized neighbouring properties;

5.2.5 The scale and massing of all new development should be characteristic of the hamlet feel that is predominant within the Islington Avenue Corridor. Where the height of a proposed building exceeds the neighbouring building, the massing should step down to recognize its neighbour and reduce both visual and shadow impact.

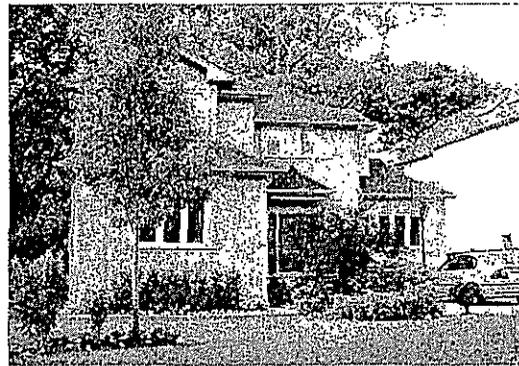
5.2.6 Front porches and/or covered entrances are encouraged in the front yard, in an effort to animate the street, create a positive building edge and provide eyes on the street.



5.2.7 Front yards or side/rear yards fronting onto publicly accessible space should be designed as positive, pedestrian spaces and not used for parking or servicing;

5.2.8 The open space of any new development should be designed to relate to and enhance adjacent open space and should support a range of pedestrian-based activities. This may include both privately owned setbacks along the Humber River land conveyed as parkland to public ownership, or cultural meeting places for seasonal activities such as plays, festivals, arts and craft sales and other special events;

5.2.9 Any new development should have regard for "green" principles of conserving energy, preserving natural resources and minimizing lot coverage. Wherever possible, hard-surfaced areas should be constructed of permeable materials that allow for a higher degree of ground water infiltration.



6. SITE SPECIFIC URBAN DESIGN GUIDELINES

6.1 Site Design

This section refers to the general character of the site, the lot and the landscape. It includes site organization, lot coverage, setbacks, building envelopes, building design and landscape design.



The Land Use Plan and Zoning By-Law identify a number of densities of development. These can be generally characterized as

- Small Lot Residential (low density)
- Large Lot Residential (low density)

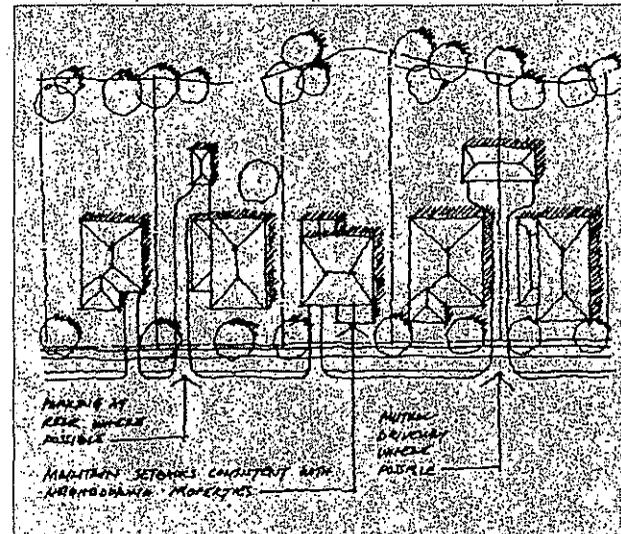


- Areas of Intensification (medium and high density)
- Institutional
- Commercial
- Public

6.1.1 Small Lot Residential Condition (Low Density)

The small lot residential condition is a condition where a single family house is generally located on a lot less than 15 meters wide or smaller; this includes semi-detached homes. In this condition, the following guidelines apply:

- Houses should be setback to a front yard setback line that is determined to be no less than the average of the two adjacent properties, maintaining the same front yard that exists today;
- Where the site can accommodate a driveway, the front yard should be reserved for landscaping, and not parking; driveways should be designed to access parking areas at the sides and the rear of the houses;





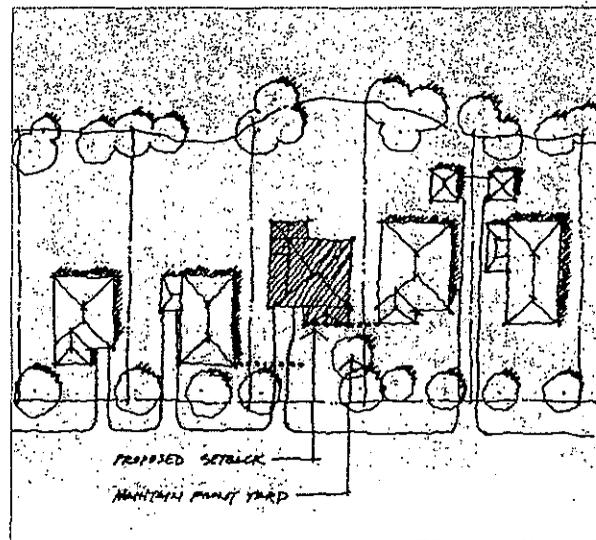
- C. The design and character of housing should vary along the street and avoid repetition on a given portion of street;
- D. Houses should be designed to not be dominated by garage and driveway – priority should be given to the positive elements of the house, its scale, its proportions and its overall architectural character.

6.1.2 Large Lot Residential Condition (Low Density)

The large lot residential condition is the condition where a house is located on a lot greater than 15 meters wide. In this condition, the following guidelines apply:

- A. Houses should be setback to a front yard setback line that is determined to be no less than the average of the two adjacent properties, maintaining the same front yard that exists today;
- B. Where the site can accommodate a laneway, the front yard should be reserved for landscaping, and not parking; laneways should be designed to access parking areas at the sides and the rear of the houses;

C. The design and character of housing

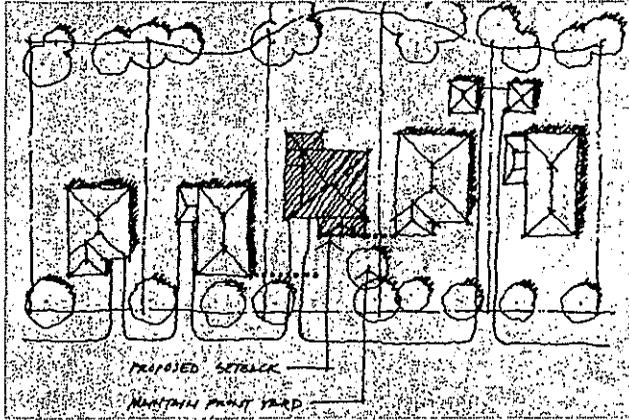


- should vary along the street and avoid repetition on a given portion of street;
- D. Wherever possible, the use of laneways designed to provide for public safety, increased parking on private property and contribute to a positive community benefit should be provided;
- E. Houses should be designed so as not to be dominated by garage and driveway – priority should be given to the positive elements of the house, its scale, its proportions and its overall architectural character.

6.1.3 Areas of Intensification Condition (Medium and High Density)

Areas of Intensification have been identified in the Islington Avenue Land Use Plan. In these areas, where a new development may take the shape of a townhouse or an apartment built form, the following guidelines apply:

- A. The built form located closest to the street



should be setback to a line that is consistent with where the existing front yard is today, or where there are varying conditions, the setback is determined to be no less than the average of the two adjacent properties, thereby maintaining approximately the same front yard that exists today;

- B. The built form should be oriented towards Islington Avenue and organized in a manner that creates an animated public frontage;
- C. The front yard setback should be reserved for landscaping, and not parking; laneways

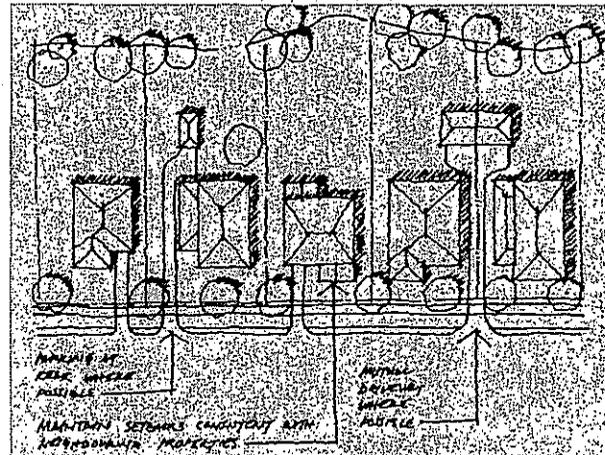
should be designed to access parking areas at the sides and the rear of the development;

- D. Buildings should be sited and designed to minimize the visual impact and overshadowing of adjacent properties;
- E. Wherever possible, the use of driveway should be incorporated to provide access to private property and these driveways should be designed to be a part of the open space system and incorporated into the landscape design of the properties.

6.1.4 Institutional and Commercial Condition

The institutional and commercial condition includes properties which are currently zoned for institutional and/or commercial uses that provide some form of service to the community. These properties include churches, schools and convenience retail and should be responsible for providing a positive public image and character.

- The front yard should be reserved for landscape improvements which provide for public access and animated uses, such as plazas or patios which encourage activity and eyes on the street;
- Driveways within the front yard setback should be kept to a minimum and all park-



ing and servicing should be provided to the rear or side of the building and not in the front yard setback.

6.1.5 Public Space Condition

The public space condition includes any publicly owned and/or publicly accessible open space that is physically adjacent to the public realm. This includes lands that are currently under the jurisdiction of the Toronto Region Conservation Authority (TRCA) and the City of Vaughan. Improvements on these lands should follow the applicable guidelines listed below:

- A. Where the streetscape passes through a public open space, the design of the open space should be an integral part of the design of the streetscape, and should be designed as one space;
- B. The location of sidewalks or pedestrian trails should be designed to be away from the road edge to minimize the impact of traffic on the pedestrian environment, to create a better and safer pedestrian experience, and to integrate the streetscape design into the park and open space system;
- C. Entrances to parks and open spaces

should be celebrated as special places, and the design should include walls, features, planting focus areas and interpretive signage that signify the importance of these areas as "gateways" to park and open space system. All structures located within the flood-prove areas controlled by the TRCA are subject to final TRCA approvals.

- D. Any above grade services required within the parks should be located so as to minimize their visual impact and should be integrated into the landscape design of the parks or open spaces.

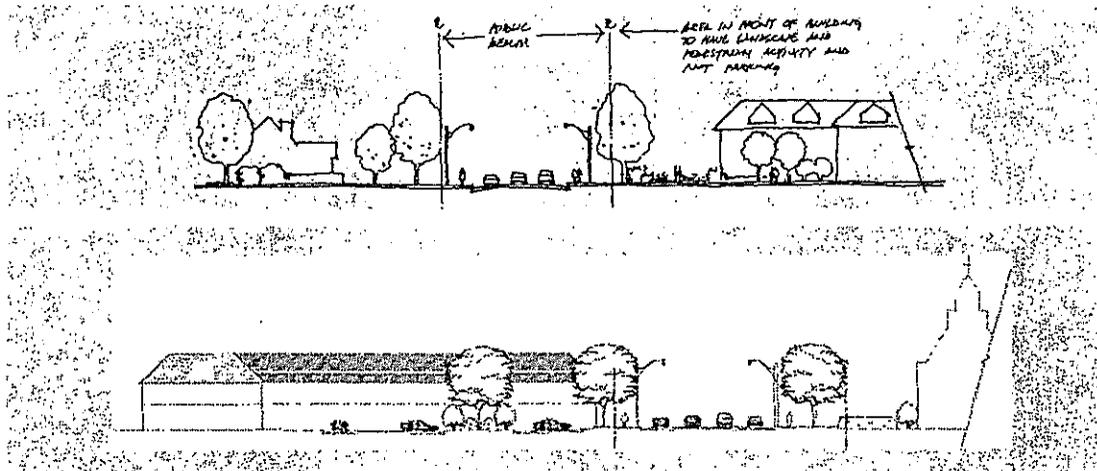
6.2 Built Form and Siting



Built form and siting refers to the size, massing and general appearance of buildings and the way they are located on the site.

6.2.1 Building Siting and Orientation

- A. Site plan submissions should include adjacent developments, all relevant setbacks, top of bank locations, easements and any fill lines that may affect either the siting or the environmental conditions on adjacent sites;
- B. Buildings should be sited to promote positive building to street relationships, by providing building frontages;
- C. Reverse lot development will not be permitted along Islington Avenue. Where parking access is located off the street by way of an internal driveway circulation system, the units should still have a functional pedestrian entry that relates to the street;
- D. Where zones of either medium or high density development occur, as defined in



the Zoning Bylaw, which are located adjacent to single family housing or low density development, there must be an appropriate transition in scale and building form. Buildings should be sited to minimize their impact on neighbouring properties and the massing and height should be terraced to respect the neighbouring condition;

E. Where site conditions permit, buildings should be oriented, generally in an east west direction, to avoid the creation of a continuous building wall running parallel

to the East Humber River Valley or to Islington Avenue. Where lot sizes or configurations do not allow this to occur, then buildings should be broken with visual breaks between sections of buildings;

F. The built form along Islington Avenue should include a variety of building types and setbacks, creating a diverse street edge, dominated by landscaped front yards;

G. Buildings and building entrances should be oriented towards the street with front doors and useable spaces like living rooms, common rooms and kitchens ori-

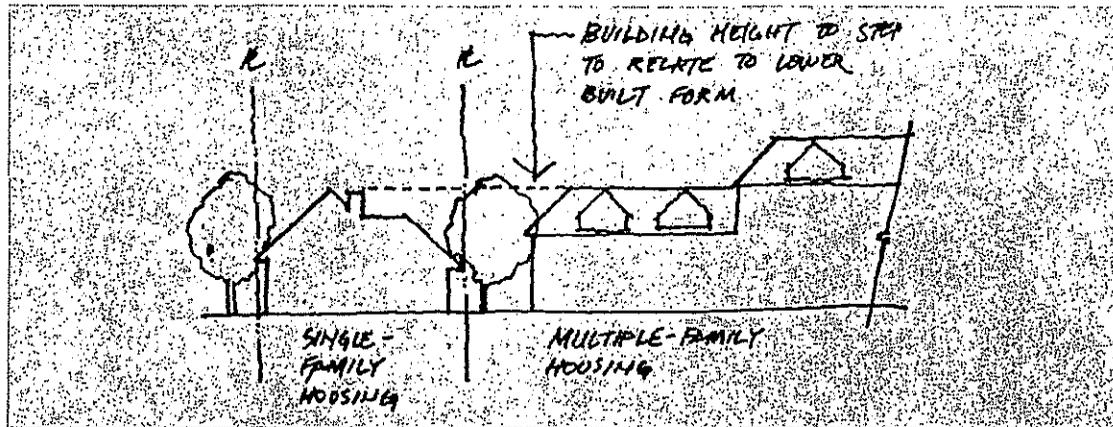
ented to the street, thereby creating a positive building edge and eyes on the street;

H. Buildings should be sited so the dominant element is not the garage driveway, the garage, or in the case of an apartment building, the access to parking. Wherever possible, garages should be located on the side or rear of the building and not facing the street;

I. Buildings located on corner lots should be oriented in a positive manner to both streets, with animated uses on both frontages and the driveway and garage on the less dominant frontage;

J. Commercial and institutional buildings should have a positive relationship to the street and not be separated from the street by either parking or drive aisles;

K. Service areas and access to underground parking should be designed to be integral with the buildings and incorporated into the overall landscape design.



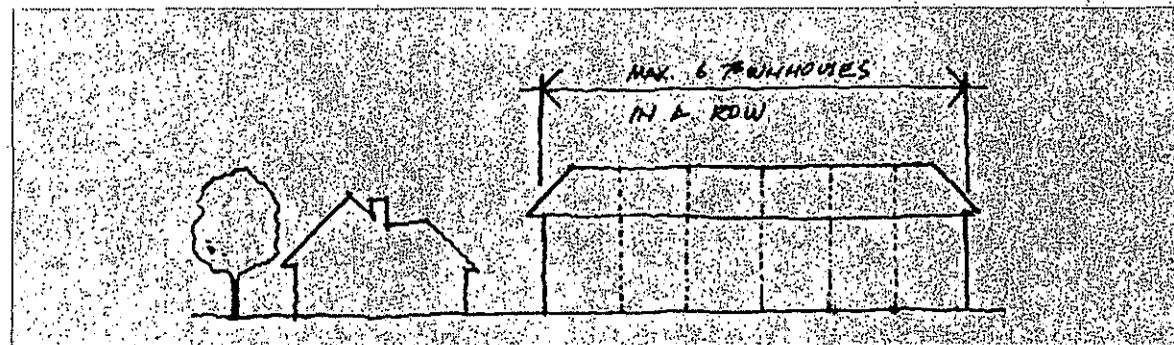
6.2.2 Building Height and Massing

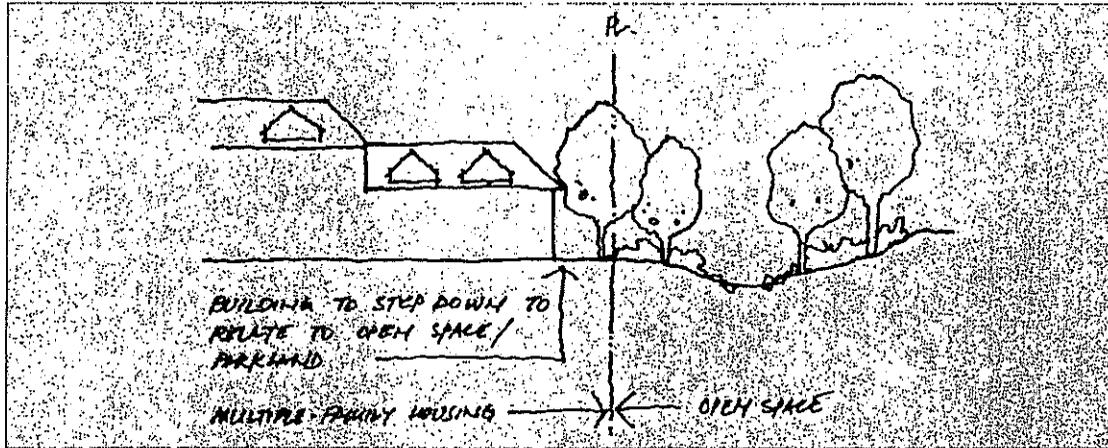
- A. The maximum permitted height of buildings in a medium density zone is restricted to 3.5 storeys (11 metres). In a high density zone, the maximum permitted height is restricted to 5 storeys (15 metres). The building height should be measured as defined in the Zoning By-Law;
- B. Buildings should be massed to minimize their overall impact on neighbouring properties and to encourage views into the East Humber River Valley and the valley wall located west of Islington Avenue;

the Vaughan Design Standards Review;

- D. Building massing and volumes should be arranged to create pedestrian-oriented courtyards and shared private open spaces visible from the street;

- C. The overall length of an apartment building should not exceed 60 metres, and it is encouraged that buildings do not exceed 45 metres. In the case of townhouses, the overall length of a block of townhouses should not exceed 6 units as defined in





E. The design, massing and character of housing should vary along the street and avoid repetition on a given portion of street. In the case of an apartment form, the building should be articulated to visually break the building down into a town-house scale;

F. Building heights should be scaled down through articulated massing and roofline treatments. Sloped roofs with dormers or upper-storey setbacks are encouraged;

G. The garage presence on the street should be subordinate to the house design;

H. Where the garage is on the predominant building facade, garage doors should be limited to two, and the face of the garage should be set back a minimum of .6 metres from the main front elevation of house. In no case should more than 2



garages be located on the same facade;

1. Where the building is located adjacent to public open space, the massing should step down to recognize a pedestrian scale through the use of lower building elements, wrap-around porches, etc.

6.3 BUILDING DESIGN

Building design refers to the specifics of building design or those attributes that are encouraged to ensure quality architectural design, in all buildings within the study area.

6.3.1 Materials (All Zones)

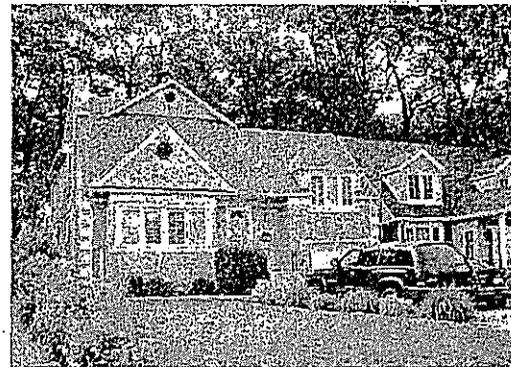
- A. Preferred building materials include brick, stucco, stone and wood siding;
- B. It is recommended that no more than two dominant materials and/or colours should be used on the dominant elevation and that materials on buildings are selected to give a clear design expression;
- C. Glass and steel as dominant building materials are strongly discouraged.

6.3.2 Treatment of Doors and Entries (All Zones)

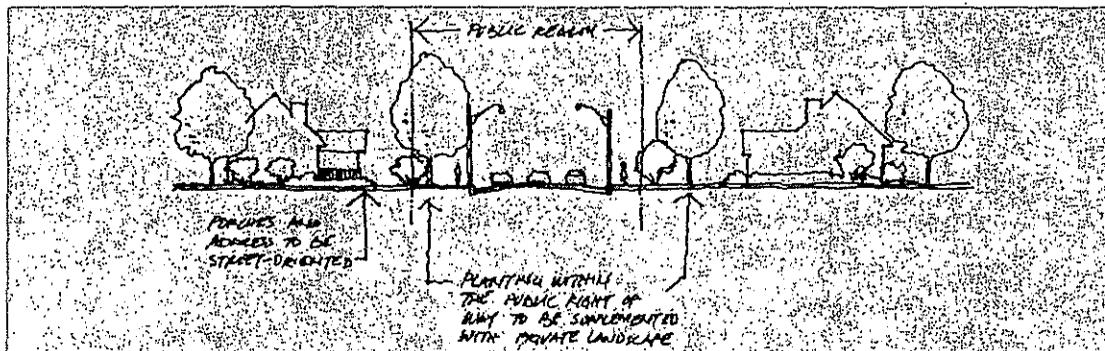
- A. Doors and entries are positive design elements and should be well designed to contribute positively to the character of a building and a streetscape;
- B. Front porches, wrap-around porches and side-lit front doors are encouraged as special focal points of architectural expression;

6.3.3 Use of Colour (All Zones)

- A. Colours of natural earth tones that blend with the natural setting are to be encouraged for predominant building facades;
- B. Complementary colours should be selected for use on trim and door elements including porches, front doors and garage doors, eaves and balconies.



- B. Spotlighting or washing of light on special building elements is encouraged and back-lighting of signs is prohibited.



6.3.5 Building Servicing (All Zones)

- C. Special attention should be paid to doorways to make them well lit, and inviting with generous landings and surrounding plantings that are well integrated with the front yard design.

6.3.4 Building Lighting (All Zones)

- A. Building lighting should be integrated with landscape design, and should be provided at a pedestrian level;

- A. Servicing areas should not be located along Islington Avenue or visible from the street;
- B. Garbage storage on multiple-unit buildings should be internal to the buildings;
- C. Garage entrances and any areas which relate to building servicing should be located discreetly within the side or rear

yard setback;

- D. Servicing on multiple-unit buildings should be coordinated and located in the least visible portion of the site and should be integrated into the building/landscape design;
- E. Location of hard-surface laneways to servicing and parking should be kept to a minimum.

6.3.6 Location and Treatment of Surface Parking (Medium and High Density Zones)

- A. Wherever possible, surface is prohibited within the front yard setback zone. The driveway width should be no wider than 6 metres and should be designed to incorpo-

rate pedestrian access and to minimize the amount of hard-surfacing;

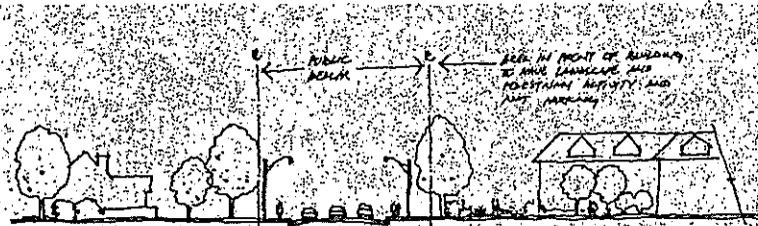
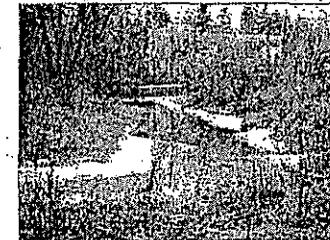
- B. Where surface parking does occur on multiple-family lots, the parking should be visibly screened from the street with a minimum landscape; screen of 3.0 metres
- C. Surface parking should be kept as minimal as possible, with a curb cut not to exceed 4 metres wide at the widest point for single family and 6 metres wide for multiple-family or shared laneways;
- D. Preferred materials for driveways are stone, permeable unit paver, unit paver, concrete, patterned concrete, asphalt and finally aggregate, in that order;
- E. Continuous sidewalk conditions should occur along the street giving priority to

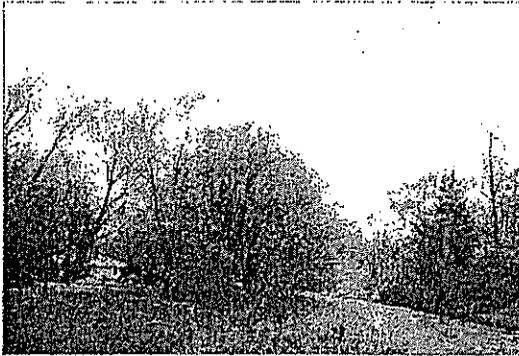


pedestrian crossing over vehicles;

6.3.7 Environmental Considerations (All Zones)

- A. There are areas within the Islington Avenue Study Area that fall within a Special Study Areas designation, as defined by the Toronto Region Conservation Authority, whereby these areas are susceptible to flooding. If there are inconsistencies with the objectives of these Guidelines, the flood proofing





requirements will prevail with best efforts to meet the intent of the Guidelines.

- B. In those areas subject to TRCA Fill Regulation Lines, where grades are being altered, grading plans will require the written approval of the TRCA. All plans will be subject to TRCA Valley and Stream Corridor Policy and TRCA approval.
- C. Stormwater management for any new development should be aimed at maintaining pre-development water balance and ensuring that any clean run-off is redirected back into the groundwater, and ultimately the Humber River. A variety of methods can be used to mitigate runoff, however each of these methods should

ensure that groundwater flows to the East Humber River are unimpeded;

- D. Sediment and erosion control plans for individual applications should be submitted to the City prior to any regrading or site servicing occurring;
- E. Location of driveways, sidewalks, parking areas or buildings, including regrading of sideslopes should have consideration for root systems of existing trees;
- F. Proposed planting should be comprised of indigenous, non-invasive species which are selected to stabilize and restore banks, and reduce potential erosion, planting plans should show inventory of size and species of existing trees;
- G. Wherever possible, soil bio-engineering techniques in combination with planting

should be used as opposed to structural solutions;

- H. Wherever possible, given the nature of the surrounding natural environment, green building techniques should be encouraged;
- I. These techniques should include re-use of grey water, stormwater management, landscape treatments to assist in micro-climatic elements and the creation of a more comfortable pedestrian environment;
- J. Wherever possible, the open space of any new development should be designed to relate to and enhance adjacent open space which supports a range of pedestrian-based activities. This may vary from a setback beside a trail along the Humber River, to cultural meeting places for seasonal activities such as plays, festivals,



arts and craft sales and other special events;

K. All areas disturbed by construction should be restored with planting of indigenous trees, shrubs and groundcovers;

L. All slopes in excess of 3:1 should be stabilized using a combination of erosion control measures and planting to occur in the same construction season as the actual site work undertaken.

6.4 PRIVATE SPACE LANDSCAPE DESIGN

Private space landscape design refers to all landscaping located on private property, primarily that which is visible from the public realm.



6.4.1 Guiding Landscape Principles (All Zones)

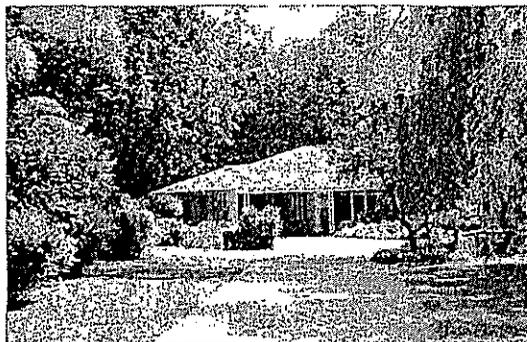
- A. Preservation of existing trees should be enforced through By-law and/or site plan agreements;
- B. Where new construction occurs adjacent to mature trees, the owner / developer shall be held responsible to protect and preserve the trees using acceptable arboricultural practices;
- C. Natural areas that are disturbed by construction are to be repaired and rehabilitated with a mix of trees, shrubs and groundcovers with no invasive species or species prone to disease;

- D. Where private development borders on naturalized areas, there may be a need for an Environmental Impact Study (EIS) as determined by the City and the TRCA, and there should be a buffer edge which remains natural, the size of which shall be determined through the EIS (if required) and be no less than that required by TRCA policy.



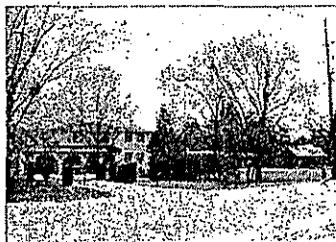
6.4.2 Landscape Design / Small & Large Lot Residential

- A. The guiding landscape design principle for small lot residential landscape is to maximize the amount of soft landscaping using native and non-invasive plant material;
- B. Plant selection should be based on seasonal colours, planting structure and strong, simple planting design;



C. Highly ornate structures, fences and formal planting and the use of strong colours, which detract from the natural environment are strongly discouraged;

D. Hard surfaces, including driveways, front landings, front porches and sitting areas should be designed to blend into the soft landscape and the natural environment using naturalized materials including stone and wood wherever possible;



E. Landscape design should give priority to

pedestrian areas and building entrances and downplay the garage and driveway condition.

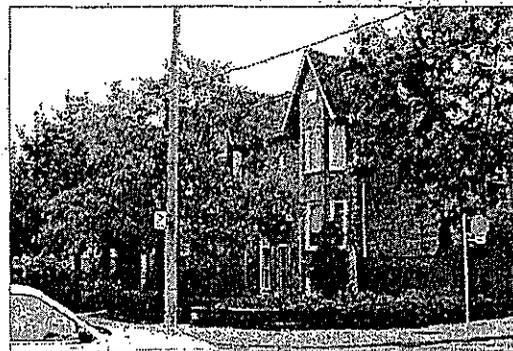
6.4.3 Landscape Design / Areas of Intensification

A. The guiding landscape design principle for large lot residential landscape conditions is to maintain the rural estate character of landscape by maximizing the character of the neighbouring woodlands through the use of native and non-invasive plant material;



B. Plant selection should be based on groupings of complementary species, seasonal colours, planting structure and strong, simple planting design that complements the adjacent East Humber River Valley and the Valley Wall slope to the west of Islington Avenue;

- C. Highly ornate structures, fences and formal planting and the use of strong colours, which detract from the natural environment are strongly discouraged;
- D. Hard surfaces, including plazas, forecourts and terraces should be designed to blend into the soft landscape and the natural environment using naturalized materials including stone and wood wherever possible.



6.4.4 Landscape Design / Institutional and Commercial

A. The guiding landscape design principle for institutional and commercial landscape conditions is to minimize the amount of

hard-surfacing and to maximize the amount of soft landscaping using clusters of hardy, low maintenance, native and non-invasive plant material;

- B. Plant selection should be based on seasonal colours, planting structure and strong, simple planting design. Seating areas and forecourts of buildings visible from the street should include planting of ornamental trees and shrubs that highlight activity areas that should encourage pedestrian activity;
- C. Spaces should be designed to be inviting to the public, to be visible from the street, to be well-lit and of a comfortable pedestrian scale and to be designed to be complementary of the architecture of the buildings and adjacent neighbourhood;
- D. Highly ornate structures, fences and formal planting and the use of strong colours, which detract from the natural environment are strongly discouraged;
- E. Signage should be floodlit or downlit and

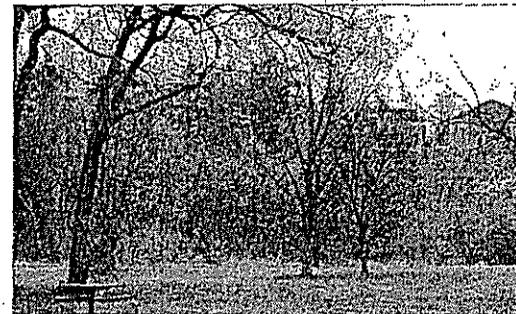
be discreet and respectful of the residential neighbourhood within which it resides;

- F. Pylon signs and portable signs are prohibited. At service stations, where pylon signs are permitted, the lower form of pylon signs are strongly encouraged;
- G. Hard surfaces, including plazas, forecourts and terraces should be designed to blend into the soft landscape and the natural environment using naturalized materials including stone and wood wherever possible.

6.4.5 Landscape Design / Public Space Including Streetscape and Parks or Open Spaces

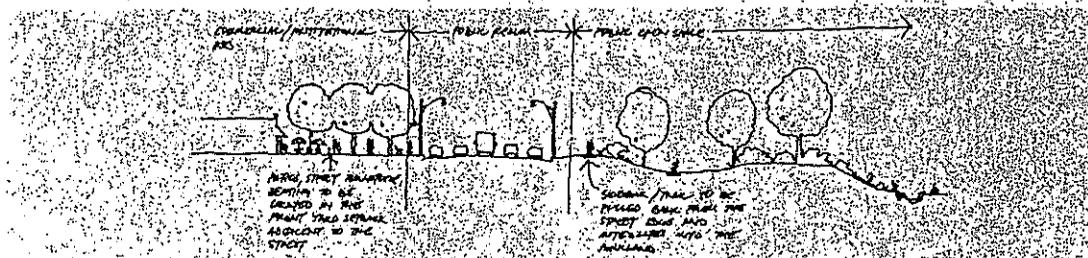
- A. The guiding landscape design principle for the design of Public open space, whether owned by the City of Vaughan or the

Toronto Region Conservation Authority (TRCA), is to provide a safe, comfortable and visually aesthetic pedestrian environment for the use of the community which is made up of street tree planting, park



improvements and a coordinated pedestrian realm which includes hardscape and street furniture;

- B. Special feature areas should be located along the corridor, linked by a common



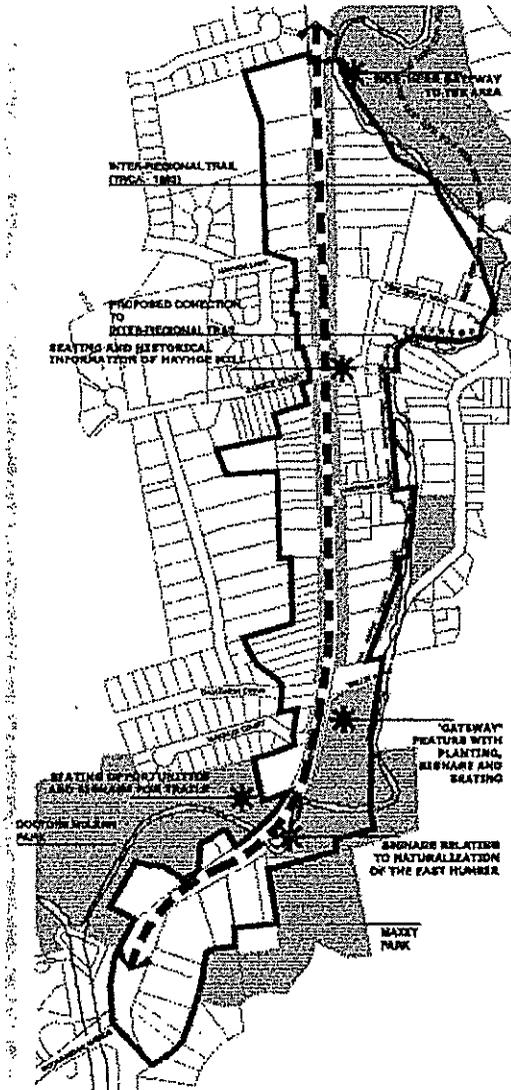


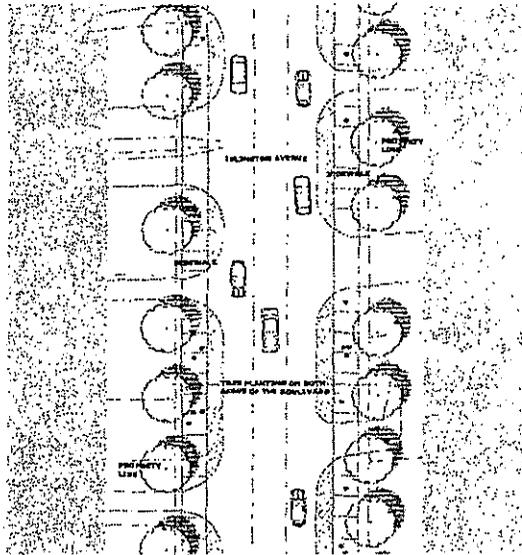
theme, design and selection of detailed materials. These areas should include, but not be limited to the following areas:

- C. The west side of Islington Avenue north of the Woodbridge Avenue bridge, where the local trail connects to the Inter-Regional Trail system. This area should include seating opportunities and signage to both local and regional trails;
- D. The bridge crossing where Islington Avenue crosses the East Humber River is an opportunity to provide interpretive signage relating to naturalization of the East Humber and upstream improvements currently underway;
- E. The Open Space located at the southeast

corner of Islington and Willis provides an opportunity for a "gateway" feature with planting, signage and seating which defines the "hamlet", its local history, its pre European settlement by First Nations groups, the Toronto Carrying Place Trail are specifically information regarding Hurricane Hazel;

- F. The intersection of Gamble Street/ Pine Grove Road is an opportunity to provide for a seating area/resting area, close to the seniors housing, that recognizes Hayhoe Mill as an important contributor to the community and a historical presence;
- G. The north end of Islington Avenue, close to where the East Humber River "touches" the east side of Islington Avenue provides an opportunity for a northern "gateway" to the "hamlet" for those travelling southward along Islington Avenue.
- H. Whether in public or private ownership, safe and convenient access is key to the limited parkland that is available within the Study Area;





I. At a regional level, the Islington Avenue Corridor is a critical link to the Regional Trail Network that will eventually tie the Martin Goodman Trail along Toronto's waterfront to trails in the Bolton region. Varying from a roadside trail along the west side of Islington adjacent to the Woodbridge Core to a sidewalk, the trail should be identifiable as a piece of this critical linkage and should be signed appropriately;

J. Wherever possible, the trail should be constructed to provide for up to 3.0 metres of hard surfaces for walking, running, cycling or rollerblading. The trail width should never be less than 1.5 metres in width;

K. Wherever the sidewalk or trail passes through a park or open space, such as along the east side of Islington Avenue south of Willis Road, the traveling portion of the sidewalk should be pulled back from the curb edge by 4 to 5 metres to allow for a soft edge along the roadside and to separate the pedestrians from the road edge;

L. In more urban conditions, where traffic is generally slower and the right-of-way is

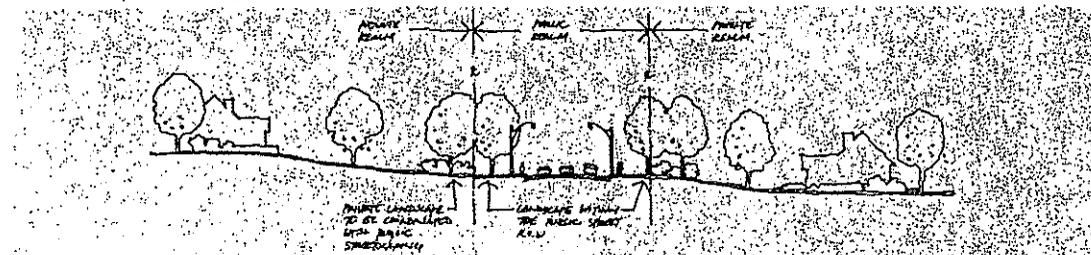
limited, the planting along this expanded boulevard, should occur on both sides of the sidewalk to create a canopy.

6.5 STREETSCAPE DESIGN

Streetscape design refers to all areas within the public road right-of-way along the edges of Islington Avenue and any existing or proposed local roads that extend off Islington Avenue along the corridor. These include walkways, mid-block connections, greenways.

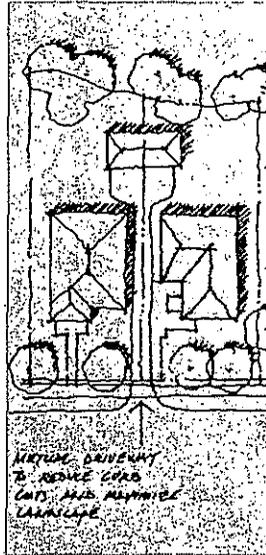
6.5.1 Hamlet Identity

A. In all cases, efforts should be made to reduce the amount of paving, the number and size of curb cuts and to increase the



amount of soft landscaping along the edge of the right-of-way;

- B. Boulevards should be designed to maximize the amount of planting, and where possible, to incorporate irrigation and drainage within the landscaped boulevard;

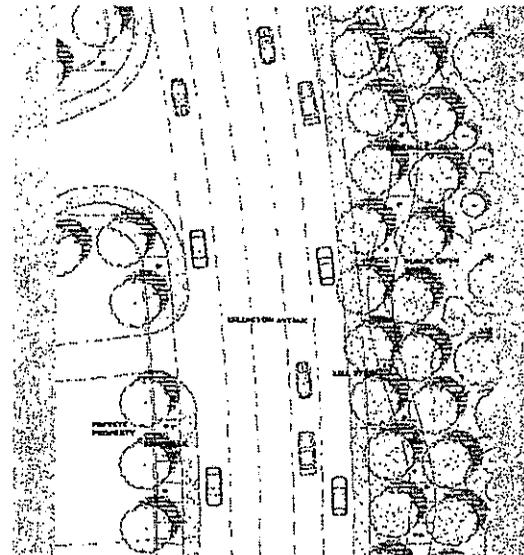


- C. The Vaughan City standard allows for a minimum 1.5 meter wide sidewalk, the sidewalk should be located so as to mini-



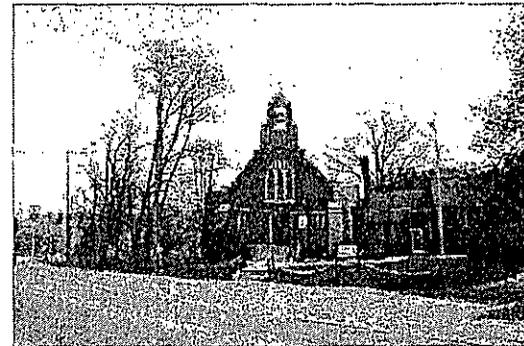
mize the amount of regrading and possible loss of existing trees;

- D. Wherever possible, taking into account location of overhead wires, the planting of street trees along the edge of Islington Avenue should be placed in alignment to provide for a continuous overhead canopy;
- E. Trees should be spaced at 8.0 to 10 meter intervals depending on driveway locations and size of trees. Smaller trees, for example adjacent to convenient retail, can be spaced more closely to reflect a tighter urban environment;
- F. Due to the traffic volumes along Islington Avenue, all trees should be salt and pollution tolerant and should be selected for



seasonal colour and variety;

- G. In places such as along parks or open spaces, where there is an abundance of



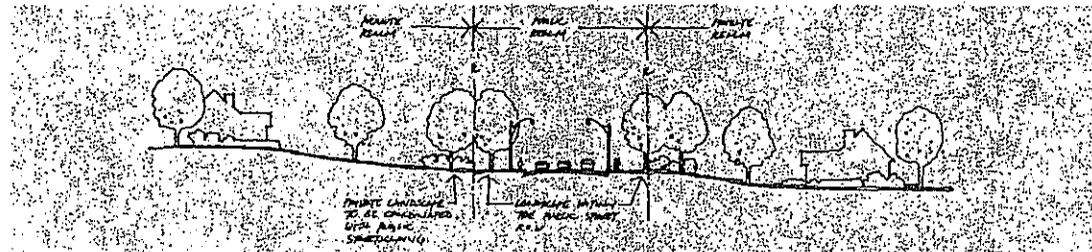


room for both root and canopy growth, special species should be located for accent.

6.5.2 Street Tree Planting

- A. The current construction plans for the Islington Avenue reconstruction have included a 1.0 metre (varies) asphalt strip on the back face of curb to assist in the protection of trees and sod from salt sprayed from the roadway;
- B. Whereas the Region of York will have street tree planting within the public right-of-way, planting within the private property should enhance the streetscape improve-

- ments;
- C. Specific locations of street trees relative to the public right-of-way are subject to the approval of the City of Vaughan and York Region;
- D. Street trees should be selected based on no more than 6 trees of the same species
- E. In areas where street trees occur adjacent to the Humber River Valley, trees should be indigenous and if space permits, enhanced plantings which restore and contribute to the natural environment should be encouraged. These areas include stream crossings and river banks.
- F. It is preferable that all street trees be indigenous, specifically in areas adjacent to existing natural areas; non-native or invasive species will not be acceptable.



- G. Plant selection should be made with regard to general hardiness, and resistance to pollution and salt.
- H. Plant selection is to have regard for other

environmental stresses including resistance to disease and insects;

- I. Plant selection has been made to be relatively maintenance free;
- J. Plant selection on individual sites should comply to the overall design intent of this document;
- K. The optimal planting environment for street trees is in a location where root development has room to grow, soil compaction and disturbances are minimized, and the trees are back far enough from the curb lines and adjacent traffic so as to give trees a chance to become established;
- L. Balances and tradeoffs will need to be made in some instances, where space within the right-of-way is limited;
- M. Trees should be spaced on average between 7 and 8 metres on centre, as evenly spaced as possible on each individual stretch of Islington Avenue;
- N. Where space is limited, due to existing

conditions, tree spacing can be spaced even tighter to increase the pedestrian canopy, but should not be reduced to less than 6 metres;

- O. All landscape designs that include planting within the Regional right-of-way should be submitted to Regional Forestry staff for their subsequent review and approval;
- P. Recommended street trees include the following:
 - Silver Maple
 - Sugar Maple
 - Red Maple
 - White Ash
 - Green Ash
- Recommended Small Trees include the following:
 - Serviceberry
 - Virginian Witch Hazel
 - Common Ninebark

- Choke Cherry
- Hop Hornbeam
- Recommended shrubs include the following:
 - Serviceberry
 - Sumac
 - Pasture Rose

6.5.3 Streetscape Paving

The use of a special paving pattern along the edge of the road to highlight and coordinate a themed character to Islington Avenue is recommended. This can occur on both public and private property and should be applied as follows:

- A. A standard soldier course banding should occur along the edge of any paved areas either within or adjacent to the sidewalk;
- B. The standard paver should be a 100 x 200 precast paver running perpendicular to the curb, with 200 x 200 precast unit

pavers within the field;

- C. The selected colour should be distinct to this stretch of Islington Avenue and should include a blend of colours which can be used to give variation along the roadway;
- D. For special feature areas, such as pedestrian crossings and intersections, a combination of patterns can be selected and/or mixed, providing the standard soldier course runs through as a constant pattern;
- E. All pavers should be set on a properly prepared sub-base to industry standards or on a concrete base, to avoid differential settlement;
- F. Special interpretive areas such as beside the bridge or along the creek crossing, are areas for public information and sponsorship. In these areas, embedded plaques, stone or metal inserts can be laid into the paving pattern to offer interpretive value (i.e. Hurricane Hazel);
- G. Crosswalks at major intersections should

be accented with unit paver crosswalks to recognize and enhance the pedestrian arrival;

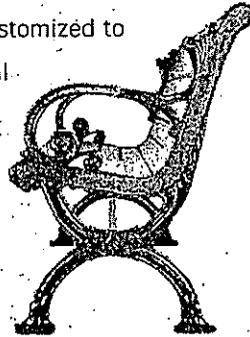
6.5.4 Streetscape Furniture

- A. The fine grain of a well-detailed pedestrian environment along the road edge serves several purposes:
 - B. First, it gives a common theme or identity to the corridor or community and thereby creates a sense of place;
 - C. Second, it allows for special feature areas or "gateways" that create a sense of arrival along the corridor;
 - D. Third, it defines the pedestrian realm and identifies a place for walking along the edge of the roadway;
 - E. Fourth, it creates a language that is repeated along the roadway and establishes a sense of unity;
 - F. All streetscape furniture is subject to the approval of the City of Vaughan.

Guidelines may be updated from time to time.

Benches

- A. The standard bench is to be the comfort bench (716) by Toronto Fabricating or the Trystan Model TCT-1 bench which is used by the City of Vaughan;
- B. This bench has been selected by the City for its durability, attractive design and low maintenance – the same bench could be used, but could be customized to include a special metal frame colour, unique to the Islington Avenue Streetscape;
- C. The bench could also include a customized logo and colour for the "Hamlet";
- D. Where this bench is located in the front yard setback, and is visible from the street, even on private property, it is encouraged that the same bench standard be used.



Trash Containers

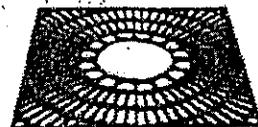
- A. The standard trash container should be the City of Vaughan standard, Trystan TU-3a with flip top lid (colour to be black).

Tree Guards

- A. There may be locations in front yard plazas, in front of churches or convenience retail, where streets trees are located surrounded by hard-surfaced paving. In these locations, standard tree guards are to be TG-7 by trystan (colour to be black).

Tree Grates Urban

- A. There may be locations in front yard plazas, in front of churches or convenience retail, where streets trees are located surrounded by hard-surfaced paving. In these locations, standard tree guards and grates should be used as follows:



- B. Tree grates should be Model Number TP-48, by trystan (colour to be black).

Screenings - to be applied in more urban conditions or in the case of institutional and/or commercial applications.

Bollards

- A. Bollards to be set at the roadside or to prevent vehicular/pedestrian conflicts should be organized to define pedestrian spaces adjacent to the sidewalk Bollards



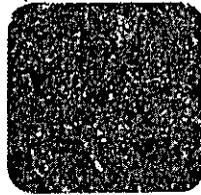
to be type ---- by trystan (colour to be black).

Pedestrian Scale Lighting

- A. The lighting that is currently being implemented along the Islington Avenue corridor, as part of the improvements, is a wooden pole with a cobra-head lighting fixture;

B. At a pedestrian scale, lighting fixtures should be implemented to improve the pedestrian scale environment, between the existing wooden poles. This will result in a safer and more well defined pedestrian zone beneath the tree canopy;

C. All future lighting poles should be spun concrete standards as supplied by King Luminaire, with a dark "Eclipse S-11" polished finish;



D. All poles should have two banner arms aligned opposite one another, perpendicular to the traffic flow;

E. Pedestrian scale poles should be 15 foot poles, with a top mounted light as per City of Vaughan standards.

Community Logo

A. The Islington Avenue ratepayers group should hold a Design Competition to design a Community Logo which repre-

sents the character, special attributes and the identity of the area. The logo should be reproducible in black and white and colour, should be simple and distinct and should be recognizable at a distance;

B. This logo should be used on banners, signage and in special paving;

C. All street furniture should be adorned with the logo.