

**BASS PRO MILLS DRIVE, FROM HIGHWAY 400 TO WESTON ROAD MUNICIPAL CLASS  
ENVIRONMENTAL ASSESSMENT**

Appendix O Consultation

## **Appendix O.3 PIC SUMMARY REPORTS**





**Online Public Information Centre  
#1 Summary Report**

Bass Pro Mills Drive MCEA, from  
Highway 400 to Weston Road

160540006

April 12, 2021

Prepared for:

City of Vaughan

Prepared by:

Stantec Consulting Ltd.



## ONLINE PUBLIC INFORMATION CENTRE #1 SUMMARY REPORT

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Prepared by J. Robinson  
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**Jenn Robinson**

Approved by D. Addley  
(signature)

**Diana Addley**



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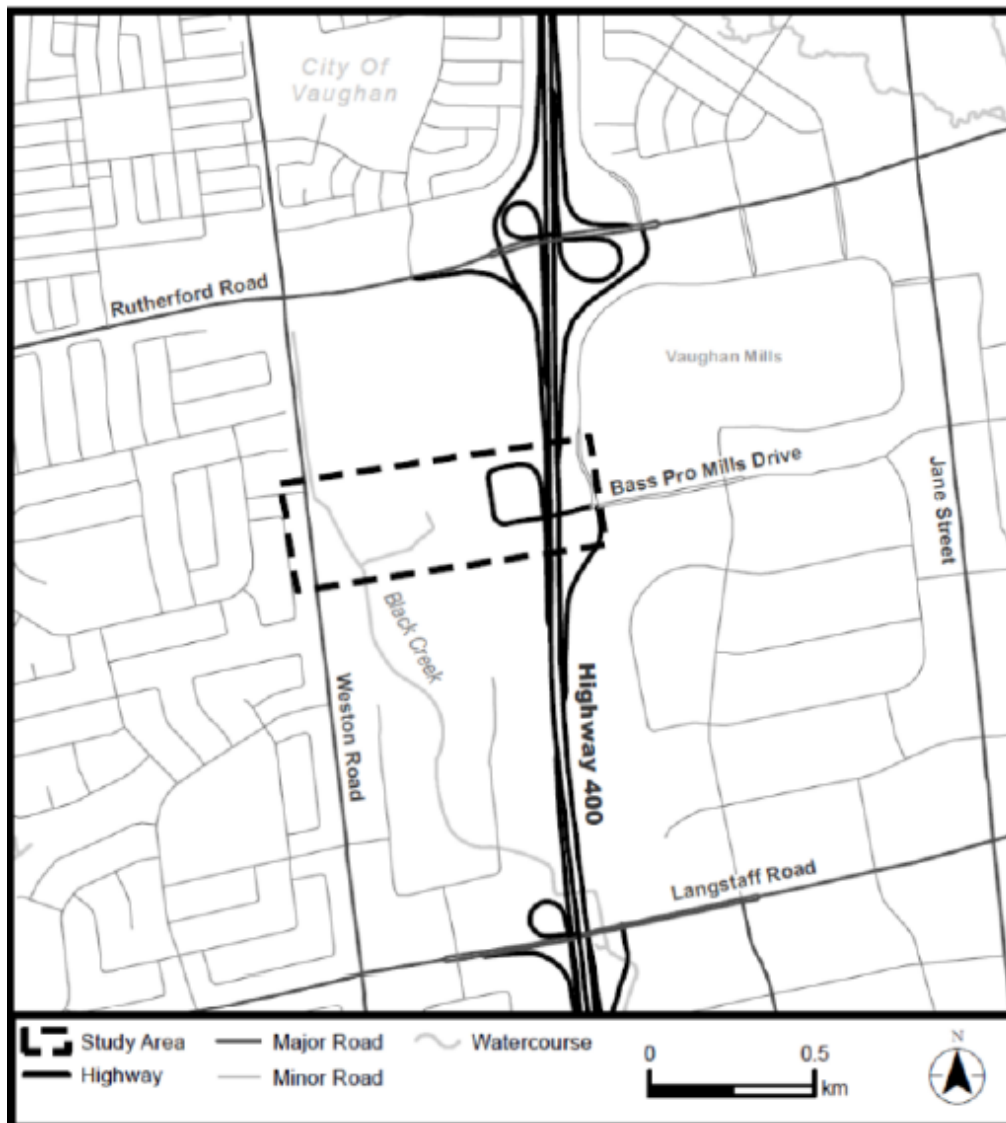


## 1.0 INTRODUCTION

Public Information Centre (PIC) 1 was held from December 3, 2020 to January 8, 2021 to present and solicit feedback on the study background, problems and opportunities, evaluation of alternative solutions, the recommended solution and next steps in the study process. Due to COVID-19 and associated physical distancing requirements, PIC 1 was hosted online via the study website ([Vaughan.ca/BassProMillsEA](http://Vaughan.ca/BassProMillsEA)).

The study area for this is generally situated between Highway 400 and Weston Road, within the City of Vaughan, as shown on the Key Plan in **Figure 1** below:

**Figure 1: Key Plan**



# ONLINE PUBLIC INFORMATION CENTRE #1 SUMMARY REPORT

## 2.0 PURPOSE

Online PIC 1 forms part of the overall consultation plan for the Bass Pro Mills Drive Municipal Class Environmental Assessment (Class EA) and was designed to inform and engage City residents and other stakeholders on the study background, problems and opportunities, evaluation of alternative solutions and associated evaluation criteria, the recommended solution, and next steps in the study process.

## 3.0 LOCATION, DATE AND TIME

The Online PIC 1 was hosted on the study website ([Vaughan.ca/BassProMillsEA](http://Vaughan.ca/BassProMillsEA)) from December 3, 2020 through January 8, 2021. A recorded presentation and survey were available for online participants to listen to and/or read study information, as well as offer feedback and ask questions through the online survey, or by contacting the study team directly via telephone or email.

## 4.0 NOTIFICATION

The Notice of Online PIC 1 provided information about the PIC, including the date that the materials and online survey would become available for review online, where to access the materials and online survey, and a requested end date to provide feedback to the study team.

The City of Vaughan published a Public Service Announcement regard the Bass Pro Mills EA study to their social media platforms on December 3, 2020, as well as a News Release on December 12, 2020 which encouraged the public to provide their input for various City-building initiatives including the Bass Pro Mills EA study. Both publications included thought provoking questions such as “What should be considered for the proposed extension of Bass Pro Mills Drive? How can the road become more pedestrian-oriented? What about features to accommodate cyclists?” and provided links to the study website and Online PIC, noting that the deadline to participate was January 8, 2021.

The Notice of Online PIC 1 was distributed to the public, agencies, utilities, stakeholders and First Nation communities through the methods outlined below in **Table 1**.

**Table 1: Notification Distribution**

Method of Distribution	Date of Distribution
Posted in the Thornhill Liberal local newspaper	November 19, 2020
	December 3, 2020
	December 17, 2020
Posted in the Vaughan Citizen local newspaper	November 19, 2020
	December 3, 2020
	December 17, 2020



## ONLINE PUBLIC INFORMATION CENTRE #1 SUMMARY REPORT

Hardcopies mailed via Canada Post Ad Mail to 3,730 residents within a 200m radius of the study area.	November 24, 2020
Hardcopies mailed directly via Canada Post to residents backing directly onto Weston Road (from Astona Boulevard to 500m south of Astona Boulevard), external agencies, as well as members of the public who expressed an interest in the study, where addresses were available.	November 24, 2020
Hardcopies mailed directly via Canada Post to First Nation communities	November 26, 2020
Emailed electronic copy to the contact list	November 26, 2020
Emailed electronic copy of notice, along with a Public Service Announcement, to the Technical Advisory Committee (TAC) and Stakeholder Group	December 3, 2020

Copies of the Notice of Online PIC, Public Service Announcement and News Release are included within **Appendix A**.

In addition to the above, the City of Vaughan launched online PIC 1 notifications through their social media accounts (i.e., Facebook, Instagram and Twitter), to help encourage followers and members of the public to access the study website and review the online PIC 1 materials. The social media 'pushes' included a direct link to the study website. The dates social media 'pushes' were released are summarized as follows:

Date of Social Media 'Push'
December 4, 2020
December 7, 2020
December 15, 2020
December 20, 2020
December 23, 2020
December 26, 2020
December 30, 2020
January 3, 2021
January 7, 2021

A copy of the social pushes can be found within **Appendix A**.



## 5.0 REFERENCE MATERIALS AND HANDOUTS

The following City of Vaughan Bass Pro Mills Extension Class EA displays were presented as part of online PIC 1:

- Title Page
- PIC 1 Objectives
- Study Overview
- Vaughan Mills Centre Secondary Plan
- Background Studies
- Municipal Class Environmental Process
- Transportation Analysis Study Areas
- Existing Transportation Network (1)
- Existing Transportation Network (2)
- Existing Transportation Network (3)
- Existing vs. Future Baseline Analysis (1)
- Existing vs. Future Baseline Analysis (2)
- Existing vs. Future Baseline Analysis (3)
- Problems and Opportunities
- Technical and Environmental Studies
- Existing Area Conditions (1)
- Existing Area Conditions (2)
- Evaluation Process
- Evaluation Criteria
- Preliminary Evaluation of Alternative Solutions - Summary
- Recommended Solution
- Evaluation Approach – Recommended Design
- Potential Cross-Section
- Next Steps

A copy of the online PIC 1 displays and accompanying script are included in **Appendix B**.

## 6.0 FORMAT

In light of COVID-19 and associated physical distancing requirements, the PIC was held online via the study website and included a pre-recorded presentation and an online survey through which to provide feedback. A summary of the online survey questions, and associated feedback received is provided in **Section 8.0**.





## ONLINE PUBLIC INFORMATION CENTRE #1 SUMMARY REPORT

The Articulate Storyline presentation platform was used to encourage interaction throughout the presentation, allowing users to pause the presentation, or fast forward to sections of the presentation that interested them most and/or rewind to review information in more detail. The text of each narrated presentation slide was offered as part of the online PIC to provide both a visual and audio experience, and to help to ensure that accessibility needs were accommodated.

All attendees were encouraged to provide their feedback by January 8, 2021. As part of the online PIC review and comment period, members of the study team were available to answer questions and/or respond to concerns submitted through the comment form provided on the study website, as well as via mail, phone, and email.

The information provided during online PIC 1, including the displays and narration are provided within **Appendix B**, and a copy of the online survey can be reviewed within **Appendix D**.

### 7.0 PARTICIPATION

Statistics were gathered during the PIC period (i.e., December 3, 2020 to January 8, 2021) to determine the number of viewers of the online presentation. The website visit statistics were broken down into the following two categories:

- 1) **Unique Visitors** – the total number of people that visited the site. The same person visiting the site multiple times during the PIC time period is only counted once.
- 2) **Number of Visits** – the total number of visits by browsing session. If a visitor viewed another page on the site within 30 minutes of their last pageview, it is counted as the same visit. If a visitor returns to the study website 30 minutes after their last pageview, it is counted as a separate visit.

The data gathered for each category is illustrated below within **Table 2** and **Table 3**. While a daily value can be determined for the number of visits, the unique visitor statistic is only available on a monthly basis. However, based on the comparison of data collected for these two categories from December 2020, the unique visitor statistic is on average 22% of the number of visits. Thus, it can be approximated that there



## ONLINE PUBLIC INFORMATION CENTRE #1 SUMMARY REPORT

were 325 unique visitors from December 3 to December 31, 2020 and 146 unique visitors from January 1 to January 8, 2021.

**Table 2: Number of Unique Visitors to the Study Website**

Unique Visitors	
December 2020	325 visitors
January 2021	146 visitors
<b>Total:</b>	<b>471 visitors</b>

**Table 3: Number of Visits to the Study Website**

Number of Visits	
December 3, 2020 to December 31, 2020	1475 visits
January 1, 2021 to January 8, 2021	666 visits
<b>Total:</b>	<b>2141 visits</b>

A copy of the raw statistics from the cPanel platform can be reviewed within **Appendix C**.

## 8.0 FEEDBACK

All online PIC 1 participants were encouraged to provide feedback through a link to the online survey offered at the end of the PIC presentation. In addition, the study team contact information was provided as part of the online PIC 1 notice, the study website and the online presentation.

As part of the online survey, participants were asked to answer a series of questions and share their thoughts in relation to their experience with travel in the vicinity of the study area, including greatest difficulties when driving, walking, cycling and taking public transportation within the study area; frequency of travel and frequency of public transportation use; and what community improvements they would like to see. Participants were also encouraged to submit additional comments via the online survey, or by emailing the study team directly. In total, 101 survey responses, 2 telephone calls, 12 emails from the general public and 4 emails from the stakeholder group were received following the notification of online PIC 1 and during the online PIC comment period. Additionally, through the City's social media pushes, 17 comments were received via Instagram and 2 comments were received via Facebook.

Based on the online survey, telephone conversations, and email responses, the following key comment themes were noted:



## ONLINE PUBLIC INFORMATION CENTRE #1 SUMMARY REPORT

- Concern for increased traffic congestion and a lack of alternative routes within the study area.
- Concern that an extension of Bass Pro Mills Drive will encourage east-west traffic through the Weston Downs community, or increase traffic on an already congested Weston Road.
- Suggestions that there is a greater need for alleviation of north-south traffic rather than east-west traffic, primarily on Weston Road.
- Need for pedestrian and cyclist friendly routes within the study area (i.e., through separated bike lanes and the implementation of sidewalks, multi-use paths and wider boulevards)
- Concern regarding the impact to the environment, wildlife, and noise/air pollution.
- Desire for wider boulevards with trees/streetscaping.
- Roundabouts on the potential Bass Pro Mills extension may alleviate congestion that would otherwise build up at traffic lights.
- Implementation of a Highway 400 northbound ramp could reduce traffic congestion.

Based on the responses received as part of the online survey, it was noted that while 86% of respondents use public transportation less than once per month, 37% of respondents noted that there is a lack of public transportation options within the study area.

A copy of the online comment form, as well as all comments and survey responses received is included within **Appendix E**. It should be noted that all names and addresses from respondents were included on the study mailing list.

## 9.0 NEXT STEPS

All comments received as part of Online PIC 1 have been reviewed and considered by members of the study team and will continue to be considered as the Class EA study progresses.

Next steps will include the evaluation of alternative alignments, cross-sections and identifying a recommended design. A second PIC will be held to present and solicit feedback on the findings, the recommended design and potential impacts and mitigation measures.

Consultation will be ongoing, and all interested parties will be notified of study updates, including PIC 2.



# Appendix A

## Notification Materials



# NOTICE OF ONLINE PUBLIC INFORMATION CENTRE 1

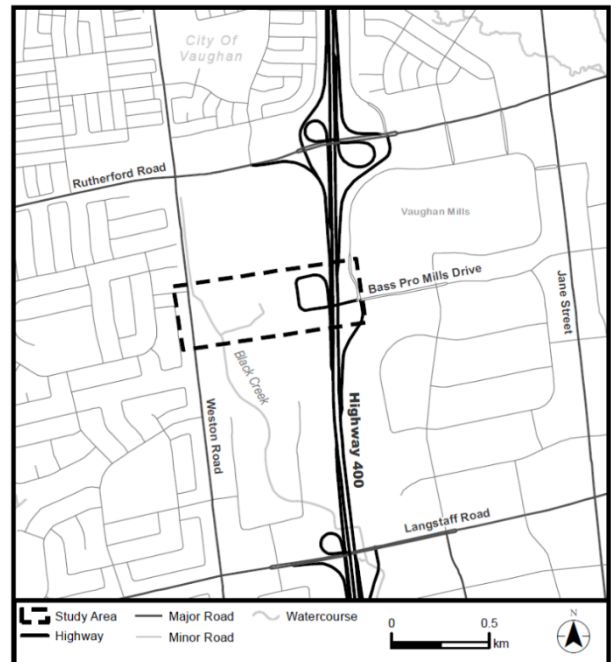
## MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT STUDY

### Bass Pro Mills Drive, from Highway 400 to Weston Road

#### THE STUDY

The City of Vaughan (City) has initiated a Municipal Class Environmental Assessment (Class EA) study to assess the need to extend Bass Pro Mills Drive, from Highway 400 to Weston Road, as recommended in the Vaughan Mills Centre Secondary Plan (2014). These recommendations were made to:

- provide a new east-west multi-modal connection between Highway 400 and Weston Road;
- help alleviate traffic congestion along Rutherford Road;
- support future growth and development within the plan area;
- create new multi-modal transportation connections; and,
- develop a safe and comfortable environment for active transportation users.



#### THE PROCESS

This Class EA study will define the problems and opportunities, identify and evaluate alternative solutions and designs, and determine a preferred design for the proposed improvements. The study will assess the proposed improvements in consideration of potential impacts to transportation service and the natural, socio-economic and cultural environments.

The study is being completed in accordance with the planning and design process for Schedule 'C' projects, as outlined in the Municipal Engineers Association (MEA) Municipal Class EA guidelines (October 2000, amended 2007, 2011 and 2015), which is approved under the Ontario Environmental Assessment Act. Upon study completion, an Environmental Study Report (ESR) will be made available for public review and comment. The ESR will document the study, consultation process and decision-making rationale.

#### ONLINE PUBLIC INFORMATION CENTRE 1

The City of Vaughan values the voice of its citizens and is dedicated to promoting a dialogue with the community that is open, transparent, accessible, and inclusive. Consultation with citizens is a key component of the study and as part of this Class EA process.

The first of two Public Information Centres (PICs) has been arranged to present and solicit feedback on the study background, evaluation of alternative solutions and associated criteria, the recommended solution, and the next steps in the study process. In light of COVID-19 and associated physical distancing requirements, PIC 1 will be hosted online. **PIC 1 will be available for your review on the project website at [Vaughan.ca/BassProMillsEA](http://Vaughan.ca/BassProMillsEA) on December 3, 2020.** A recorded presentation will be available as part of the online PIC, as well as an online survey, until January 8, 2021.

#### CONTACT US

To join the study mailing list or to share comments, please contact:

**Hilda Esedebe, P.Eng.**  
**City of Vaughan**  
**Project Manager**  
2141 Major Mackenzie Dr.  
Vaughan, ON L6A 1T1  
T: 905-832-2281, ext. 8484  
E: [Hilda.Esedebe@vaughan.ca](mailto:Hilda.Esedebe@vaughan.ca)

**Diana Addley**  
**Stantec Consulting Ltd.**  
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E: [Diana.Addley@stantec.com](mailto:Diana.Addley@stantec.com)

Personal information on this form is collected under the authority of the Municipal Act, 2001 and will be used for the purpose of administering the Bass Pro Mills Environmental Assessment Study (between Highway 400 and Weston Road). Questions about this collection can be directed to the Manager, Transportation Planning, City of Vaughan, 2141 Major Mackenzie Drive, Vaughan, ON L6A 1T1, 905-832-8585.



Nick Kozak/Torstar

Toronto Star publisher Jordan Bitove, chair of Torstar Paul Rivett, president and CEO of Toys R Us Canada Vic Bertrand and Geoffrey the Giraffe with the first package to be delivered by Metroland Parcel Services, Saturday. The new venture provides parcel delivery for retailers and distributors, including Toys R Us.

# METROLAND LAUNCHES NEW PARCEL DELIVERY SERVICE

## LOCAL NEWSPAPER COMPANY CAPITALIZING ON DISTRIBUTION NETWORK

CHRIS TRABER

Metroland Media has launched an innovative parcel delivery service heralding a new chapter in its venerable 150-year history of serving Ontarians.

A division of Torstar Corporation, Metroland publishes more than 70 community and daily newspapers delivered to 147,000 Ontario postal codes by a network of 15,000 delivery contractors.

Metroland's facility hubs, distribution networks and expertise, in addition to the upward trajectory of e-commerce, ideally positions the organization to successfully extend its services into parcel delivery, Torstar Group of Companies head of parcel services Mike Banville said.

"Our research and clients tell us e-commerce channel growth has exploded and is largely expected to sustain post-COVID," he said. "Parcel carriers are struggling to meet service level commitments. Some carriers

have inconvenient hard stop deadlines for holiday deliveries.

"Almost 50 per cent of consumers will shop online this year to avoid large crowds and retailers want more delivery options.

"Our new solution is designed to deliver the high value premium final mile parcel delivery experience in Ontario, right up to Christmas Eve and beyond."

Metroland will offer next-day express and standard two- to three-day delivery at competitive prices to retailers and distributors and includes delivery guarantees, real-time parcel tracking and photo delivery confirmation. The new service also provides seamless technology integration into client platforms.

For information, visit [metrolandparcel-services.ca](http://metrolandparcel-services.ca) or email [mbanville@metroland.com](mailto:mbanville@metroland.com).

**Michael's**  
MAKE CREATIVITY HAPPEN  
**CORRECTION NOTICE**

In the circular beginning Friday, November 06, 2020, the Christmas Candles & Home Fragrance on page 2 printed incorrectly with a promotional price it should be at a reg. price. The correct reg. price is 6.99 – 44.99.

We apologize for any inconvenience this may have caused.



## NOTICE OF ONLINE PUBLIC INFORMATION CENTRE 1 MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT STUDY Bass Pro Mills Drive, from Highway 400 to Weston Road

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## BUSINESS



Nick Kozak/Torstar

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SPONSORED CONTENT

# MY BROTHER THREW IT ALL AWAY

*The Wills Lawyers*



Father & Daughter Team  
Les & Michelle Kotzer

A caller to a radio show, one of two brothers, told us the following story, voicing the hope that he was not going to sound like a selfish person.

He said that he was the saver in the family and his brother had always been the spender. In the caller's own words, "nothing was too good for my brother. He always had a live-for-today attitude." When their dad passed away, the two brothers shared his estate, worth well over three million dollars. The caller used part of his share of the inheritance to pay off his mortgage. He invested the balance for his retirement.

On the other hand, the caller said that his brother sold the house he was living in and moved into a large, expensive home. Less than six months had passed since their father died, when the caller visited his brother's new home. In the driveway he could see his brother's two brand new cars. His brother bragged to him that he was about to buy a new condominium in Florida, and that his wife had booked an appointment to get some very expensive cosmetic surgery. The caller's tone on the air sounded somewhat sarcastic at this point, when he said, "like they really needed all of this."

If that was bad enough, his brother and sister-in-law also bragged to him about the investments they had made. They laughed at the caller for being so conservative in the investments he chose. His brother and sister had placed the majority of their share of the inheritance in high-risk investments. They justified their choice by telling the caller that they would be very rich "once things go our way." They added their own advice to this, saying to the caller, in their own words, "Life is full of risks. Your way of investing is too timid. What are you afraid of?"

The caller said that he had vivid memories of his brother and sister-in-law talking to him this way, because over the next year, all he seemed to hear from them were their lectures on how to get rich. "We're making some real money from what Dad left us," they would repeat over and over.

The good times did not last very much longer, though, for the caller's brother and sister-in-law. One night, the caller got a desperate call from his brother, begging for money. His brother complained that most of his investments were lost in the market downturn, and now he had no money to pay the mortgage on his new house. He had no money left to maintain his Florida condominium. His car payments and his other bills were overwhelming him. His salary was not enough to cover all of these expenses. He pleaded with our caller for help. In the words quoted by the caller, his brother said, "The money you inherited came from OUR father and he would have wanted you to help me when I needed it. A brother should help a brother when he is down."

The caller said that his brother's plea for help tore him apart. On one hand, he had always been there for his brother, but on the other hand, he felt that his brother's spending habits had ruined him. Our caller believed very strongly that once he opened the door and bailed out his brother, the rest of their parents' life savings would just flow into a bottomless pit.

After wrestling with this troubling decision, the caller said that this time, he had to say "no" to his brother because he had blown his half of the inheritance. The caller spoke of how this inherited money represented years and years of Dad's hard work, and his brother had just thrown the money away on extravagance in less than two years.

The caller's last words were, "I feel really bad for my brother, and all of this really bothers me. But, I know Dad would feel that I did the right thing."

### **Making a Will With Our Law Firm During These Challenging Times**

Firstly, we offer a free telephone will consultation to answer some questions you may have.

If you wish to have us prepare a will for you, we have developed a three step process:

1. We will arrange a **telephone appointment** for us to discuss your wishes and take instructions for a will and powers of attorney.
2. We will email you a draft will and powers of attorney, which we will discuss together during a **second telephone appointment**.
3. If you approve of the draft will and powers of attorney after we have gone through them with you over the phone, then we will arrange a signing appointment which will take place outside on our **covered office porch**. We are located in a house at 7951 Yonge Street in Thornhill (Yonge St. south of Highway 7). You do not need to enter into our offices.

To contact Michelle please call her at (905) 881-1500 ext. 22 or email her at [mkotzer@fishlaw.ca](mailto:mkotzer@fishlaw.ca). To contact Les please call (905) 881-1500 ext. 19 or contact him through his website [leskotzer.com](http://leskotzer.com). You can also visit our law firm's website [willappointment.com](http://willappointment.com).



## NOTICE OF ONLINE PUBLIC INFORMATION CENTRE 1 MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT STUDY Bass Pro Mills Drive, from Highway 400 to Weston Road

### THE STUDY

The City of Vaughan (City) has initiated a Municipal Class Environmental Assessment (Class EA) study to assess the need to extend Bass Pro Mills Drive, from Highway 400 to Weston Road, as recommended in the Vaughan Mills Centre Secondary Plan (2014). These recommendations were made to:

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### THE PROCESS

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### CONTACT US

To join the study mailing list or to share comments, please contact:

**Hilda Esedebe, P.Eng.**

**City of Vaughan**

**Project Manager**

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**Diana Addley**

**Stantec Consulting Ltd.**

**Senior Environmental Planner**

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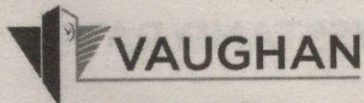
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This Notice was first issued November 19, 2020.





# NOTICE OF ONLINE PUBLIC INFORMATION CENTRE 1

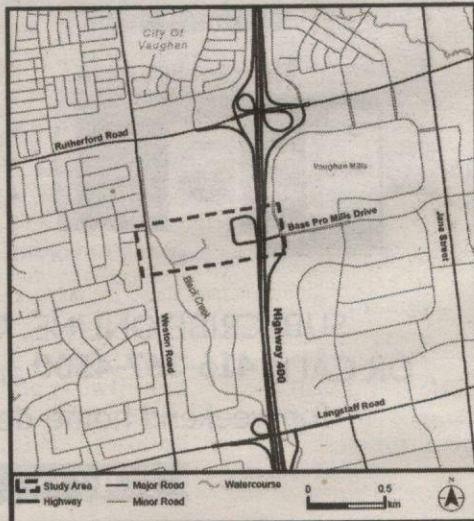
## MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT STUDY

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#### THE PROCESS

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# WATER IS SAFE

Delivering clean, safe drinking water requires constant care and monitoring. From protecting our water sources to meeting regulated treatment processes, York Region proudly delivers 325 million litres of clean, safe drinking water to 1.2 million residents every day.

Learn how we do it at [york.ca/wateris](http://york.ca/wateris)



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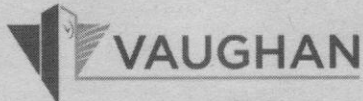
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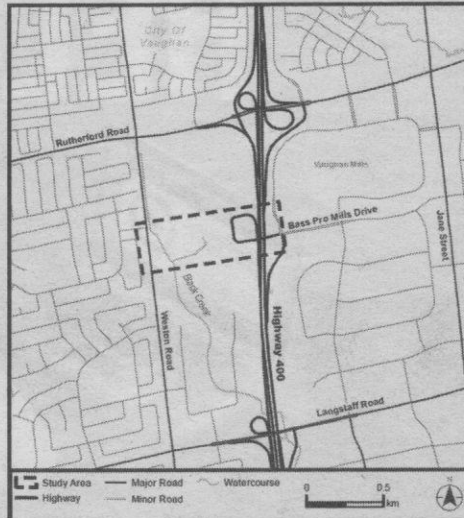


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# Public Service Announcement

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## Shape the future of Bass Pro Mills Drive – join the conversation

*Provide feedback and help inform the City's proposed extension plan*

**VAUGHAN, ON (Dec. 3, 2020)** – What should be considered for the proposed extension of Bass Pro Mills Drive? How can the road become more pedestrian-oriented? What about features to accommodate cyclists? The City of Vaughan wants to hear what citizens think about the future of Bass Pro Mills Drive.

To clearly understand and respond to the needs of the community, while effectively developing a plan for the future of the area, the City is launching an online Public Information Centre which includes a self-guided presentation and survey as part of the Bass Pro Mills Extension Municipal Class Environmental Assessment Study. Visit [vaughan.ca/BassProMillsEA](http://vaughan.ca/BassProMillsEA) to participate until Friday, Jan. 8, 2021.

As recommended in the [Vaughan Mills Centre Secondary Plan](#) (PDF), the study will assess the need to extend Bass Pro Mills Drive from Highway 400 to Weston Road. These recommendations were made to:

- provide a new east-west transportation network between Highway 400 and Weston Road, including a new route for York Region Transit.
- help distribute east-west traffic and alleviate congestion along Rutherford Road to the north.
- support future growth and development within the plan area.
- develop a safe and effective environment for active transportation users.

Ultimately, the proposed extension could provide a new major collector roadway that unites Vaughan neighbourhoods from Weston Road to Jane Street, enhance the pedestrian experience and foster accessibility and inclusivity in the area.

As part of the process, the Bass Pro Mills Extension Municipal Class Environmental Assessment Study will define problems and opportunities, identify solutions and determine a design for recommended improvements that align with the area's natural, social, economic and cultural environment. Upon study completion, an Environmental Study Report (ESR) will be made available for public review and comment. The ESR will document the study, consultation process and decision-making rationale.

As public engagement is vital to the development and success of this study, the City encourages citizens to join the conversation – virtually. The first of two Online Public Information Centres has been arranged to present and solicit feedback on the study background, evaluation of alternatives solutions and associated criteria, the recommended solution and the next steps in the study process. Participants can learn about the project, share thoughts on current challenges, comment on potential solutions and active transportation opportunities and more.

# Public Service Announcement

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To be a fully connected and integrated community, the City is committed to undertaking initiatives like the proposed Bass Pro Mills Extension to improve the municipal road network, promote the development of transit and increase cycling and pedestrian-friendly infrastructure. This project also supports Vaughan's Transportation and Mobility objective – a priority outlined in the [2018-2022 Term of Council Service Excellence Strategic Plan](#).

To learn more about the Bass Pro Mills Extension Municipal Class Environmental Assessment Study, or to participate in the online Public Information Centre, visit [vaughan.ca/BassProMillsEA](http://vaughan.ca/BassProMillsEA).

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**ABOUT VAUGHAN:** The City of Vaughan is one of Canada's fastest growing cities with a population of more than 335,000. Incorporated in 1991, Vaughan includes the communities of Concord, Kleinburg, Maple, Thornhill and Woodbridge. This culturally diverse municipality is located in the heart of York Region and the Greater Toronto Area.

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[vaughan.ca](http://vaughan.ca)



# News Release

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## Join the conversation – virtually

*City seeking public input on a variety of city-building initiatives throughout Vaughan*

**VAUGHAN, ON (Dec. 20, 2020)** – Vaughan is a city on the move – and despite the global COVID-19 pandemic, city-building has not slowed down. Although the City of Vaughan's physical doors remain closed, key projects and initiatives continue to move forward. As public consultation remains an important and necessary part of the process, Vaughan is committed to providing virtual opportunities for citizens and stakeholders to be engaged, learn about active projects, provide feedback and get involved. Visit [vaughan.ca/HaveYourSay](https://vaughan.ca/HaveYourSay) to explore current engagement opportunities such as online surveys, digital presentations, virtual open houses, online workshops and more. By participating, you can help inform municipal decisions and shape the future of Vaughan.

Citizens are encouraged to join these current conversations – virtually – from the safety and comfort of home.

### **Bass Pro Mills Extension Municipal Class Environmental Assessment Study**

What should be considered for the proposed extension of Bass Pro Mills Drive? How can the road become more pedestrian-oriented? What about features to accommodate cyclists? To clearly understand and respond to the needs of the community, while effectively developing a plan for the future of the area, the City has launched an [online Public Information Centre](#), which includes a self-guided presentation and survey as part of the Bass Pro Mills Extension Municipal Class Environmental Assessment Study. Visit [vaughan.ca/BassProMillsEA](https://vaughan.ca/BassProMillsEA) to participate until Friday, Jan. 8, 2021.

### **VMC Secondary Plan Update**

The Vaughan Metropolitan Centre (VMC) is the largest and most ambitious development project in the City's history – and it is transforming quickly. With rapid growth, the exceeding of residential targets, an expanding workforce and growing visitor and commuter numbers, the City has initiated an update to the existing VMC Secondary Plan. Join the conversation that will help define Vaughan's downtown by participating in an [online survey](#) – share thoughts on travel patterns, space usage, growth concepts and more. The survey is available until Friday, Jan. 22, 2021 at [vaughan.ca/VMCPlan](https://vaughan.ca/VMCPlan).

### **VMC Transportation Master Plan**

With more people living, working and travelling through the VMC, the City recognizes the existing [VMC Transportation Plan](#) needs to be updated. The revised master plan will confirm community transportation needs and support a strategy that focuses on street connectivity, accessibility and support for pedestrians, cyclists, transit users and vehicles well into 2041. Join the conversation and provide input on travel patterns to and around the VMC through the [VMC Secondary Plan Update survey](#). Learn more about the study at [vaughan.ca/VMCTMP](https://vaughan.ca/VMCTMP).

# News Release

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## Ward Boundary Review

The City is undertaking a comprehensive review of the city's five ward boundaries. The objective of the Ward Boundary Review is to assess whether Vaughan's present wards are continuing to provide effective, equitable and democratic representation. Public input is a vital part of this process. There are a number of ways to get involved and join the conversation – including an [online survey](#). Participate until Friday, Jan. 22, 2021 to help define priorities for the City as the review begins. Learn more at [vaughan.ca/WardBoundary](http://vaughan.ca/WardBoundary).

For more information on these projects, or to share your thoughts, visit [vaughan.ca/HaveYourSay](http://vaughan.ca/HaveYourSay). Continue to check this webpage as new public engagement opportunities are frequently posted.

-30-

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## Bass Pro Mills Extension: social media campaign

### Organic promotions –

#### Friday, Dec. 4, 2020

- Join the conversation that is helping to shape the future of Bass Pro Mills Drive. Visit [vaughan.ca/BassProMillsEA](http://vaughan.ca/BassProMillsEA) to watch a presentation, learn more about the extension study and share your feedback through the survey.



#### Monday, Dec. 7, 2020

- The City has launched an online Public Information Centre as part of the Bass Pro Mills Extension study. Feedback received will help inform the study in improving the municipal road network, promoting active transportation & more. Have your say at [vaughan.ca/BassProMillsEA](http://vaughan.ca/BassProMillsEA)



#### Tuesday, Dec. 15, 2020

- Join the conversation that will help define the City's Bass Pro Mills Extension Study. Take the survey at [vaughan.ca/BassProMillsEA](http://vaughan.ca/BassProMillsEA) and share your thoughts on growth, expansion and effective travel within the area until Jan. 8, 2021.



#### Sunday, Dec. 20, 2020



## Bass Pro Mills Extension: social media campaign

- Bass Pro Mills Drive has proposed plans to extend providing a new major collector roadway that will improve traveler safety, enhance the pedestrian experience and foster accessibility and inclusivity through the area. Take the online survey at [vaughan.ca/BassProMillsEA](http://vaughan.ca/BassProMillsEA).



### Wednesday, Dec. 23, 2020

- What should be considered for the extension of Bass Pro Mills Drive? The City wants to hear from you about the recommended solution, next steps in the study process and more by taking the survey at [vaughan.ca/BassProMillsEA](http://vaughan.ca/BassProMillsEA). Survey runs until Jan. 8, 2021.



### Saturday, Dec. 26, 2020

- The proposed Bass Pro Mills Drive extension will provide a new major collector roadway, improve traveler safety, enhance the pedestrian experience and foster accessibility and inclusivity in the area. Complete the survey at [vaughan.ca/BassProMillsEA](http://vaughan.ca/BassProMillsEA) to join the conversation.



### Wednesday, Dec. 30, 2020

- Have an idea about what the City should consider in the extension of Bass Pro Mills Drive? Visit the online Public Information Centre at [vaughan.ca/BassProMillsEA](http://vaughan.ca/BassProMillsEA) to take

## **Bass Pro Mills Extension: social media campaign**

the survey, learn about the project and share thoughts on current challenges and potential solutions.



### **Sunday, Jan. 3, 2021**

- Bass Pro Mills Drive has proposed plans for an extension, and the City wants your feedback. Participate in the survey that will help define problems and opportunities and identify solutions. Share your thoughts until Jan. 8, 2021 at [vaughan.ca/BassProMillsEA](http://vaughan.ca/BassProMillsEA)



### **Thursday, Jan. 7, 2021**

- Public engagement is vital to the development and success of the Bass Pro Mills Extension study. Get involved by sharing thoughts on travel patterns, pedestrian and cyclist safety and more through the online survey: [vaughan.ca/BassProMillsEA](http://vaughan.ca/BassProMillsEA)



# Appendix B

## Online PIC 1 Presentation (Displays and Script)





# Bass Pro Mills Extension

Highway 400 to Weston Road

Schedule C Municipal Class Environmental Assessment

Online Public Information Centre #1

December 3, 2020



# PIC 1 Objectives

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This is the first Public Information Centre (PIC) for the Bass Pro Mills Extension (from Highway 400 to Weston Road) Municipal Class Environmental Assessment (MCEA).

The objectives of this PIC are to:

- Introduce the project and outline the MCEA process that is being followed
- Provide background information on the need for the improvements
- Present the evaluation of alternative solutions and the recommended solution
- Seek input on the existing conditions within the study area (i.e. natural, social, economic and cultural)
- Gather your feedback on this study

# Study Overview

The City of Vaughan is undertaking a Municipal Class Environmental Assessment study for the proposed extension of Bass Pro Mills Drive, from Highway 400 westerly to Weston Road. This extension would provide a new major collector roadway that unites neighbourhoods from Weston Road to Jane Street, redistributes east-west traffic and alleviates congestion on Rutherford Road.

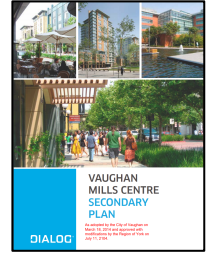
An enhanced boulevard could accommodate new York Region Transit amenities, a pedestrian friendly multi-use trail, as well as on-street cycling facilities.

The proposed extension of Bass Pro Mills Drive is envisaged to support future development in the study area, including the employment and intensification plans developed as part of the Vaughan Mills Centre Secondary Plan (VMCSP).



# Vaughan Mills Centre Secondary Plan

The City of Vaughan adopted the Vaughan Mills Centre Secondary Plan (VMCSP) in 2014 to establish a framework for land use planning in the area and to guide future development planning within the subject lands.



As part of the VMCSP, recommendations for transportation improvements within the study area were developed based on an assessment of:

- Future land use
- Forecasted population and employment growth
- Projected future traffic volumes
- Existing and future roadway operations
- Roadway engineering design criteria
- Technical evaluation of transportation options
- Feedback received from the public, agencies and landowners/developers

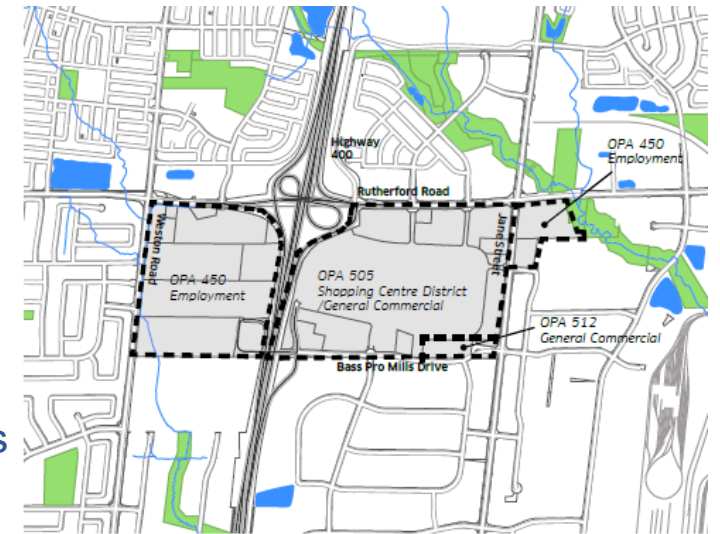


FIGURE 3: OPA 600 DESIGNATIONS AND RELATED OPAS

The extension of Bass Pro Mills Drive was recommended in the VMCSP, consisting of a wide, visually appealing streetscape with a multi-use trail and on-street cycling facilities along the new road right-of-way.

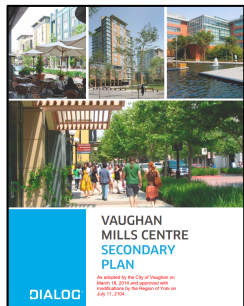
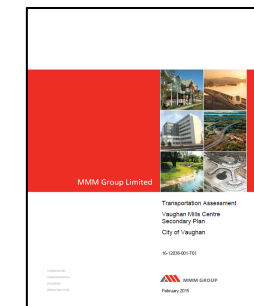
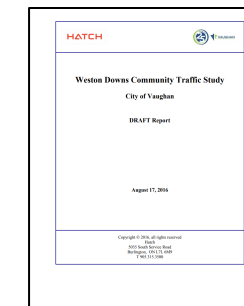
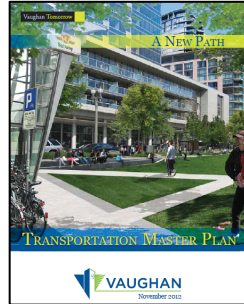
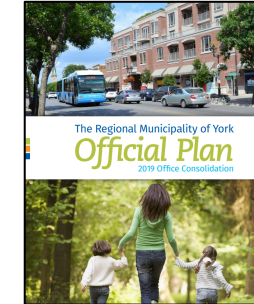
# Background Studies

The following documents were prepared to provide a framework for land use and transportation planning within the City and Region and are being considered as part of this study:

- [2020 Provincial Policy Statement](#)
- [2019 Growth Plan for the Greater Golden Horseshoe](#)
- [The City of Vaughan Transportation Master Plan \(2012 – currently being updated\)](#)
- [The York Region Official Plan \(2010\)](#)
- [The City of Vaughan Official Plan \(2010\)](#)
- [The York Region Transportation Master Plan \(2016\)](#)

Several background studies from adjacent areas were considered for this study, including:

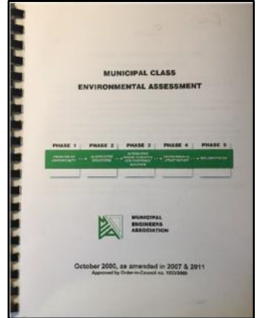
- [Weston Downs Traffic Study](#)
- [Vaughan Mills Centre Secondary Plan](#)
- [Vaughan Mills Centre Public Realm Strategy and Streetscape Master Plan](#)
- [Western Vaughan Individual Environmental Assessment](#)
- [Langstaff Road Environmental Assessment](#)
- [Weston Road Design and Construction](#)



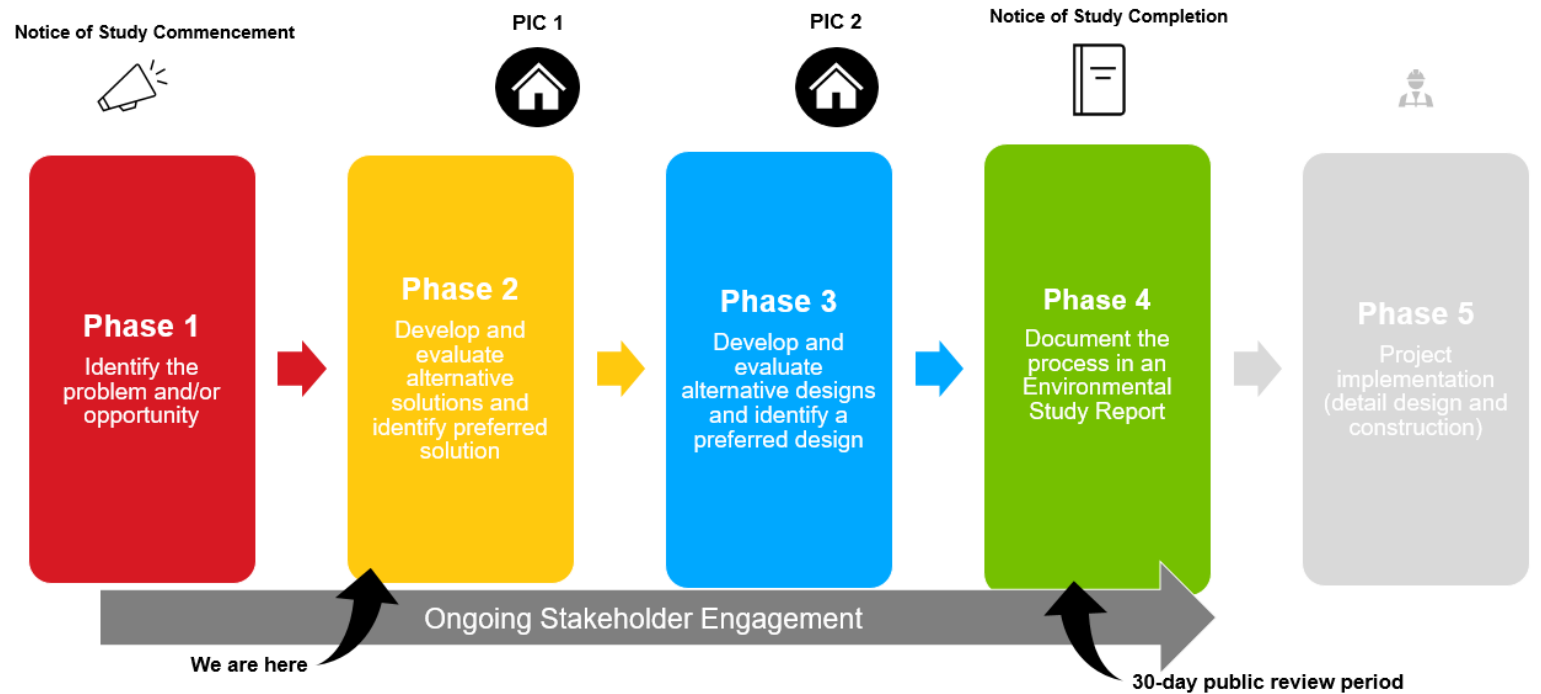


# Municipal Class Environmental Process

Municipal planning for infrastructure improvements must be completed in accordance with the Municipal Engineers Association’s Municipal Class Environmental Assessment (MCEA) document (October 2000, as amended in 2015). This is an approved process under the Environmental Assessment Act.



The proposed extension of Bass Pro Mills Drive classifies as a Schedule ‘C’ project. These projects are required to follow Phases 1 through 4 of



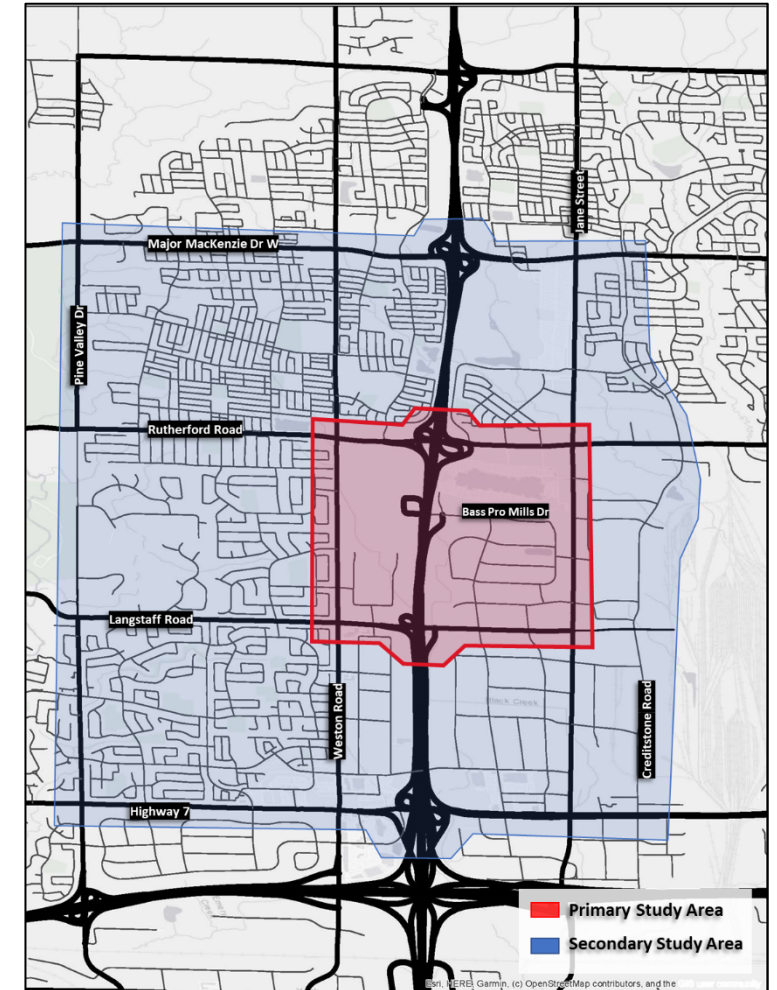
# Transportation Analysis Study Areas

## Primary Study Area

- This is where major changes in traffic will be experienced as a result of the Bass Pro Mills extension

## Secondary Study Area

- This is where minor changes in traffic may be experienced as a result of changes in the Primary Study Area



# Existing Transportation Network

## Road Network

The road network includes the following major roads:

- Rutherford Road
- Jane Street
- Bass Pro Mills Drive
- Langstaff Road
- Weston Road
- Highway 400

The study area is car-oriented and auto use makes up over 90% of trips to/from Vaughan Mills Mall and over 85% of total trips in the City of Vaughan.

	Transit	Cycling	Auto	Walk	Other
<b>AM Peak Period</b>					
<b>Vaughan Mills Mall</b>	5%	2%	91%	2%	0%
<b>City of Vaughan</b>	5%	0%	85%	7%	2%
<b>PM Peak Period</b>					
<b>Vaughan Mills Mall</b>	6%	0%	94%	0%	0%
<b>City of Vaughan</b>	9%	0%	89%	1%	0%

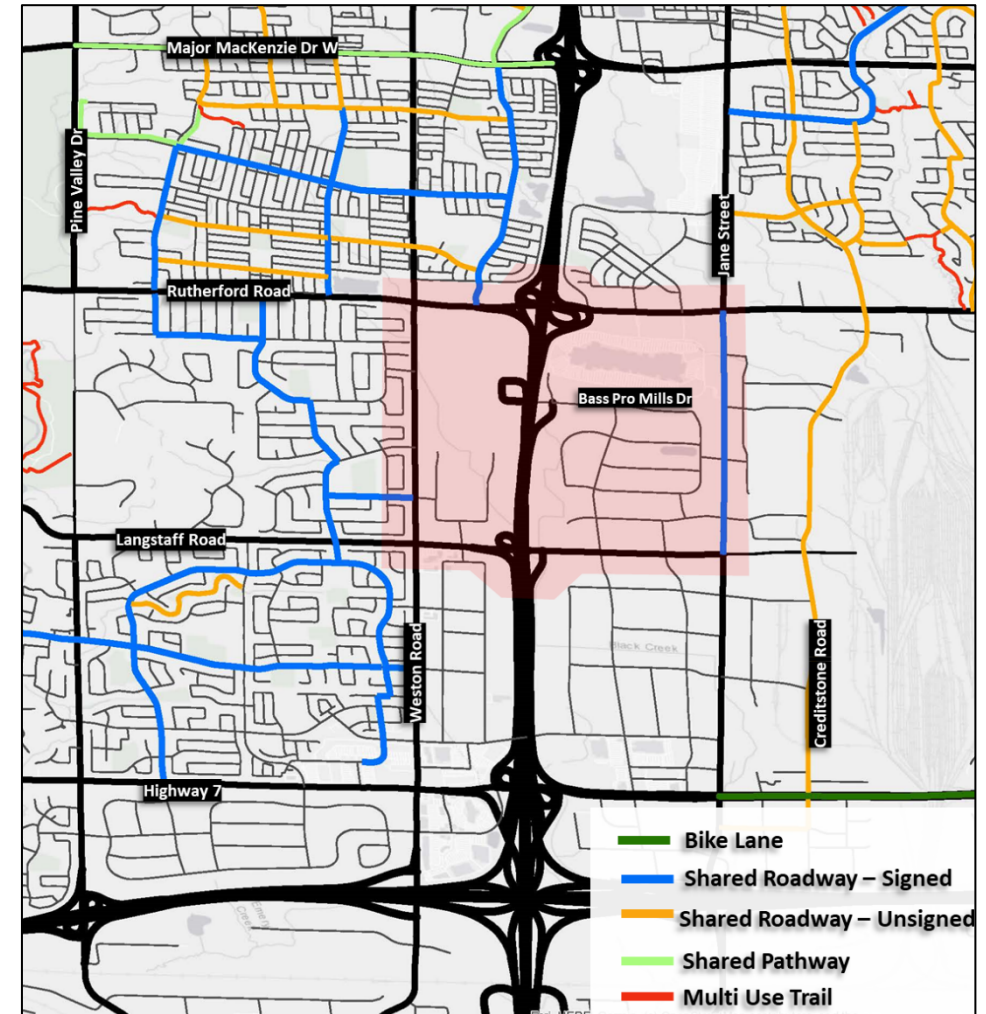
# Existing Transportation Network

## Active Transportation Network

The active transportation network includes:

- Limited cycling or multi-use paths;
- One shared roadway with sharrows on Jane Street and an unsigned bike route on Springdale Road; and
- Sidewalks throughout.

Only 4% active transportation users travel to/from Vaughan Mills Mall during the AM peak and 0% during the PM peak.



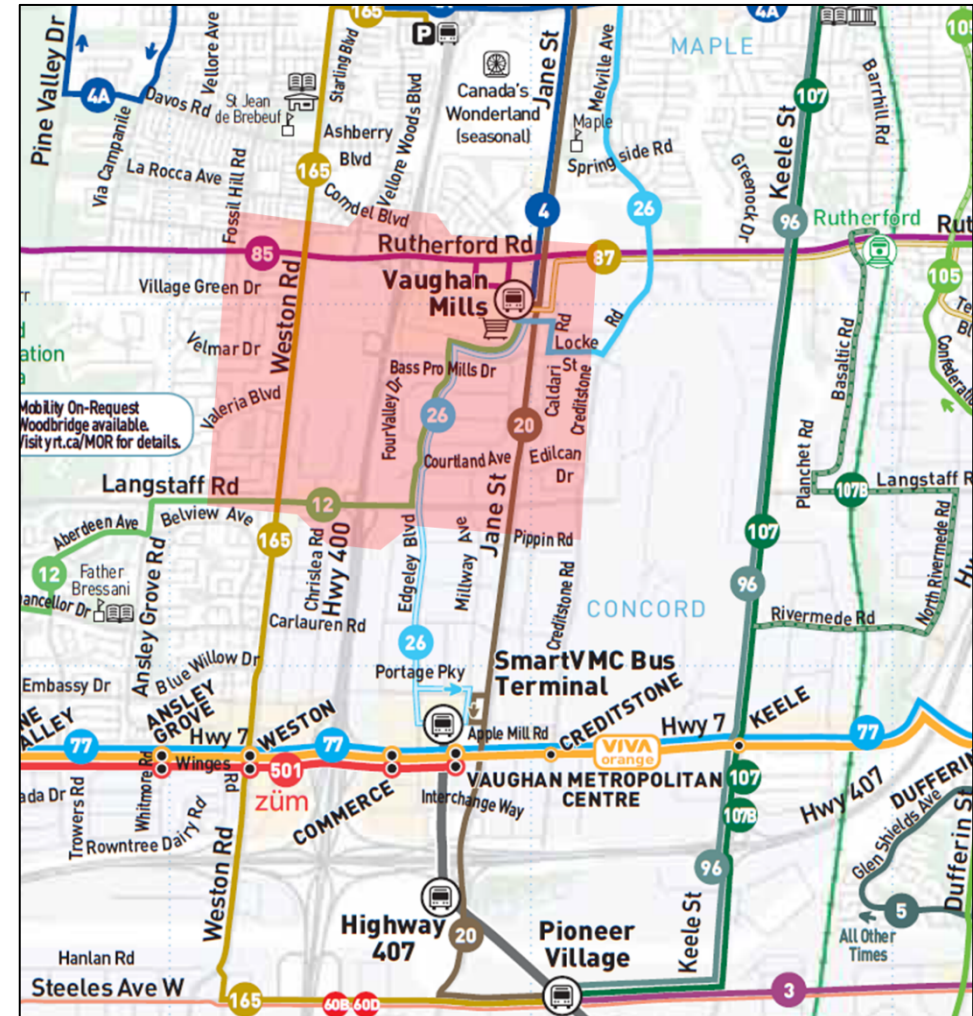
# Existing Transportation Network

## Transit Network

The transit network includes:

- 8 York Region Transit (YRT) routes;
- Connections to TTC, Mississauga Transit, Brampton Transit and GO Transit in the surrounding area; and
- Frequent service on Route 20 Jane (every 15 minutes or better).

Route Number/ Name	Frequency Weekday AM (minutes)	Frequency Weekday PM (minutes)
4 - Major Mackenzie	30	30
12 - Pine Valley	30	30
20 - Jane	12	13
21 - Vellore	60	60
26 - Maple	27	30
85 - Rutherford	27	24
87 - Autumn Hill	35	40
165 - Weston	30	35



# Existing vs. Future Baseline Analysis

## Preliminary Transportation Analysis – Network Traffic Operations

Based on transportation modelling of future traffic conditions (horizon years 2031 and 2041), there will be a significant increase in average delays and travel times within the area, when compared to existing conditions.

AM Peak Hour	Existing 2020	Future Baseline 2031	Future Baseline 2041
Delay (s/km)	51.1	158.8	140.13
Total Travel Time (veh-h)	2,411	4,448	4,902
Speed (km/h)	48.9	31.8	29.7

PM Peak Hour	Existing 2020	Future Baseline 2031	Future Baseline 2041
Delay (s/km)	79.4	147.1	153.4
Total Travel Time (veh-h)	2,820	4,940	5,136
Speed (km/h)	45.73	37.6	36.5

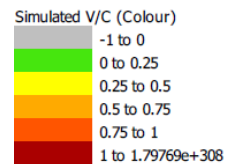
# Existing vs. Future Baseline Analysis

Based on preliminary review of the existing (2020) AM peak travel times and future 2031 AM peak travel times, a significant increase in study area traffic volumes is expected by 2031.

2020 Volumes - AM Peak Travel Times



2031 Volumes - AM Peak Travel Times



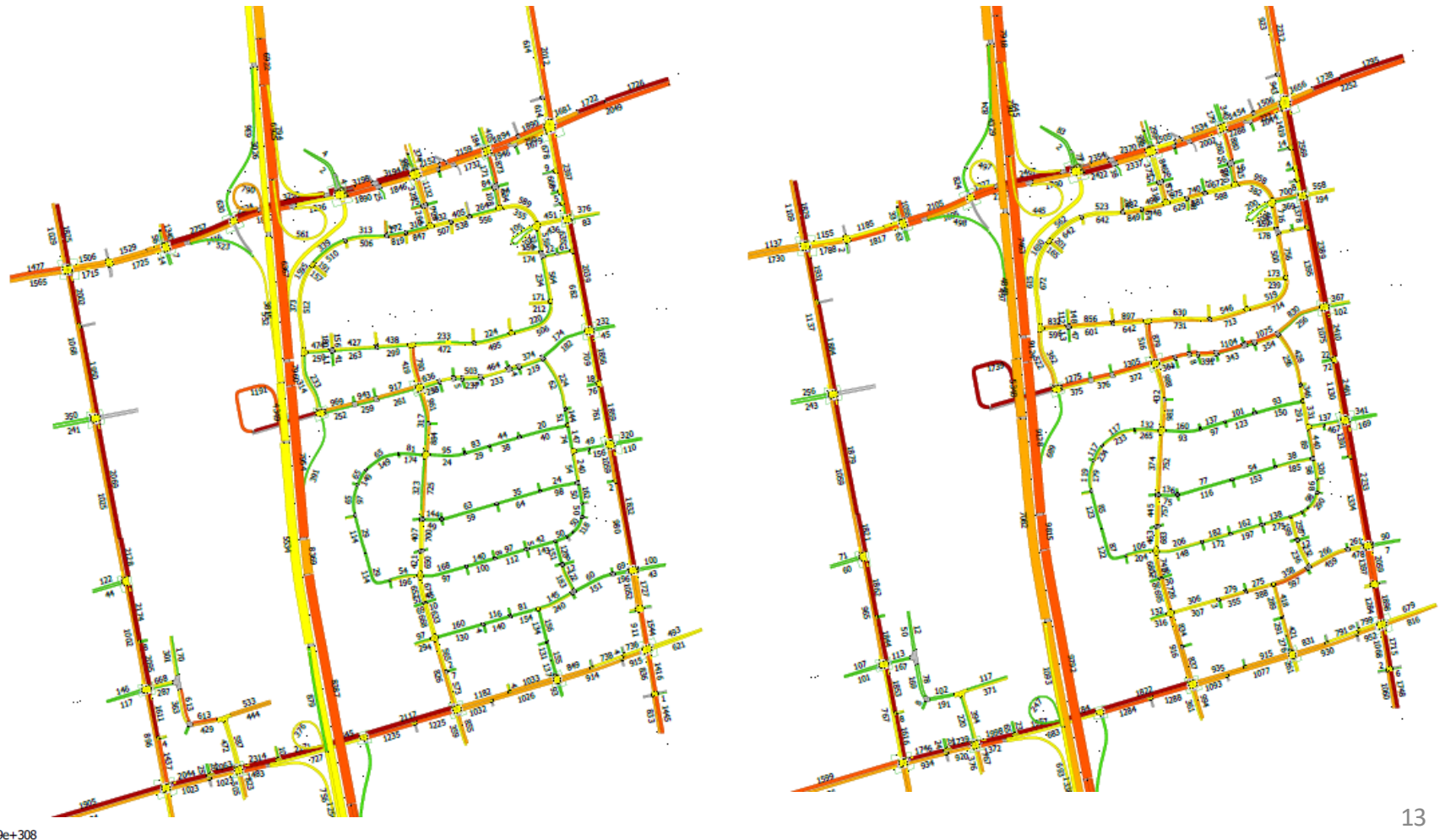
# Existing vs. Future Baseline Analysis

Based on preliminary review of the existing (2020) PM peak travel times and future 2031 PM peak travel times, a significant increase in study area traffic volumes is expected by 2031.

2020 Volumes - PM Peak Travel Times



2031 Volumes - PM Peak Travel Times





# Existing vs. Future Baseline Analysis

## Transportation Analysis – Major Intersection Operations (Preliminary Results)

- Many intersections show significantly worse level of service (LOS) in 2031 and 2041, when compared to existing conditions

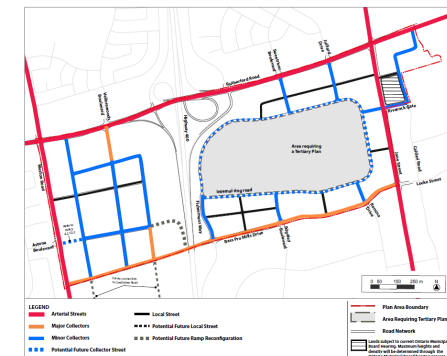
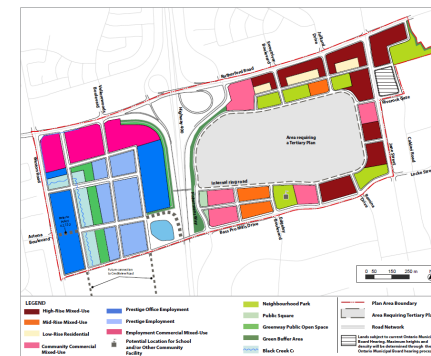
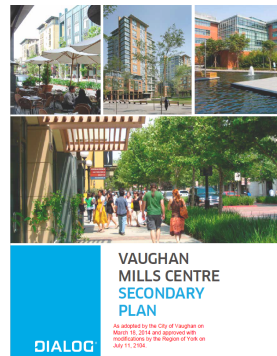
Intersection	Existing 2020				Future Baseline 2031				Future Baseline 2041			
	AM Peak Hour		PM Peak Hour		AM Peak Hour		PM Peak Hour		AM Peak Hour		PM Peak Hour	
	Delay (s)	LOS	Delay (s)	LOS	Delay (s)	LOS	Delay (s)	LOS	Delay (s)	LOS	Delay (s)	LOS
Rutherford Road at Vellore Woods Blvd.	29	C	24	C	120	F	75	E	100	F	76	E
Rutherford Road at Canada's Wonderland Drive / Hwy 400 Northbound Off-Ramp	20	B	20	C	90	F	47	D	89	F	44	D
Rutherford Road at Julliard Drive	18	B	23	C	61	E	79	E	43	D	82	F
Rutherford Road at Jane Street	43	D	46	D	78	E	100	F	68	E	90	F
Langstaff Road at Hwy 400 Northbound Off-Ramp	51	D	17	B	531	F	91	F	181	F	83	F
Langstaff Road at Weston Road	38	D	51	D	398	F	155	F	130	F	218	F

# Problems and Opportunities

## Problem and Opportunity Statement:

The purpose of this study is to assess the need to extend Bass Pro Mills Drive, from Highway 400 to Weston Road, as recommended in the Vaughan Mills Centre Secondary Plan to:

- Provide a new east-west multi-modal connection between Highway 400 and Weston Road, including a new route connection for York Region Transit (YRT);
- Help distribute east-west traffic and alleviate congestion along Rutherford Road to the north;
- Support future growth and development within the plan area; and,
- Develop a safe and comfortable environment for active transportation users.



# Technical and Environmental Studies

A number of technical and environmental studies are planned as part of this MCEA, including but not limited to:

- Traffic operations
- Socio-economic
- Ecological
- Cultural heritage
- Archaeology
- Contamination
- Stormwater management
- Noise
- Air quality
- Climate change

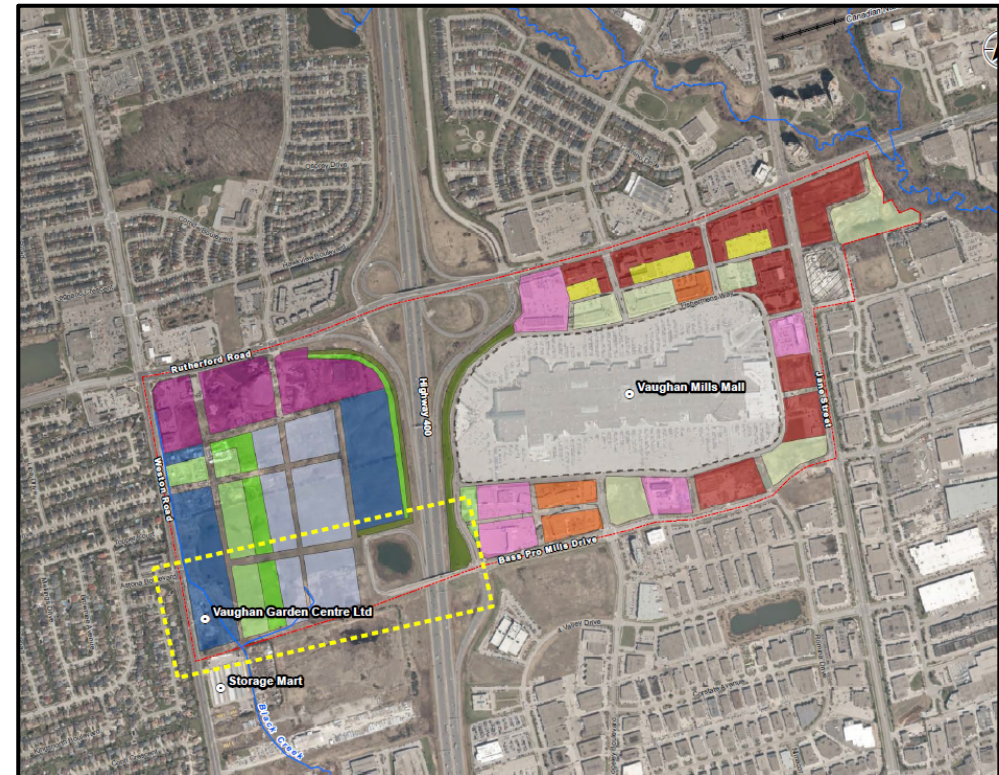


The findings of these studies will be documented and appended to the Environmental Study Report.

# Existing Study Area Conditions

## Study Area Land Use

- Along with the lands south of Rutherford Road, only remaining agricultural designated lands within this area of City
- Primarily residential to west, and mix of chain commercial, service commercial, and industry/office uses to north, south, and east



# Existing Study Area Conditions

## Natural Environment

- Primarily meadow with some shallow marsh
- Shallow water/small stormwater management pond inside the southbound ramp at the Bass Pro Mills Drive/Highway 400 interchange
- Potential for habitat for significant species, including turtle nesting/wintering habitat
- Black Creek tributary located on west side of study area may support warmwater fish species

## Archaeology

- Large portions of study area have potential for recovery of archaeological resources

## Cultural Environment

- No cultural heritage features identified within and/or surrounding the study area



# Evaluation Process

The following alternative solutions have been identified to help to address the problems and opportunities:

**Alternative 1:** Do Nothing (maintain existing conditions)

**Alternative 2:** Improve Transit, Employ Travel Demand Management measures

**Alternative 3:** Intersection and/or Operational Improvements

**Alternative 4:** Improve Existing East-West Roadways in the Area

**Alternative 5:** Extend Bass Pro Mills Drive to Weston Road

The alternatives are subjected to a comparative evaluation using a reasoned argument approach, which describes both the advantages and disadvantages of each alternative in response to a defined set of evaluation criteria.



# Evaluation Criteria

The following criteria was used to carry out the preliminary evaluation of alternative solutions:

## Transportation

- Safety
- Active Transportation
- Transit
- Traffic Operations
- Enhance Emergency Vehicle Response/Access
- Road Network Compatibility/Connectivity

## Socio-Economic Environment

- Accessibility
- Impacts to Entrances/Access to Private Properties
- Property Acquisition Requirements
- Provincial Planning Objectives
- Municipal Planning Objectives
- Compatibility with Existing and Proposed Development
- Business Operations
- Noise
- Community Access during Construction Phase

## Cultural Environment

- Archaeological Resources
- Built Heritage Resources and Cultural Landscapes

## Natural Environment

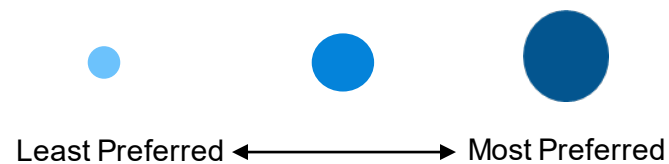
- Fisheries and Aquatic Habitat
- Surface Water Quality/Quantity
- Vegetation
- Wildlife Habitat
- Air Quality
- Climate Change

## Technical

- Impacts to utilities
- Construction Feasibility
- Stormwater/Drainage Quality/Quantity
- Cost

# Preliminary Evaluation of Alternative Solutions - Summary

Criteria	Do Nothing	Improve Transit, Employ Travel Demand Management Measures	Intersection and/or Operational Improvements	Improve Existing East West Roadways in Area	Extend Bass Pro Mills Drive to Weston Road
Transportation	●	●	●	●	●
Socio-Economic	●	●	●	●	●
Cultural Environment	●	●	●	●	●
Natural Environment	●	●	●	●	●
Technical	●	●	●	●	●
<b>Summary</b> Recommended to be carried forward?	<b>No</b> Does not address the problems and opportunities	<b>Yes</b> Within the overall strategy	<b>Yes</b> Within the overall strategy	<b>No</b> Subject to separate studies	<b>Yes</b> Carried forward as Recommended Solution



A copy of the detailed Evaluation of Alternative Solutions is available for your review [here](#).



# Recommended Solution

Based on the findings of the Evaluation of Alternative Solutions, **extending Bass Pro Mills Drive to Weston Road is recommended** based on the following key rationale:

- Provides the best opportunity to improve travel safety, enhance the pedestrian environment, support the development of new multi-modal transportation connections, provide an alternative east-west route and alleviate traffic congestion for the long-term
- Highest potential to increase accessibility, improve access to private property, support existing and future development and is in line with municipal and provincial planning objectives
- Provides the best opportunity to increase resilience to climate change, improve air quality and implement surface water control measures.
- Although the cost is moderate to high compared to the other alternatives, there are limited impacts to utilities or the disruption of traffic operations as the construction activities would be generally limited to undeveloped lands.



# Evaluation Approach – Recommended Design

A staged approach will be used to identify and evaluate alternative design concepts, and to identify a recommended design that is cost effective, provides safe and functional traffic operations, improves local access, and minimizes impacts to the environment.

A second PIC will be scheduled to present the evaluation of alternatives design concepts and identified recommended design for the proposed extension of Bass Pro Mills Drive.

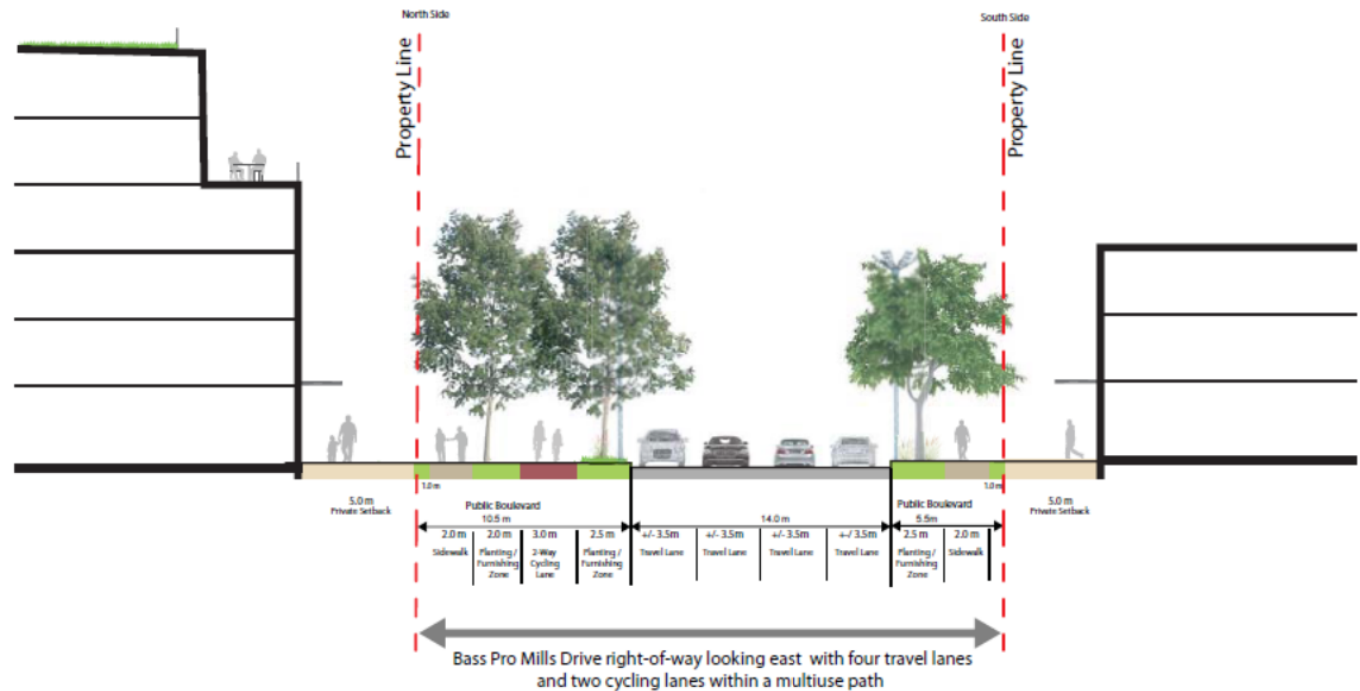
**Stage 1**  
**Evaluate Alternative**  
**Alignments**

**Stage 2**  
**Evaluate Alternative**  
**Cross-Sections**

**Stage 3**  
**Identify**  
**Recommended Design**

# Potential Cross-Section

Major collector with multiuse path  
(30 metre right-of-way)



Vaughan Mills Centre Secondary Plan, Section D: Major Collector Special Condition (Bass Pro Mills)

# Next Steps

- Review and consider Feedback Following the Comment Period
- Evaluate Alternative Alignments and Cross-Sections
- Identify a Recommended Design
- Attend Public Information Centre 2 (tentatively scheduled for Spring 2021)
- Draft Environmental Study Report for Agency Review (June 2021)
- Final Environmental Study Report for 30-day Public Review Period (September 2021)

Please use the below link to provide valuable feedback through the PIC 1 survey or contact one of the below project team members directly. We would greatly appreciate receiving any comments or question you may have by **January 8, 2021**.

## [Survey Link](#)

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### **Slide 1 – Cover**

Hello, and thank you for joining us for this online Public Information Centre for the Bass Pro Mills Extension (from Highway 400 to Weston Road) Schedule C Municipal Class Environmental Assessment.

This video presentation will briefly take you through some of the key features and details of this study. Please stay tuned until the end of the presentation to discover ways in which you can provide valuable feedback to the project team.

### **Slide 2 – PIC 1 Objectives**

This is the first Public Information Centre for the Bass Pro Mills Extension (from Highway 400 to Weston Road) Municipal Class Environmental Assessment or MCEA study.

The objectives of this online PIC are to:

- Introduce the project and outline the MCEA process that is being followed
- Provide background information on the need for the improvements
- Present the evaluation of alternative solutions and the recommended solution
- Seek input on the existing conditions within the study area (i.e. natural, social, economic and cultural)
- Gather your feedback on this study

### **Slide 3 – Study Overview**

The City of Vaughan is undertaking a Municipal Class Environmental Assessment study for the proposed extension of Bass Pro Mills Drive, from Highway 400 westerly to Weston Road. This extension would provide a new major collector roadway that unites neighbourhoods from Weston Road to Jane Street, redistributes east-west traffic and alleviates congestion on Rutherford Road.

An enhanced boulevard could accommodate additional York Region Transit amenities, a pedestrian friendly multi-use trail, as well as on-street cycling facilities.

The proposed extension of Bass Pro Mills Drive is envisioned to support future development within the study area, including the employment and intensification plans developed as part of the Vaughan Mills Centre Secondary Plan.

### **Slide 4 – Vaughan Mills Centre Secondary Plan**

The City of Vaughan adopted the Secondary Plan in 2014 to establish a framework for land use planning in the area and to guide future development planning within the subject lands.

As part of the Secondary Plan, recommendations for transportation improvements within the study area were developed based on an assessment of:

- Future land use
- Forecasted population and employment growth
- Projected future traffic volumes
- Existing and future roadway operations

- Roadway engineering design criteria
- Technical evaluation of transportation options
- Feedback received from the public, agencies, and landowners/developers

The extension of Bass Pro Mills Drive was recommended in the Secondary Plan, consisting of a wide, visually appealing streetscape with a multi-use trail and on-street cycling facilities along the new road right-of-way.

### **Slide 5 – Background Studies**

The following documents were prepared to provide a framework for land use and transportation planning in the City and Region and are being considered as part of this study. Please click the links provided on this slide to review more information about these important documents.

The Provincial Policy Statement is the guiding document that provides overall policy direction on matters of provincial interest relating to land use planning and development in Ontario. Local Official Plans are the primary mechanism for implementing the policies of the PPS, and in accordance with the Planning Act, decisions affecting planning matters shall be consistent with the Provincial Policy Statement.

The Growth Plan for the Greater Golden Horseshoe (or Growth Plan) assists in the expansion of economic opportunities for the growing number of people expected to reside within the Greater Golden Horseshoe over the next 20 years. It is a goal of the GPGGH to develop an integrated transportation network that allows people choices for easy travel both within and between urban centres.

The City of Vaughan Transportation Master Plan was developed in 2012 and focused on auto-oriented urban structure. The City is one of the fastest-growing municipalities in Canada and a major Urban growth Centre within the GGH. As part of the Transportation Master Plan, the extension of Bass Pro Mills Drive to the collector road network west of Highway 400 was identified as a major gap in the transportation network.

The 2010 York Region Official Plan guides the economic, environmental and community building decisions to assist in the coordination and management of growth within the Region.

The 2010 City of Vaughan Official Plan is part of the overall Growth Management Strategy that will shape the future of the City and guide its transformation into a vibrant, beautiful and sustainable City. A main goal of the Official Plan is to create a sustainable transportation network that compliments sustainable development, recognizing that land use and transportation are inextricably linked.

The 2016 York Region Transportation Master Plan is a fundamental tool to accommodate the population and employment growth forecasted out to 2041. The Transportation Master Plan establishes the vision for transportation services, assesses existing transportation system performance, forecasts future travel demand and defines actions and policies to address road, transit and active transportation needs in York Region to 2041.

A number of background studies from adjacent areas are also being considered as part of this study. The links provided on this slide will guide you to the latest information about these City or Region initiatives.

### **Slide 6 – Municipal Class Environmental Process**

Municipal planning for infrastructure improvements must be completed in accordance with the Municipal Engineers Association's Municipal Class Environmental Assessment. This is an approved process under the Environmental Assessment Act.

The proposed extension of Bass Pro Mills Drive classifies as a Schedule 'C' project, which are required to follow Phases 1 through 4 of the MCEA process. The four phases of the Municipal Class Environmental Assessment Process are:

Phase 1 – Identify the problem and/or opportunity

Phase 2 – Develop and evaluate alternative solutions and identify a preferred solution

Phase 3 – Develop and evaluate alternative designs and identify a preferred design

Phase 4 – Document the process in an Environmental Study Report, which is available for a 30-day public review period before the project is able to move to the next and final phase of the process, Project Implementation.

As indicated in the figure, consultation with the public and community stakeholders is carried out throughout the study process, and you are encouraged to participate and share your ideas with members of the study team at any time.

At this time, we are currently within Phase 2 of the study process, and we will confirm the preferred solution once we have reviewed and considered the feedback received during this online PIC.

### **Slide 7 – Transportation Analysis Study Areas**

A Transportation Analysis is being undertaken as part of this study. The study area includes a Primary Study Area (shown in red in this figure) and a Secondary Study Area (shown in blue in this figure).

The Primary Study Area is generally bounded by Weston Road to the west, Jane Street to the east, Langstaff Road to the south, and Rutherford Road to the north. This is where the greatest impact to transportation will be experienced as a result of an extension to Bass Pro Mills Drive, and a microsimulation model was developed for this study to examine the effects a proposed extension of the roadway will have on traffic movements .

The Secondary Study Area is generally bounded by Pine Valley Drive to the west, Credit Stone Road to the east, Highway 7 to the south, and Major Mackenzie Drive West to the north. This is where minor changes in traffic may be experienced as a result of an extension to Bass Pro Mills Drive.

### **Slide 8 – Existing Transportation Network**

The existing road network in the vicinity of the study area includes the following major roads:

- Rutherford Road
- Jane Street

- Bass Pro Mills Drive
- Langstaff Road
- Weston Road
- Highway 400

As shown on the table on this slide, the study area is car-oriented and auto use makes up over 90% of trips to and from Vaughan Mills Mall and over 85% of the total trips taken within the City of Vaughan.

#### **Slide 9 – Existing Transportation Network**

With the exception of sidewalks along Weston Road, there are currently no active transportation facilities provided within the study area. Sidewalks are provided along many of the roadways surrounding the study area; however, the existing active transportation network is limited to few cycling or multi-use paths, including a shared roadway with sharrows on Jane Street, and an unsigned bike route on Springdale Road.

As travel within the area is predominately car oriented, active transportation consists of only 4% of trips to and from Vaughan Mills Mall during the AM peak hours and 0% during the PM peak hours.

#### **Slide 10 – Existing Transportation Network**

The existing transit network includes eight York Region Transit routes, connections to the TTC, Mississauga Transit, Brampton Transit and GO Transit in the surrounding area and frequent service on route 20 Jane Street (every 15 minutes or better).

The existing transit routes are based on York Region Transit existing service routes provided on YRT's website. The table shown on this slide outlines the existing York Region Transit routes that operate within the study area during the AM and PM peak hours.

#### **Slide 11 – Existing vs. Future Baseline Analysis**

The preliminary results of the transportation analysis are based on modelling future traffic operations in 2031 and 2041. The results of this modelling indicate that if no modifications are implemented within the study area, there is expected to be a significant increase in travel delays and travel times within the Primary Study Area when compared to existing conditions. For example, vehicle travel times are anticipated to increase by 84% in 2031, and by 103% in 2041, during the peak morning travel periods. In addition, vehicle travel times are anticipated to increase by 75% in 2031 and by 82% in 2041 during the peak evening travel periods.

#### **Slide 12 – Existing vs. Future Baseline Analysis**

Traffic in the area includes thousands of trips generated by residents going to work and other destinations within and outside Vaughan, trips from outside Vaughan coming into Vaughan to work and other destinations, trips to/from the Vaughan Mills Centre (and surrounding businesses) which is a key destination for the City, trips between regions (north-south and east-west), future trips generated to/from the planned employment areas west of Highway 400 and much more.

Existing and future traffic capacity can be expressed in terms of Volume/Capacity (or V/C) ratio. A V/C ratio in excess of 1 means that the traffic volumes exceed the capacity of the roadway. The figures



shown on this slide illustrate the traffic volumes for the existing 2020 AM and expected volumes during the 2031 AM peak travel times, and demonstrate that a significant increase in study area traffic volumes is expected by 2031.

### **Slide 13 – Existing vs. Future Baseline Analysis**

The figures shown on this slide demonstrate the traffic volumes for the existing 2020 PM and future 2031 PM peak travel time. It is noted that a significant increase in study area traffic volumes is expected by 2031 during the PM peak travel time.

### **Slide 14 – Existing vs. Future Baseline Analysis**

The level of service (or LOS) is a measure of the free flow of traffic on a roadway. The table shown on this slide outlines the expected level of service results at 6 of the major intersections in the vicinity of the study area. The results illustrated in red within this table demonstrate that many of the intersections in the vicinity of the study area are anticipated to perform at a significantly worse level of service in 2031 and 2041, when compared to the existing conditions.

### **Slide 15 – Problems and Opportunities**

Based on the recommendations set forth in the Secondary plan, the following Problem and Opportunity Statement was outlined for this study:

The purpose of this study is to assess the need to extend Bass Pro Mills Drive, from Highway 400 to Weston Road, as recommended in the Vaughan Mills Centre Secondary Plan to:

- provide a new east-west multi-modal connection between Highway 400 and Weston Road, including a new route connection for York Region Transit (YRT);
- help distribute east-west traffic and alleviate congestion along Rutherford Road to the north;
- support future growth and development within the plan area; and,
- develop a safe and comfortable environment for active transportation users.

### **Slide 16 – Technical and Environmental Studies**

A number of technical and environmental studies are planned as part of this MCEA, including but not limited to:

- Traffic operations
- Socio-economic
- Terrestrial and aquatic
- Cultural heritage
- Archaeology
- Contamination
- Stormwater management
- Noise
- Air quality
- Climate change

The findings of these studies will be documented and appended to the Environmental Study Report, which will be available for public review during the 30-day public review period.

### **Slide 17 – Existing Study Area Conditions**

A number of desktop studies have been completed to date, including a socioeconomic review, natural environment review, Cultural Heritage Overview Assessment.

Based on the findings of this review, land uses within the study area generally consist of residential to west, and a mix of chain commercial, service commercial, and industry/office uses to north, south, and the east, although a large portion of the study area currently consists of one of the only remaining agricultural land designation in the City.

### **Slide 18 – Existing Study Area Conditions**

Based on the findings of the natural heritage reviews undertaken to date, much of the study area consists of meadow communities with some shallow marsh.

There is the potential for habitat of several significant species, and a tributary to Black Creek is located on the west side of the study area. This watercourse flows south to connect to Humber River, and may support warmwater fish species.

A large portion of the study area is undeveloped and there has potential to contain archaeological resources. However, there are no built heritage and/or cultural heritage landscapes present within or adjacent to the study area.

### **Slide 19 – Evaluation Process**

As noted earlier in this presentation, we are in Phase 2 of the Municipal Class Environmental Assessment process, where Alternative Solutions are developed and assessed to identify a solution that helps to address the identified problems and opportunities. As such, a solution that is cost-effective, helps to alleviate east-west travel, supports future growth within the study area, and provides a safe and comfortable environment for active transportation users while minimizing the effects on the natural, social and cultural environments is preferred. To achieve this, the following five alternative solutions have been identified to help to address the problems and opportunities:

- Alternative 1: Do Nothing
- Alternative 2: Improve Transit, Employ Travel Demand Management measures
- Alternative 3: Intersection and/or Operational Improvements
- Alternative 4: Improve Existing East-West Roadways in the Area
- Alternative 5: Extend Bass Pro Mills Drive to Weston Road

The alternatives are evaluated using a reasoned argument approach, which describes both the advantages and disadvantages of each alternative in response to a defined set of evaluation criteria.

Opportunities to incorporate mitigation measures to offset potential adverse impacts are also considered within this process.

### **Slide 20 – Evaluation Criteria**

The following criteria was used to carry out the preliminary evaluation of alternative solutions and form the basis of the criteria that will be used to evaluate alternative design concepts as part of the next phase.

Please let us know if there are any criteria you feel should be included.

### **Slide 21 – Preliminary Evaluation of Alternative Solution – Summary**

The table shown on this slide presents a summary of the evaluation of the five alternative solutions, including: Do nothing; Improve Transit and Employ Travel Demand Management Measures; Intersection and/or Operational Improvements; Improve Existing East-West Roadways in the area; and, extend Bass Pro Mills Drive to Weston Road. Based on this comparative evaluation, the five alternatives are ranked from least, to most preferred. As it was ranked as one of the more preferable alternatives in each of the evaluation categories, the extension of Bass Pro Mills Drive has been identified as a Recommended Solution.

Note that, while not preferred as standalone solutions, improvements to transit, employment of travel demand measures and intersection and/or operational improvements will also be carried forward as part of the overall strategy.

A more detailed copy of this evaluation can be viewed by clicking the link provided at the bottom right corner of this slide.

### **Slide 22 – Recommended Solution**

Based on the findings of the evaluation of Alternative Solutions, extending Bass Pro Mills Drive to Weston Road is recommended based on the following key rationale:

- It provides the best opportunity to improve travel safety, enhance the pedestrian environment, support the development of new multi-modal transportation connections, provide an alternative east-west route, and alleviate traffic congestion for the long-term
- It has the highest potential to increase accessibility, improve access to private property, support existing and future development and is in line with municipal and provincial planning objectives
- It provides the best opportunity to increase resilience to climate change, improve air quality and implement surface water control measures.
- Although the cost of the proposed extension is moderate to high compared to the other alternatives, there are limited impacts to utilities or the disruption of traffic operations as the construction activities would be generally limited to undeveloped lands.

### **Slide 23 – Evaluation Approach – Recommended Design**

A staged approach will be used to identify and evaluate alternative design concepts, and to identify a recommended design that is cost effective, provides safe and functional traffic operations, improves local access, and minimizes impacts to the environment.

The three stages of this evaluation approach include:

- Stage 1: Evaluation of Alternative Alignments

160540006  
Bass Pro Mills Drive Extension  
Online PIC 1 Script

- Stage 2: Evaluation of Alternative Cross-Sections; and
- Stage 3: Identification of a Recommended Design

A second PIC will be scheduled to present and gather your feedback on the evaluation of the alternatives design concepts and the recommended design for the proposed extension of Bass Pro Mills Drive.

#### **Slide 24 – Potential Cross-Section**

The cross-section shown on this slide was developed as part of the Vaughan Mills Centre Secondary Plan and provides a potential cross-section for the proposed extension of Bass Pro Mills Drive. This potential cross-section consists of a four-lane right-of-way (two travel lanes in each direction), a two-way cycling lane and sidewalk separated by a landscape buffer located on either side of the right-of-way.

This cross-section will be assessed further as we move forward in the evaluation process.

#### **Slide 25 – Next Steps**

Following this online Public Information Centre, the study team will review and consider all feedback received during the comment period. A second and final Public Information Centre is tentatively scheduled for Spring 2021, at which time the identification and evaluation of alternative alignments and cross-sections, as well as a recommended design, will be shared for your review and feedback. The entire study process will be documented within an Environmental Study Report, which will be available for public review for a 30-day public review period.

On behalf of the City of Vaughan, we would like to thank you for your interest in this study, and for taking the time to participate in this online PIC. Your input is very important to us, and there are several ways to provide your comments.

We encourage you to review all PIC 1 materials available on the project website, as well as participate in the survey offered through the link included on this slide

Should you have any comments or questions, please do not hesitate to contact one of the project team members listed on this slide.

In the interim, we would appreciate receiving any comments or questions you may have by January 8, 2021.

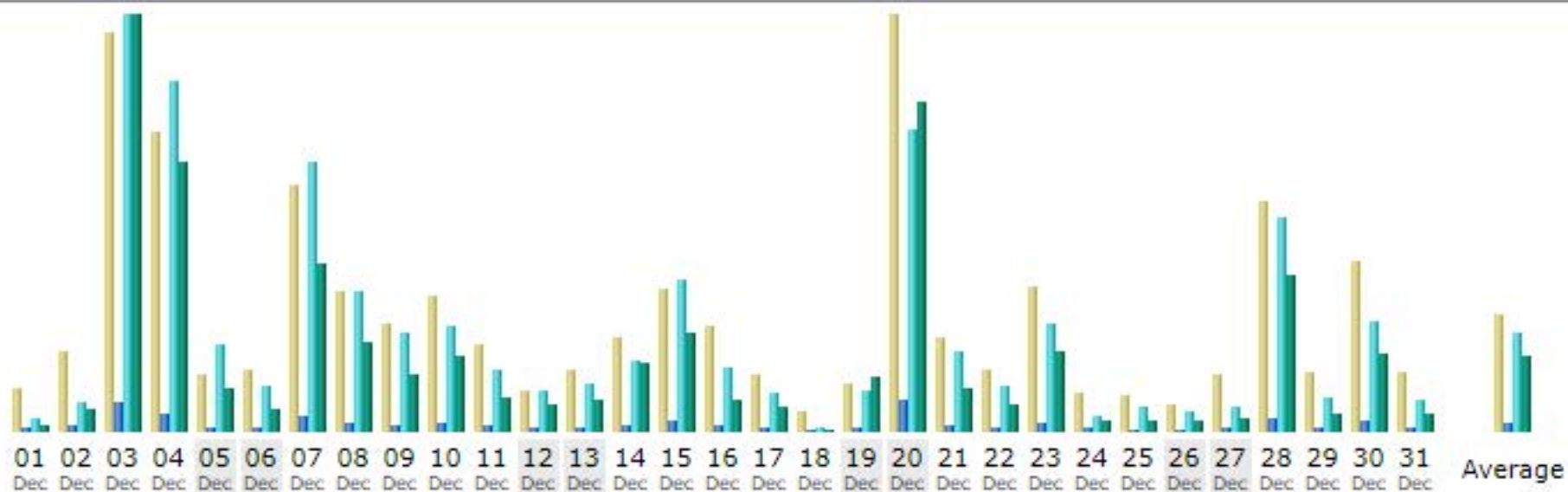
Thank you.

# Appendix C

## Online PIC 1 Visitor Statistics (Raw Data)



## Days of month



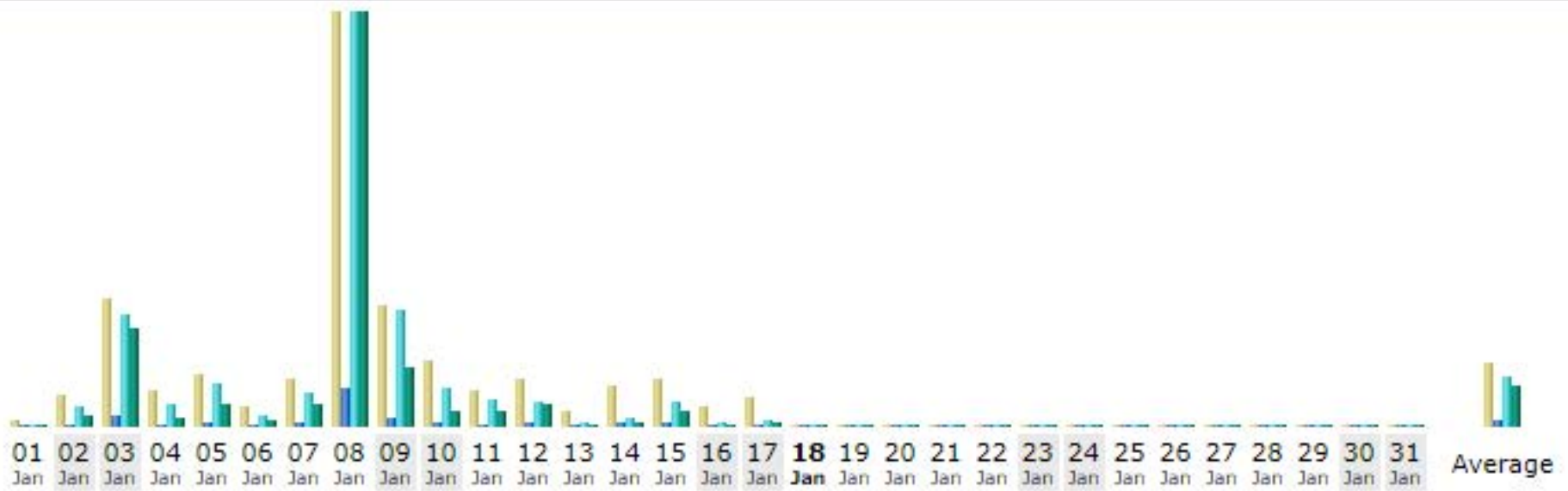
Day	Number of visits	Pages	Hits	Bandwidth
01 Dec 2020	18	20	94	14.93 MB
02 Dec 2020	34	38	235	60.66 MB
03 Dec 2020	169	232	3,322	1.13 GB
04 Dec 2020	127	144	2,789	745.67 MB
05 Dec 2020	24	29	696	121.29 MB
06 Dec 2020	26	33	355	63.76 MB
07 Dec 2020	104	128	2,152	467.43 MB
08 Dec 2020	59	65	1,122	249.65 MB
09 Dec 2020	45	51	786	156.87 MB
10 Dec 2020	57	66	837	209.18 MB
11 Dec 2020	37	39	486	94.69 MB
12 Dec 2020	17	19	317	72.59 MB
13 Dec 2020	26	27	385	83.55 MB
14 Dec 2020	40	48	569	186.76 MB
15 Dec 2020	60	76	1,205	273.73 MB
16 Dec 2020	44	48	511	89.02 MB
17 Dec 2020	24	29	305	65.48 MB
18 Dec 2020	8	10	21	1.17 MB
19 Dec 2020	20	22	322	153.23 MB
20 Dec 2020	176	257	2,415	913.07 MB
21 Dec 2020	40	49	635	121.09 MB
22 Dec 2020	26	30	365	73.42 MB
23 Dec 2020	61	73	850	218.72 MB
24 Dec 2020	16	20	120	26.44 MB
25 Dec 2020	15	17	198	30.97 MB
26 Dec 2020	11	13	157	30.28 MB
27 Dec 2020	24	26	192	33.33 MB
28 Dec 2020	97	110	1,709	434.03 MB
29 Dec 2020	25	28	273	50.09 MB
30 Dec 2020	72	85	877	215.01 MB
31 Dec 2020	25	26	254	48.72 MB
Average	49	59	792	208.40 MB
Total	1,527	1,858	24,554	6.31 GB

## Monthly history



Month	Unique visitors	Number of visits	Pages	Hits	Bandwidth
Jan 2020	0	0	0	0	0
Feb 2020	0	0	0	0	0
Mar 2020	0	0	0	0	0
Apr 2020	0	0	0	0	0
May 2020	0	0	0	0	0
Jun 2020	0	0	0	0	0
Jul 2020	0	0	0	0	0
Aug 2020	0	0	0	0	0
Sep 2020	0	0	0	0	0
Oct 2020	0	0	0	0	0
Nov 2020	45	54	69	406	73.38 MB
Dec 2020	328	1,527	1,858	24,554	6.31 GB
Total	373	1,581	1,927	24,960	6.38 GB

## Days of month



Day	Number of visits	Pages	Hits	Bandwidth
01 Jan 2021	6	7	24	5.31 MB
02 Jan 2021	29	32	321	77.60 MB
03 Jan 2021	116	164	1,896	661.32 MB
04 Jan 2021	32	36	387	58.36 MB
05 Jan 2021	47	52	730	145.58 MB
06 Jan 2021	17	19	189	39.50 MB
07 Jan 2021	43	49	547	155.21 MB
08 Jan 2021	376	651	6,996	2.75 GB
09 Jan 2021	110	141	1,952	396.90 MB
10 Jan 2021	59	68	644	94.88 MB
11 Jan 2021	32	36	450	100.21 MB
12 Jan 2021	42	53	407	146.96 MB
13 Jan 2021	14	15	58	10.37 MB
14 Jan 2021	36	39	140	26.88 MB
15 Jan 2021	42	46	421	96.88 MB
16 Jan 2021	18	22	45	6.13 MB
17 Jan 2021	26	34	112	20.65 MB
<b>18 Jan 2021</b>	1	1	1	0
19 Jan 2021	0	0	0	0
20 Jan 2021	0	0	0	0
21 Jan 2021	0	0	0	0
22 Jan 2021	0	0	0	0
23 Jan 2021	0	0	0	0
24 Jan 2021	0	0	0	0
25 Jan 2021	0	0	0	0
26 Jan 2021	0	0	0	0
27 Jan 2021	0	0	0	0
28 Jan 2021	0	0	0	0
29 Jan 2021	0	0	0	0
30 Jan 2021	0	0	0	0
31 Jan 2021	0	0	0	0
Average	58	81	851	269.91 MB
Total	1,046	1,465	15,320	4.74 GB



## Monthly history



Month	Unique visitors	Number of visits	Pages	Hits	Bandwidth
Jan 2021	268	1,046	1,465	15,320	4.74 GB
Feb 2021	0	0	0	0	0
Mar 2021	0	0	0	0	0
Apr 2021	0	0	0	0	0
May 2021	0	0	0	0	0
Jun 2021	0	0	0	0	0
Jul 2021	0	0	0	0	0
Aug 2021	0	0	0	0	0
Sep 2021	0	0	0	0	0
Oct 2021	0	0	0	0	0
Nov 2021	0	0	0	0	0
Dec 2021	0	0	0	0	0
Total	268	1,046	1,465	15,320	4.74 GB

# Appendix D

## Online PIC 1 Survey





## Bass Pro Mills Extension - Online Public Information Centre 1

Your feedback is very important to this study. Please complete this online comment form or contact us by email or phone to share your thoughts by **January 8, 2021**.

For information about this study, please contact:

Hilda Esedebe, P.Eng.  
City of Vaughan  
Project Manager  
2141 Major Mackenzie Dr.  
Vaughan, ON L6A 1T1  
T: 905-832-2281, ext. 8484  
E: [Hilda.Esedebe@vaughan.ca](mailto:Hilda.Esedebe@vaughan.ca)

Peter Cholewa, P.Eng.  
Stantec Consulting Ltd.  
Project Manager  
300W-675 Cochrane Drive  
Markham, ON L3R 0A8  
T: 905-415-6358  
E: [Peter.Cholewa@stantec.com](mailto:Peter.Cholewa@stantec.com)

Diana Addley  
Stantec Consulting Ltd.  
Senior Environmental Planner  
300W-675 Cochrane Drive  
Markham, ON L3R 0A8  
T: 905-415-6401  
E: [Diana.Addley@stantec.com](mailto:Diana.Addley@stantec.com)

*Personal information on this form is collected under the authority of the Municipal Act, 2001 and will be used for the purpose of administering the Bass Pro Mills Environmental Assessment Study (between Highway 400 and Weston Road). Questions about this collection can be directed to the Manager, Transportation Planning, City of Vaughan, 2141 Major Mackenzie Drive, Vaughan, ON L6A 1T1, 905-832-8585.*

1. What are your greatest difficulties when driving east-west in the vicinity of the Bass Pro study area? (Select All That Apply)

- Traffic congestion
- Limited alternative routes
- Traffic signal timing
- Road construction

2. What are your greatest difficulties when using alternative modes of transportation in the vicinity of the Bass Pro study area? (Select All That Apply)

- Cycling facility is on the roadway
- Lack of bike lanes or cycling facilities on the roadway
- Lack of bike lanes or cycling facilities within the boulevard
- Lack of public transit services
- Limited pedestrian friendly routes

3. What Active Transportation improvements would you most like to see through the extension of Bass Pro Mills Drive (Select All That Apply)

- Bike Paths
- Sidewalks
- Shared Multi-Use Paths (for both pedestrians and cyclists)
- Separated On-Road Bike Paths

4. What Community improvements would you most like to see through the extension of Bass Pro Mills Drive (Select All That Apply)

- Street furniture (i.e. benches, bike racks)
- Bus shelters
- Wider Boulevards with street trees
- Enhanced Streetscaping

5. How important is it to you to have a pedestrian/cycling friendly connection between Weston Road and Jane Street?

- Very Important

- Important
- Somewhat Important
- Of Little Importance
- Not Important at All

6. How often do you travel east-west to Vaughan Mills Mall?

- < Once per month
- 1-2 times per month
- Weekly
- Multiple times per week

7. How often do you use public transportation?

- < Once per month
- 1-2 times per month
- Weekly
- Multiple times per week

8. Would you like to receive future notifications about the study?

- Yes
- No

9. Your opinion is important to the Study Team. In the area below, please provide any comments or feedback that you wish to share with the Study Team.

10. Please provide contact information of which you would like to receive future updates regarding the study to (i.e. Mailing Address, Email etc.)

Done

---

Powered by



See how easy it is to [create a survey](#).

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# Appendix E

## Online PIC 1 Comments & Survey Responses



## Robinson, Jennifer

---

**To:** Esedebe, Hilda  
**Subject:** RE: Bass Pro Mills EA - Online PIC is now Live!

---

**From:** Frieri, Michael <[Michael.Frieri@vaughan.ca](mailto:Michael.Frieri@vaughan.ca)>

**Sent:** Thursday, December 3, 2020 4:29 PM

**To:** Esedebe, Hilda <[Hilda.Esedebe@vaughan.ca](mailto:Hilda.Esedebe@vaughan.ca)>; Perretta, Gabriella <[Gabriella.Perretta@vaughan.ca](mailto:Gabriella.Perretta@vaughan.ca)>; Hubjer, Selma <[Selma.Hubjer@vaughan.ca](mailto:Selma.Hubjer@vaughan.ca)>; Martino, Gino <[Gino.Martino@vaughan.ca](mailto:Gino.Martino@vaughan.ca)>; Couto, Carlos <[Carlos.Couto@vaughan.ca](mailto:Carlos.Couto@vaughan.ca)>; Velasquez, Diego <[Diego.Velasquez@vaughan.ca](mailto:Diego.Velasquez@vaughan.ca)>; Madhusoothanan, Manu <[Manu.Madhusoothanan@vaughan.ca](mailto:Manu.Madhusoothanan@vaughan.ca)>; Shahrokni, Mani <[Mani.Shahrokni@vaughan.ca](mailto:Mani.Shahrokni@vaughan.ca)>; Lai, Winnie <[Winnie.Lai@vaughan.ca](mailto:Winnie.Lai@vaughan.ca)>; Roias, Marta <[Marta.Roias@vaughan.ca](mailto:Marta.Roias@vaughan.ca)>; Tam, Christopher <[Christopher.Tam@vaughan.ca](mailto:Christopher.Tam@vaughan.ca)>; Kowpak, Dorothy <[Dorothy.Kowpak@vaughan.ca](mailto:Dorothy.Kowpak@vaughan.ca)>; Emelianov, Petr <[Petr.Emelianov@vaughan.ca](mailto:Petr.Emelianov@vaughan.ca)>; Wong, Justin <[Justin.Wong@vaughan.ca](mailto:Justin.Wong@vaughan.ca)>; Baraam, Lamya <[Lamya.Baraam@vaughan.ca](mailto:Lamya.Baraam@vaughan.ca)>; Dubeski, Phil <[Phil.Dubeski@vaughan.ca](mailto:Phil.Dubeski@vaughan.ca)>; Yovanovski, Jimmy <[Jimmy.Yovanovski@vaughan.ca](mailto:Jimmy.Yovanovski@vaughan.ca)>; Shelley-MacKay, Kody <[Kody.Shelley-MacKay@vaughan.ca](mailto:Kody.Shelley-MacKay@vaughan.ca)>; Pesci, Paula <[Paula.Pesci@vaughan.ca](mailto:Paula.Pesci@vaughan.ca)>; Musacchio, Vince <[Vince.Musacchio@vaughan.ca](mailto:Vince.Musacchio@vaughan.ca)>

**Subject:** RE: Bass Pro Mills EA - Online PIC is now Live!

Hey Hilda,

Just had a look at the virtual presentation...very effective! Great job...thanks.

As a resident of Block 39 (NE corner of Rutherford Rd and Weston Rd, I couldn't be more supportive of this proposed missing link for Bass Pro...let's get it built ASAP!

Thanks  
Mike

---

**From:** Esedebe, Hilda <[Hilda.Esedebe@vaughan.ca](mailto:Hilda.Esedebe@vaughan.ca)>

**Sent:** Thursday, December 03, 2020 1:16 PM

**To:** Perretta, Gabriella <[Gabriella.Perretta@vaughan.ca](mailto:Gabriella.Perretta@vaughan.ca)>; Frieri, Michael <[Michael.Frieri@vaughan.ca](mailto:Michael.Frieri@vaughan.ca)>; Hubjer, Selma <[Selma.Hubjer@vaughan.ca](mailto:Selma.Hubjer@vaughan.ca)>; Martino, Gino <[Gino.Martino@vaughan.ca](mailto:Gino.Martino@vaughan.ca)>; Couto, Carlos <[Carlos.Couto@vaughan.ca](mailto:Carlos.Couto@vaughan.ca)>; Velasquez, Diego <[Diego.Velasquez@vaughan.ca](mailto:Diego.Velasquez@vaughan.ca)>; Madhusoothanan, Manu <[Manu.Madhusoothanan@vaughan.ca](mailto:Manu.Madhusoothanan@vaughan.ca)>; Shahrokni, Mani <[Mani.Shahrokni@vaughan.ca](mailto:Mani.Shahrokni@vaughan.ca)>; Lai, Winnie <[Winnie.Lai@vaughan.ca](mailto:Winnie.Lai@vaughan.ca)>; Roias, Marta <[Marta.Roias@vaughan.ca](mailto:Marta.Roias@vaughan.ca)>; Tam, Christopher <[Christopher.Tam@vaughan.ca](mailto:Christopher.Tam@vaughan.ca)>; Kowpak, Dorothy <[Dorothy.Kowpak@vaughan.ca](mailto:Dorothy.Kowpak@vaughan.ca)>; Emelianov, Petr <[Petr.Emelianov@vaughan.ca](mailto:Petr.Emelianov@vaughan.ca)>; Wong, Justin <[Justin.Wong@vaughan.ca](mailto:Justin.Wong@vaughan.ca)>; Baraam, Lamya <[Lamya.Baraam@vaughan.ca](mailto:Lamya.Baraam@vaughan.ca)>; Dubeski, Phil <[Phil.Dubeski@vaughan.ca](mailto:Phil.Dubeski@vaughan.ca)>; Yovanovski, Jimmy <[Jimmy.Yovanovski@vaughan.ca](mailto:Jimmy.Yovanovski@vaughan.ca)>; Shelley-MacKay, Kody <[Kody.Shelley-MacKay@vaughan.ca](mailto:Kody.Shelley-MacKay@vaughan.ca)>; Pesci, Paula <[Paula.Pesci@vaughan.ca](mailto:Paula.Pesci@vaughan.ca)>; Musacchio, Vince <[Vince.Musacchio@vaughan.ca](mailto:Vince.Musacchio@vaughan.ca)>

**Subject:** Bass Pro Mills EA - Online PIC is now Live!

Hello IPCAM,

Please be advised that the first of two online Public Information Centres for the Bass Pro Mills Environmental Assessment Study is now available at [www.vaughan.ca/basspromillsea](http://www.vaughan.ca/basspromillsea), until January 8, 2020. It includes a self-guided presentation and survey, and we are seeking feedback on the study background, evaluation of alternative solutions, the recommended solution and next steps in the study process. Attached are the PSA and Notice for more information.

Hope you're able to take a look, especially those of you that live in or frequent the area 😊.

Thanks everyone!



**Hilda Esedebe, P.Eng., MBA, M.Sc.**  
**Transportation Project Manager**  
**Infrastructure Planning and Corporate Asset Management**  
905-832-8585, ext. 8484 | [hilda.esedebe@vaughan.ca](mailto:hilda.esedebe@vaughan.ca)

**City of Vaughan | Infrastructure Development**  
2141 Major Mackenzie Dr., Vaughan, ON L6A 1T1  
[vaughan.ca](http://vaughan.ca)



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**Robinson, Jennifer**

---

**To:** Esedebe, Hilda  
**Subject:** RE: [External] BASS PRO MILLS DRIVE EXTENSION ENVIRONMENTAL ASSESSMENT

**From:** [REDACTED]  
**Sent:** Thursday, December 3, 2020 9:20 PM  
**To:** Esedebe, Hilda <[Hilda.Esedebe@vaughan.ca](mailto:Hilda.Esedebe@vaughan.ca)>  
**Subject:** Re: [External] BASS PRO MILLS DRIVE EXTENSION ENVIRONMENTAL ASSESSMENT

Dear Hilda:

Thank you for replying so promptly.

Yes, I would like to be added to the contact list.

Kind regards,

[REDACTED]

On Thu., Dec. 3, 2020, 9:13 p.m. Esedebe, Hilda, <[Hilda.Esedebe@vaughan.ca](mailto:Hilda.Esedebe@vaughan.ca)> wrote:

H [REDACTED],

Thank you for your interest in the Bass Pro Mills Environmental Assessment (EA) Study. Your concerns have been noted. A Transportation/Traffic Analysis and Safety Assessment for the study area will be completed at the later stages of the study and results will be shared with the public at that time. A preliminary Transportation/Traffic Analysis has been completed and summarized in the online Public Information Centre (PIC), which I hope you've been able to view on the study website ([www.vaughan.ca/basspromillsea](http://www.vaughan.ca/basspromillsea)). The PIC will be available online to view until January 8, 2020.

Although you mentioned Kleinburg and Woodbridge which are not within this study area, there are many studies that are ongoing within the City of Vaughan that support improvements to the road network and aim to provide many of the City's missing east-west connections, including the [Langstaff Road Improvements EA](#), the [Teston Road Individual EA](#), the [Kirby Road Extension EA](#), and the [GTA West EA](#) to mention a few.

If you would like to discuss further or be added to the study contact list for future updates, please let me know.

Regards,

Hilda Esedebe, P.Eng., MBA, M.Sc.

Transportation Project Manager

Infrastructure Planning and Corporate Asset Management  
905-832-8585, ext. 8484 | [hilda.esedebe@vaughan.ca](mailto:hilda.esedebe@vaughan.ca)

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2141 Major Mackenzie Dr., Vaughan, ON L6A 1T1  
[vaughan.ca](http://vaughan.ca)



**From:** [REDACTED] >  
**Sent:** Thursday, December 3, 2020 12:09 PM  
**To:** Esedebe, Hilda <[Hilda.Esedebe@vaughan.ca](mailto:Hilda.Esedebe@vaughan.ca)>  
**Subject:** [External] BASS PRO MILLS DRIVE EXTENSION ENVIRONMENTAL ASSESSMENT

Hello

This is all very interesting but will it relieve all the huge tractor trailer traffic?

Woodbridge and Kleinburg have become tractor trailer transportation hubs. These drivers all bypass the main arteries, i.e. hwy 7, 27, 50 and use Langstaff and Islington to connect east/west and north/south.

A serious accident is just waiting to happen.

Thank you.



A Woodbridge resident

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## Robinson, Jennifer

---

**From:** Esedebe, Hilda <Hilda.Esedebe@vaughan.ca>  
**Sent:** Thursday, December 3, 2020 9:49 PM  
**To:** [REDACTED]  
**Cc:** Addley, Diana; Cholewa, Peter; Robinson, Jennifer  
**Subject:** RE: Extension of Bass Pro Milles Drive

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Hi [REDACTED],

Thank you for your interest in the Bass Pro Mills Environmental Assessment (EA) Study. You will be added to the study contact list as requested. If you have not already, please endeavor to review the online Public Information Centre (PIC) which will be available on the study website ([www.vaughan.ca/basspromillsea](http://www.vaughan.ca/basspromillsea)) until January 8, 2020.

Regards,

Hilda Esedebe, P.Eng., MBA, M.Sc.  
Transportation Project Manager  
Infrastructure Planning and Corporate Asset Management  
905-832-8585, ext. 8484 | [hilda.esedebe@vaughan.ca](mailto:hilda.esedebe@vaughan.ca)

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2141 Major Mackenzie Dr., Vaughan, ON L6A 1T1  
[vaughan.ca](http://vaughan.ca)



---

**From:** [REDACTED]  
**Sent:** Wednesday, December 2, 2020 9:37 AM  
**To:** Esedebe, Hilda <Hilda.Esedebe@vaughan.ca>  
**Subject:** [External] Extension of Bass Pro Milles Drive

Please add me to the mailing list for the Extension of Bass Pro Milles Drive, to Weston Rd.

Thank You

[REDACTED]

[REDACTED]

[REDACTED]



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## Robinson, Jennifer

---

**From:** Esedebe, Hilda <Hilda.Esedebe@vaughan.ca>  
**Sent:** Thursday, December 3, 2020 9:39 PM  
**To:** [REDACTED]  
**Cc:** Cholewa, Peter; Addley, Diana; Robinson, Jennifer  
**Subject:** RE: Bass Pro Mills Drive

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Hi [REDACTED]

Thank you for your interest in the Bass Pro Mills Environmental Assessment (EA) Study. Your comments have been noted. Plans for the extension of Bass Pro Mills Drive will consider options for pedestrian and cyclist facilities. We are certainly aware of concerns regarding traffic infiltration within the Weston Downs community and these will be considered during this study. A Transportation/Traffic Analysis for the study area will be completed at later stages of the study, once the recommended design/connection to Weston Road is identified. Results will be shared with the public at that time. A preliminary Transportation/Traffic Analysis has been completed and summarized in the online Public Information Centre (PIC), which I hope you've been able to view on the study website ([www.vaughan.ca/basspromillsea](http://www.vaughan.ca/basspromillsea)). The PIC will be available online to view until January 8, 2020.

If you would like to discuss further or be added to the study contact list for future updates, please let me know.

Regards,

**Hilda Esedebe**, P.Eng., MBA, M.Sc.  
**Transportation Project Manager**  
**Infrastructure Planning and Corporate Asset Management**  
905-832-8585, ext. 8484 | [hilda.esedebe@vaughan.ca](mailto:hilda.esedebe@vaughan.ca)

**City of Vaughan | Infrastructure Development**  
2141 Major Mackenzie Dr., Vaughan, ON L6A 1T1  
[vaughan.ca](http://vaughan.ca)



---

**From:** [REDACTED]  
**Sent:** Thursday, December 3, 2020 12:02 PM  
**To:** Esedebe, Hilda <Hilda.Esedebe@vaughan.ca>  
**Subject:** [External] Bass Pro Mills Drive

Hilda

I have been a Vaughan resident for over 10 years and live in Weston Downs. I have travelled Rutherford Road every day to work and very much look forward to a new connection between Weston Road and Jane Street. While Vaughan Mills is not very walkable at present, the proposed Bass Pro Mills extension extension should be, with travel lanes separated from the

sidewalk wherever feasible. There will be a number of concerns expressed about traffic infiltration to Weston Downs and therefore it should ideally not connect with Astona Boulevard. If there is no choice about this, then measures will be needed to prevent further infiltration into Weston Downs.

Good luck with the project!

[Redacted]

[Redacted]

[Redacted]

[Redacted]



## Robinson, Jennifer

---

**From:** Esedebe, Hilda <Hilda.Esedebe@vaughan.ca>  
**Sent:** Friday, December 4, 2020 1:42 PM  
**To:** [REDACTED]  
**Cc:** Cholewa, Peter; Addley, Diana; Robinson, Jennifer  
**Subject:** RE: Bass Pro Mills Dr. EA - Stakeholders Group - Notice of Online PIC

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Hi [REDACTED]

Thank you for your interest in the Bass Pro Mills Environmental Assessment (EA) Study. Your comments have been noted. We are certainly aware of concerns regarding traffic infiltration within the Weston Downs community and these will be considered during this study. A Transportation/Traffic Analysis for the study area will be completed at later stages of the study, once the recommended design/connection to Weston Road is identified. Results will be shared with the public at that time.

If you would like to discuss further, please let me know.

Regards,

Hilda Esedebe, P.Eng., MBA, M.Sc.  
**Transportation Project Manager**  
**Infrastructure Planning and Corporate Asset Management**  
905-832-8585, ext. 8484 | [hilda.esedebe@vaughan.ca](mailto:hilda.esedebe@vaughan.ca)

**City of Vaughan | Infrastructure Development**  
2141 Major Mackenzie Dr., Vaughan, ON L6A 1T1  
[vaughan.ca](http://vaughan.ca)



---

**From:** [REDACTED]  
**Sent:** Friday, December 4, 2020 10:10 AM  
**To:** Esedebe, Hilda <Hilda.Esedebe@vaughan.ca>  
**Subject:** [External] RE: Bass Pro Mills Dr. EA - Stakeholders Group - Notice of Online PIC

Thank you for the presentation, I found it informative.

As a resident of Weston Downs subdivision, I was wondering what measures would be implemented to avoid “pushing” the east west traffic through our neighbourhood? This is a safety concern, as many young children are at play. Thank you.

Regards,

[REDACTED]

[REDACTED]

[REDACTED]

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---

**From:** Esedebe, Hilda <[Hilda.Esedebe@vaughan.ca](mailto:Hilda.Esedebe@vaughan.ca)>

**Sent:** December 3, 2020 2:09 PM

**To:** Hubjer, Selma <[Selma.Hubjer@vaughan.ca](mailto:Selma.Hubjer@vaughan.ca)>; 'Addley, Diana' <[Diana.Addley@stantec.com](mailto:Diana.Addley@stantec.com)>; Cholewa, Peter <[Peter.Cholewa@stantec.com](mailto:Peter.Cholewa@stantec.com)>; [REDACTED]

[REDACTED]

**Subject:** Bass Pro Mills Dr. EA - Stakeholders Group - Notice of Online PIC

Hello all,

Please be advised that the first of two online Public Information Centres for the Bass Pro Mills Environmental Assessment Study is now available at [www.vaughan.ca/basspromillsea](http://www.vaughan.ca/basspromillsea), until January 8, 2020. It includes a self-guided presentation and survey, and we are seeking feedback on the study background, evaluation of alternative solutions, the recommended solution and next steps in the study process. Attached are the PSA and Notice for more information.

If you have any questions or concerns, please contact the undersigned.

Regards,

Hilda Esedebe, P.Eng., MBA, M.Sc.  
Transportation Project Manager  
Infrastructure Planning and Corporate Asset Management  
905-832-8585, ext. 8484 | [hilda.esedebe@vaughan.ca](mailto:hilda.esedebe@vaughan.ca)

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## Robinson, Jennifer

---

**From:** Esedebe, Hilda <Hilda.Esedebe@vaughan.ca>  
**Sent:** Friday, December 11, 2020 8:18 PM  
**To:** [REDACTED]  
**Cc:** Cholewa, Peter; Addley, Diana; Robinson, Jennifer  
**Subject:** RE: [External] BASS PRO MILLS DRIVE EXTENSION

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Hello [REDACTED],

Thank you for your interest in the Bass Pro Mills Environmental Assessment (EA) Study. Your comments have been noted. The Project Team is certainly aware of concerns regarding traffic infiltration within the Weston Downs community and these will be considered during this study. A Transportation/Traffic Analysis for the study area will be completed at later stages of the study, once the recommended design/connection to Weston Road is identified. Results will be shared with the public at that time. A preliminary Transportation/Traffic Analysis has been completed and summarized in the online Public Information Centre (PIC), which I hope you've been able to view on the study website ([www.vaughan.ca/basspromillsea](http://www.vaughan.ca/basspromillsea)). The PIC takes about 30 minutes to review the narrated presentation, followed by a brief survey at the end. **It will be available online to view until January 8, 2020.**

The City and Region has a number of ongoing studies aimed at improving Vaughan's east-west connectivity including this Bass Pro Mills EA, [Kirby Road missing link](#), [Teston Road missing link](#), [Langstaff Road missing link](#) to mention a few. Although Weston Road and Rutherford Road fall under York Region's jurisdiction and Highway 400 falls under the province's jurisdiction, the City works with all parties to preserve the interest of citizens. With my knowledge of Highway 400, there is not enough room for a full interchange at Bass Pro Mills drive, but you may contact the Ontario Ministry of Transportation to confirm. Please note that a full interchange is being proposed at Langstaff Road as part of the study noted above.

If you have any further questions or would like to be added to the study contact list, please let me know.

Regards,

**Hilda Esedebe**, P.Eng., MBA, M.Sc.  
**Transportation Project Manager**  
**Infrastructure Planning and Corporate Asset Management**  
905-832-8585, ext. 8484 | [hilda.esedebe@vaughan.ca](mailto:hilda.esedebe@vaughan.ca)

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[vaughan.ca](http://vaughan.ca)



**From:** [REDACTED]  
**Sent:** Monday, December 7, 2020 12:16 PM  
**To:** Esedebe, Hilda <Hilda.Esedebe@vaughan.ca>  
**Subject:** [External] BASS PRO MILLS DRIVE EXTENSION

Hi,

I just wanted to voice my opinions in regards to this matter.

I have lived in Weston Downs since 1993. During the past 27 years the traffic along Weston Road and Rutherford Road has gotten so bad that cars have started using our subdivision as a shortcut. To extend the Bass Pro Mills Drive extension will make what is a traffic jam into a parking lot.

Why can't you make the Bass Pro Mills Drive a full interchange? The traffic to and from the mall should only use Highway 400. Currently all you have is Highway 400 northbound traffic exiting and then getting back on Highway 400 going southbound. If you make this interchange a full interchange - then only local traffic will leave the mall on Jane Street and Rutherford Road.

I hate Vaughan Mill Mall with a passion. It brings so much traffic to my neighborhood and you are trying to make it worse by trying to extend that overpass towards my subdivision.

[REDACTED]

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## Robinson, Jennifer

---

**From:** Esedebe, Hilda <Hilda.Esedebe@vaughan.ca>  
**Sent:** Tuesday, December 15, 2020 10:21 PM  
**To:** [REDACTED]  
**Cc:** Robinson, Jennifer; Addley, Diana; Cholewa, Peter  
**Subject:** RE: [External] Bass Pro Mills Drive Extension Objection

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Hello [REDACTED],

Thank you for your interest in the Bass Pro Mills Environmental Assessment (EA) Study. Your comments have been noted. The Project Team is certainly aware of concerns regarding traffic infiltration within the Weston Downs community and these will be considered during this study. A Transportation/Traffic Analysis for the study area will be completed at later stages of the study, once the recommended design/connection to Weston Road is identified. Results will be shared with the public at that time. A preliminary Transportation/Traffic Analysis has been completed and summarized in the online Public Information Centre (PIC), which I hope you've been able to view on the study website ([www.vaughan.ca/basspromillsea](http://www.vaughan.ca/basspromillsea)). The PIC takes about 30 minutes to review the narrated presentation, followed by a brief survey at the end. **It will be available online to view until January 8, 2020.**

The City of Vaughan is growing and infrastructure for vehicular traffic, transit and active transportation needs to be in place to support this growth. The City and Region has a number of ongoing studies aimed at growth and improving Vaughan's connectivity including this Bass Pro Mills EA, [Kirby Road missing link](#), [Teston Road missing link](#), [Langstaff Road missing link](#) to mention a few. York Region also has plans to widen Rutherford Road and Weston Road by 2041 (see the [Transportation Master Plan | York Region](#)). Although Weston Road, Rutherford Road and Pine Valley Drive fall under York Region's jurisdiction and Highway 400 falls under the province's jurisdiction, the City works with all tiers of government to preserve the interest of citizens while reviewing multiple solutions to address growth and transportation needs. It is also important to understand that the project team is reviewing feedback from all stakeholders to inform the study to the best extent possible.

If you have any further questions or would like to be added to the study contact list, please let me know.

Regards,

Hilda Esedebe, P.Eng., MBA, M.Sc.  
Transportation Project Manager  
Infrastructure Planning and Corporate Asset Management  
905-832-8585, ext. 8484 | [hilda.esedebe@vaughan.ca](mailto:hilda.esedebe@vaughan.ca)

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-----Original Message-----

From: [REDACTED]  
Sent: Sunday, December 13, 2020 6:50 PM

To: Esedebe, Hilda <Hilda.Esedebe@vaughan.ca>; diana.addley@stantec.com

Subject: [External] Bass Pro Mills Drive Extension Objection

My name is [REDACTED] and I am a resident of Vaughan who lives at [REDACTED].  
I am writing to express my concern and objection regarding the Bass Pro Mills Drive Extension.

I am in objection to the proposed extension of Bass Pro Mills Drive to Weston Road. Weston Downs already deals with heavy vehicle infiltration on a daily basis as Weston Road is heavily trafficked. Vehicles already use Velmar Drive as a bypass to avoid the backlog of cars trying to go west on Rutherford Road from Weston Road since there is no other north/south road to connect Langstaff Road and Rutherford Road. Instead the City should take back the land they gave to the TRCA under eminent domain and complete the Pine Valley extension.

Furthermore, extending Bass Pro Mills Road will only exacerbate the congestion we face on a daily basis in Weston Downs and on Velmar Drive. Instead of this expansion, the City of Vaughan should widen Rutherford Road west of Weston Road to 3 lanes in either direction to alleviate the bottleneck at this intersection instead of encouraging infiltration into our once quiet subdivision along Weston Road, into Astona Blvd and then to Village Green Drive to Velmar Drive.

Please do the right thing and think of proper traffic mitigation measures that will properly serve this area for the long term and not this poorly hastened stop gap measure that puts the traffic burden onto small subdivision streets instead of keeping vehicular traffic on Regional Roads. Shame on the City planners for even given thought to such a poor solution. Do not put the traffic problems from one area onto another that was never designed to deal with it.

I can be reached at [REDACTED] if you have any further questions or would like to discuss the matter further. Thank you.

[REDACTED]

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## Robinson, Jennifer

---

**To:** Esedebe, Hilda  
**Subject:** RE: Bas Pro Mills Drive Extension

---

**From:** Coles, Todd <[Todd.Coles@vaughan.ca](mailto:Todd.Coles@vaughan.ca)>  
**Sent:** Thursday, December 17, 2020 5:08 PM  
**To:** Esedebe, Hilda <[Hilda.Esedebe@vaughan.ca](mailto:Hilda.Esedebe@vaughan.ca)>  
**Subject:** RE: Bas Pro Mills Drive Extension

Hilda,

He used our conversation to address many of his City-related concerns. It was a wide ranging conversation. He was aware that your contact number was included in the newspaper ad. If you would like to reach out to him, his number is [REDACTED], although he did not request a call back from you.

Thanks,

Todd

**Todd Coles**, BES, ACST(A), MCIP, RPP  
**City Clerk**  
905-832-8585, ext. 8281 | [todd.coles@vaughan.ca](mailto:todd.coles@vaughan.ca)

**City of Vaughan | Office of the City Clerk**  
2141 Major Mackenzie Dr., Vaughan ON L6A 1T1  
[vaughan.ca](http://vaughan.ca)



---

**From:** Esedebe, Hilda <[Hilda.Esedebe@vaughan.ca](mailto:Hilda.Esedebe@vaughan.ca)>  
**Sent:** December 17, 2020 5:03 PM  
**To:** Coles, Todd <[Todd.Coles@vaughan.ca](mailto:Todd.Coles@vaughan.ca)>  
**Subject:** RE: Bas Pro Mills Drive Extension

Hi Todd,

Thank you for this. He is free to call me or the consultants at any time. Our contact information is placed on all communications regarding the project, including the newspaper ads, direct mailouts to residents in the area and of course on the study website.

If he'd like to contact us, we'd be happy to walk him through the study or mail him the online PIC materials for his review.

We're in the middle of a pandemic so in-person meetings are not an option at this time.

If you have his name and contact info, that would help as this is needed to register his support for the study. We're happy to hear from all stakeholders.



Regards,

**Hilda Esedebe**, P.Eng., MBA, M.Sc.  
**Transportation Project Manager**  
**Infrastructure Planning and Corporate Asset Management**  
905-832-8585, ext. 8484 | [hilda.esedebe@vaughan.ca](mailto:hilda.esedebe@vaughan.ca)

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---

**From:** Coles, Todd <[Todd.Coles@vaughan.ca](mailto:Todd.Coles@vaughan.ca)>  
**Sent:** Thursday, December 17, 2020 4:57 PM  
**To:** Esedebe, Hilda <[Hilda.Esedebe@vaughan.ca](mailto:Hilda.Esedebe@vaughan.ca)>  
**Subject:** Bas Pro Mills Drive Extension

Hilda,

I had a long conversation today with a [REDACTED]. He was expressing his dismay at the City's electronic-participation methods for our Ward Boundary Review and the Bass Pro Mills Drive Extension Environmental Assessment, as he does not have internet access. He did ask me to pass along to you his full support for the Bas Pro Mills Drive extension. I trust you can register his support for the project.

Thanks,

Todd

**Todd Coles**, BES, ACST(A), MCIP, RPP  
**City Clerk**  
905-832-8585, ext. 8281 | [todd.coles@vaughan.ca](mailto:todd.coles@vaughan.ca)

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## Robinson, Jennifer

---

**From:** [REDACTED]  
**Sent:** Tuesday, December 29, 2020 10:30 AM  
**To:** Esedebe, Hilda  
**Cc:** Addley, Diana; Cholewa, Peter; Robinson, Jennifer  
**Subject:** Re: FW: Voice Message From: (416) 6683768 (Bass Pro Mills EA)

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Sorry fir the slow response.. thx for your note .. I will review this and get back to you

On Fri, Dec 11, 2020 at 8:01 PM Esedebe, Hilda <[Hilda.Esedebe@vaughan.ca](mailto:Hilda.Esedebe@vaughan.ca)> wrote:

Hello [REDACTED],

Thank you for the comments/questions you provided during our call on Monday December 7<sup>th</sup>, 2020. The following is a summary for documentation purposes:

Concerns/comments:

- Traffic infiltration in the Weston Down residential area
- Congestion on Weston Road
- Congestion on Rutherford Road
- Future traffic access for the Rutherford GO station
- Study Process
- Participation during the study
- Study next steps

Response:

Thank you for your interest in the Bass Pro Mills Environmental Assessment (EA) Study. Your comments have been noted. The Project Team is certainly aware of concerns regarding traffic infiltration within the Weston Downs community and these will be considered during this study. A Transportation/Traffic Analysis for the study area will be completed at later stages of the study, once the recommeded design/connection to Weston Road is identified. Results will be shared with the public at that time. A preliminary Transportation/Traffic Analysis has been completed and summarized in the online Public Information Centre (PIC), which I hope you've been able to view on the study website ([www.vaughan.ca/basspromillsea](http://www.vaughan.ca/basspromillsea)). The PIC takes about 30 minutes to review the narrated presentation, followed by a brief survey at the end. **It will be available online to view until January 8, 2020.**

The City and Region has a number of ongoing studies aimed at improving Vaughan's east-west connectivity including this Bass Pro Mills EA, [Kirby Road missing link](#), [Teston Road missing link](#), [Langstaff Road missing link](#) to mention a few. As described in the online PIC, Vaughan is growing and new infrastructure is needed to keep up. The Province provides projections for population and employment which are used for Regional and City planning initiatives. Starting from high level planning, the studies get more refined until we reach the EA stage, then eventually detailed design and construction. Timelines are subject to funding and approvals.

Although Weston Road and Rutherford Road fall under York Region's jurisdiction and the GO stations fall under Metrolinx' jurisdiction, the City works with all parties to preserve the interest of citizens.

In an effort to engage as many stakeholders as possible for the study, notices were sent at the study commencement in March 2020 which included invitations to participate in the study Stakeholders Group (SG) and/or to be added to the study contact list. The SG were consulted in September before online PIC 1. The Project Team plans to consult with the SG and public at key milestones during the study. **You have indicated that you would like to be added to the study contact list and included in the SG group.**

The study's next steps are outline in the online PIC including developing and evaluating alternative design concepts and identifying the recommended design. There will be a round of consultation afterwards, likely by Spring 2021. Followed by completing the preliminary design and filing the Environmental Study Report (ESR).

If you have any further questions, please let me know.

Regards,

**Hilda Esedebe**, P.Eng., MBA, M.Sc.

**Transportation Project Manager**

**Infrastructure Planning and Corporate Asset Management**  
905-832-8585, ext. 8484 | [hilda.esedebe@vaughan.ca](mailto:hilda.esedebe@vaughan.ca)

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[vaughan.ca](http://vaughan.ca)



---

**From:** [REDACTED]  
**Sent:** Monday, December 7, 2020 5:16 PM  
**To:** Hilda Esedebe [REDACTED]  
**Subject:** Voice Message From: (416) 6683768

Mark Read

**From:** [REDACTED]  
**Date/Time:** 2020/12/07 17:15  
[Voice Message \(50.0 s\)](#)

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--

[REDACTED]

[REDACTED]

## Robinson, Jennifer

---

**From:** Esedebe, Hilda <Hilda.Esedebe@vaughan.ca>  
**Sent:** Thursday, January 7, 2021 4:21 PM  
**To:** [REDACTED]  
**Cc:** Addley, Diana; Robinson, Jennifer; Cholewa, Peter  
**Subject:** FW: Voice Message From: [REDACTED] - Bass Pro Mills EA  
**Attachments:** VoiceMsg[ID=20000000193112 G=40 F=300 A=BA42087E-46F2-4057-96F9-DDB7E741BF9D C=1 CID=[REDACTED].mp3

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Hi [REDACTED],

Thank you for your voicemail and for taking my call today. Also, thank you for your interest in the Bass Pro Mills Environmental Assessment (EA) Study. Your comments have been noted. The Project Team is certainly aware of concerns regarding traffic infiltration within the Weston Downs community and these will be considered during this study. A Transportation/Traffic Analysis for the study area will be completed at later stages of the study, once the recommended design/connection to Weston Road is identified. Results will be shared with the public at that time. A preliminary Transportation/Traffic Analysis has been completed and summarized in the online Public Information Centre (PIC), which I hope you've been able to view on the study website ([www.vaughan.ca/basspromillsea](http://www.vaughan.ca/basspromillsea)). The PIC takes about 30 minutes to review the narrated presentation, followed by a brief survey at the end. **It will be available online to view until January 8, 2020.**

The City of Vaughan is growing and infrastructure for vehicular traffic, transit and active transportation needs to be in place to support this growth. The City and Region has a number of ongoing studies aimed at growth and improving Vaughan's connectivity including this Bass Pro Mills EA, [Kirby Road missing link](#), [Teston Road missing link](#), [Langstaff Road missing link](#) to mention a few. York Region also has plans to widen Rutherford Road and Weston Road by 2041 (see the [Transportation Master Plan | York Region](#)). Although Weston Road, Rutherford Road and Pine Valley Drive fall under York Region's jurisdiction and Highway 400 falls under the province's jurisdiction, the City works with all tiers of government to preserve the interest of citizens while reviewing multiple solutions to address growth and transportation needs. Regarding regional projects you may email [roads@york.ca](mailto:roads@york.ca) to express your concerns or visit the study websites. Please also note that the [Vaughan Mills Center Secondary Plan 2014](#) is different from the Bass Pro Mills EA and outcomes are not dictated by the Secondary Plan. It is also important to understand that the project team is reviewing feedback from all stakeholders to inform the study to the best extent possible.

Your will be added to the study contact list as discussed.

If you have any further questions, please let me know.

Regards,

Hilda Esedebe, P.Eng., MBA, M.Sc.  
**Transportation Project Manager**  
**Infrastructure Planning and Corporate Asset Management**  
905-832-8585, ext. 8484 | [hilda.esedebe@vaughan.ca](mailto:hilda.esedebe@vaughan.ca)

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[vaughan.ca](http://vaughan.ca)



---

**From:** [REDACTED]  
**Sent:** Tuesday, December 29, 2020 1:25 PM  
**To:** Hilda Esedebe [REDACTED]  
**Subject:** Voice Message From: [REDACTED]

Mark Read

**From:** [REDACTED]  
**Date/Time:** 2020/12/29 13:25

[Voice Message \(39.0 s\)](#)

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## Robinson, Jennifer

---

**From:** Esedebe, Hilda <Hilda.Esedebe@vaughan.ca>  
**Sent:** Friday, January 15, 2021 8:12 PM  
**To:** [REDACTED]  
**Cc:** [REDACTED] DeFrancesca, Rosanna; Addley, Diana; Cholewa, Peter; Robinson, Jennifer  
**Subject:** RE: [External] FW: CONNECTION FROM VAUGHAN MILLS TO WESTON ROAD AT ASTONIA

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Hello [REDACTED],

Thank you for your email and for your participation in the online Public Information Centre (PIC) for the Bass Pro Mills Environmental Assessment Study, which closed on January 8, 2021.

We appreciate feedback from the public and your comments have been noted. The Project Team is certainly aware of concerns regarding traffic infiltration within the Weston Downs community and these will be considered during this study. A Transportation/Traffic Analysis for the study area will be completed at later stages of the study, once the recommended design/connection to Weston Road is identified. Results will be shared with the public at that time.

The City of Vaughan is growing and infrastructure for vehicular traffic, transit and active transportation needs to be in place to support this growth. The City and Region has a number of ongoing studies aimed at growth and improving Vaughan's connectivity including this [Bass Pro Mills EA](#), [Kirby Road missing link](#), [Teston Road missing link](#), [Langstaff Road missing link](#) to mention a few. York Region also has plans to widen Rutherford Road and Weston Road by 2041 (see the [Transportation Master Plan | York Region](#)). Although Weston Road and Rutherford Road fall under York Region's jurisdiction, the City works with all tiers of government to preserve the interest of citizens as best as possible, while reviewing multiple solutions to address growth and transportation needs. Regarding regional projects you may email [roads@york.ca](mailto:roads@york.ca) to express your concerns or visit the study websites. It is also important to understand that the project team is reviewing feedback from all stakeholders to inform the study to the best extent possible.

If you wish to be added to the study mailing list, please let me know.

Regards,

Hilda Esedebe, P.Eng., MBA, M.Sc.  
Transportation Project Manager  
Infrastructure Planning and Corporate Asset Management  
905-832-8585, ext. 8484 | [hilda.esedebe@vaughan.ca](mailto:hilda.esedebe@vaughan.ca)

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[vaughan.ca](http://vaughan.ca)



---

**From:** DeFrancesca, Rosanna <Rosanna.DeFrancesca@vaughan.ca>  
**Sent:** Tuesday, January 12, 2021 3:20 PM

**To:** [REDACTED]  
**Cc:** Esedebe, Hilda <Hilda.Esedebe@vaughan.ca>; Tamburini, Nancy <Nancy.Tamburini@vaughan.ca>; Ward 3 Support Staff <ssward3@vaughan.ca>  
**Subject:** Fwd: [External] FW: CONNECTION FROM VAUGHAN MILLS TO WESTON ROAD AT ASTONIA

Hi [REDACTED]  
Happy New Year to you as well.

Thank you for sharing your observations and concerns in respect to traffic being redirected to Weston Rd and subsequently through Weston Downs.

By copy of this email I am sharing your comments with our Transportation Planning staff.

Thank you again and please let me know if you have any further questions.

Stay safe.  
Rosanna DeFrancesca Councillor  
Ward 3, Woodbridge/Vellore  
City of Vaughan

**From:** [REDACTED]  
**Date:** January 8, 2021 at 8:42:40 AM EST  
**To:** "DeFrancesca, Rosanna" <[Rosanna.DeFrancesca@vaughan.ca](mailto:Rosanna.DeFrancesca@vaughan.ca)>  
**Subject:** [External] FW: CONNECTION FROM VAUGHAN MILLS TO WESTON ROAD AT ASTONIA

Good morning Councillor DeFrancesca,

I have seen the information online regarding the extension of Bass Pro Mills for January 8, 2021.

Notwithstanding the goals and objectives presented, based on the traffic that currently exists (covid has reduced some traffic) in the area west of Highway 400, this extension although it wants to move traffic east/west, it appears that the extension will compound the problem on Weston Road and roads through Weston Downs.

[REDACTED]

---

**From:** [REDACTED]  
**Sent:** Tuesday, January 5, 2021 9:58 AM  
**To:** DeFrancesca, Rosanna <[Rosanna.DeFrancesca@vaughan.ca](mailto:Rosanna.DeFrancesca@vaughan.ca)>  
**Subject:** CONNECTION FROM VAUGHAN MILLS TO WESTON ROAD AT ASTONIA

Good morning Councillor DeFrancesca

Happy New Year!

Hope all is well and you are safe.



Regarding the above subject. I heard that the City of Vaughan has sent out a notice regarding this proposed road alignment. Is this correct?

Not having seen the notice (IF THERE IS ONE) and not having reviewed and traffic reports, this connection will further impact the traffic along Weston and into Weston Downs by way of a shortcut to Rutherford Road.

Please advise if there are any plans for this.



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## Robinson, Jennifer

---

**From:** Esedebe, Hilda <Hilda.Esedebe@vaughan.ca>  
**Sent:** Wednesday, January 6, 2021 8:11 PM  
**To:** [REDACTED]  
**Cc:** [REDACTED]; Addley, Diana; Cholewa, Peter; Robinson, Jennifer [REDACTED]  
**Subject:** RE: [External] City of Vaughan opinion BEFORE Community Input  
**Attachments:** Bass Pro Mills EA-WDRA Mtg 1-Response Memo-Nov 11 2020.pdf

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Hi [REDACTED],

Thanks again for your email. To reiterate, the online Public Information Center (PIC) has been available from December 3<sup>rd</sup>, 2020 and will close on January 8<sup>th</sup>, 2021. Ample feedback from the public has already been received which will be reviewed and used to inform the study.

I hope you and your colleagues have been able to review the attached memo which was initially sent to representatives of the Weston Downs Rate Payer's Association on November 11, 2020 following the meeting on November 6, 2020 with the project team.

Please note that a detailed Transportation/Traffic Analysis for the study area will be completed at later stages of the study, once the recommended design/connection to Weston Road is identified. Results will be shared with stakeholders at that time, followed by the second round of public consultation. A preliminary Transportation/Traffic Analysis has been completed and summarized in the online Public Information Centre (PIC), which I hope you've been able to view on the study website ([www.vaughan.ca/basspromillsea](http://www.vaughan.ca/basspromillsea)). The PIC takes about 30 minutes to review the narrated presentation, followed by a brief survey at the end.

Included in both the memo and online PIC is information regarding the City of Vaughan's growth, and infrastructure for vehicular traffic, transit and active transportation that needs to be in place to support this growth. The City and Region has a number of ongoing studies aimed at growth and improving Vaughan's connectivity including this Bass Pro Mills EA, [Kirby Road missing link](#), [Teston Road missing link](#), [Langstaff Road missing link](#) to mention a few. York Region also has plans to widen Rutherford Road and Weston Road by 2041 (see the [Transportation Master Plan | York Region](#)). Although most of the roadways in the area including Weston Road, Rutherford Road and Pine Valley Drive fall under York Region's jurisdiction and Highway 400 falls under the province's jurisdiction, the City works with all tiers of government to preserve the interest of citizens while reviewing multiple solutions to address growth and transportation needs. It is also important to understand that the project team is reviewing feedback from all stakeholders to inform the study to the best extent possible.

Regards,

Hilda Esedebe, P.Eng., MBA, M.Sc.  
Transportation Project Manager  
Infrastructure Planning and Corporate Asset Management  
905-832-8585, ext. 8484 | [hilda.esedebe@vaughan.ca](mailto:hilda.esedebe@vaughan.ca)

City of Vaughan | Infrastructure Development  
2141 Major Mackenzie Dr., Vaughan, ON L6A 1T1  
[vaughan.ca](http://vaughan.ca)



---

**From:** [REDACTED] >  
**Sent:** Wednesday, January 6, 2021 8:22 AM  
**To:** Esedebe, Hilda <Hilda.Esedebe@vaughan.ca>; Coles, Todd <Todd.Coles@vaughan.ca>  
**Cc:** [REDACTED] Addley, Diana <Diana.Addley@stantec.com>; Cholewa, Peter <Peter.Cholewa@stantec.com>; Robinson, Jennifer <Jennifer.Robinson@stantec.com>  
**Subject:** Re: [External] City of Vaughan opinion BEFORE Community Input

Hilda & Todd,

Due to increased health issues due to Covid high numbers reported in Vaughan, the Lockdown during this timeframe, the conflict of the holiday season, the significant traffic impact to the community and the future direction changes to develop high rise condos to replace our current Vaughan Mills Shopping Mall are all important and significant reasons to respectfully delay this survey.

In addition, the recent notification to ask for feedback included a decision already made which was not accurate; hence my reaching out to the City to ensure that the Vaughan Community and taxpayers are not misled and are respected.

I'll be available today to discuss further.

Thank you,

[REDACTED]

On Jan 5, 2021, at 8:38 PM, Esedebe, Hilda <[Hilda.Esedebe@vaughan.ca](mailto:Hilda.Esedebe@vaughan.ca)> wrote:

As a reminder, the online Public Information Centre which launched on December 3, 2020, will be available until this Friday January 8, 2021.

**Hilda Esedebe**, P.Eng., MBA, M.Sc.  
**Transportation Project Manager**  
**Infrastructure Planning and Corporate Asset Management**  
905-832-8585, ext. 8484 | [hilda.esedebe@vaughan.ca](mailto:hilda.esedebe@vaughan.ca)

**City of Vaughan | Infrastructure Development**  
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[vaughan.ca](http://vaughan.ca)  
<image001.png>

---

**From:** Esedebe, Hilda  
**Sent:** Tuesday, January 5, 2021 8:31 PM  
**To:** [REDACTED]  
**Cc:** [REDACTED]  
Addley, Diana <[Diana.Addley@stantec.com](mailto:Diana.Addley@stantec.com)>; 'Cholewa, Peter' <[Peter.Cholewa@stantec.com](mailto:Peter.Cholewa@stantec.com)>;

Robinson, Jennifer <[Jennifer.Robinson@stantec.com](mailto:Jennifer.Robinson@stantec.com)>

**Subject:** FW: [External] City of Vaughan opinion BEFORE Community Input

Hi [REDACTED]

Thank you for contacting the City of Vaughan. We value the feedback of our engaged residents and community organizations. The Bass Pro Mills Drive Extension is a Council-approved project. We encourage you to provide your thoughts regarding the Municipal Class Environmental Assessment and this city-building initiative on the project's website. For more information and to take part in the Public Information Centre, please visit [vaughan.ca/BassProMillsEA](http://vaughan.ca/BassProMillsEA).

The project team met with members of the Weston Down Rate Payer's Association (WDRA) on September 14 and November 6, 2020. A memo was prepared in response to concerns shared and sent to the WDRA on November 11, 2020 (memo is attached for convenience). During each of those meetings, the WDRA was advised of the upcoming public consultation and overall project schedule. The WDRA remains part of the study's Stakeholders Group that will continue to meet at key study milestone's and be part of the consultation as the study progresses.

Regards,

**Hilda Esedebe**, P.Eng., MBA, M.Sc.  
**Transportation Project Manager**  
**Infrastructure Planning and Corporate Asset Management**  
905-832-8585, ext. 8484 | [hilda.esedebe@vaughan.ca](mailto:hilda.esedebe@vaughan.ca)

**City of Vaughan | Infrastructure Development**  
2141 Major Mackenzie Dr., Vaughan, ON L6A 1T1  
[vaughan.ca](http://vaughan.ca)  
<image001.png>

---

**From:** [REDACTED]  
**Sent:** December 28, 2020 10:11 AM  
**To:** Coles, Todd <[Todd.Coles@vaughan.ca](mailto:Todd.Coles@vaughan.ca)>  
**Cc:** [REDACTED]  
**Subject:** [External] City of Vaughan opinion BEFORE Community Input

Hi Todd,

This is a formal complaint that the City of Vaughan is communicating their views to the greater community at large without securing the local tax payers ( community affected by this extra traffic).

<https://www.instagram.com/p/CJWB6tDgFqT/?igshid=1qnu7cg7oekvd>

[REDACTED]

This e-mail, including any attachment(s), may be confidential and is intended solely for the attention and information of the named addressee(s). If you are not the intended recipient or have received this message in error, please notify me immediately by return e-mail and permanently delete the original transmission from your computer, including any attachment(s). Any unauthorized distribution, disclosure or copying of this message and attachment(s) by anyone other than the recipient is strictly prohibited.

<Bass Pro Mills EA-WDRA Mtg 1-Response Memo-Nov 11 2020.pdf>

**DATE:** November 11, 2020

**TO:** Weston Downs Rate Payers Association (WDRA) – Co-presidents:  
[REDACTED]

**FROM:** Hilda Esedebe, Transportation Project Manager, Infrastructure Planning and Corporate Asset Management (IPCAM), City of Vaughan

**RE:** **Bass Pro Mills Municipal Class Environmental Assessment (MCEA) Study**

The purpose of this memorandum is to provide a response to questions/concerns noted during the November 6<sup>th</sup>, 2020 virtual meeting between City staff, the consultants (Stantec) and the WDRA, regarding the Bass Pro Mills MCEA study. It is hoped that this memo is able to shed light on the study process, the need and justification for the proposed extension of Bass Pro Mills Drive from Highway 400 to Weston Road, and help facilitate professional and meaningful dialogue going forward.

### **Background and Purpose**

The 2020 Provincial Policy Statement (PPS) is the guiding document that provides overall policy direction on matters of provincial interest relating to land use planning and development in Ontario. Local Official Plans are the primary mechanism for implementing the policies of the Official Plan, and in accordance with the Planning Act, decisions affecting planning matters shall be “consistent with” the PPS. The PPS includes additional policy direction for municipalities with respect to economic development and land supply. In the context of the Bass Pro Mills MCEA study, additional policy guidance is provided to promote the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimizing transit investments and standards to minimize land consumption and servicing costs.

The Bass Pro Mills Drive Extension MCEA supports matters of provincial interest as it aims to create an efficient intermodal transportation system that safely facilitates the movement of people and goods. The extension of Bass Pro Mills Drive also promotes economic development by ensuring necessary road infrastructure is provided to allow access to the planned employment area on the west side of Hwy 400, which will assist in addressing the projected employment needs of both the City and Region. This MCEA study and other city- and region-led transportation demand management strategies help to support efficient land development patterns within the City and the surrounding area, as well as address current and projected needs. In this regard, the extension of Bass Pro Mills west of Highway 400 would create a continuous mid-block crossing between Jane Street and Weston Road as Highway 400 creates a barrier to east west traffic. The extension of Bass Pro Mills will redistribute existing and future traffic, alleviate congestion on adjacent major arterial roadways, and will help to sustain an overall viable transportation network within the growing city and region.

In addition to the PPS, the Growth Plan for the Greater Golden Horseshoe (GPGGH) was updated in 2019 to assist in the expansion of economic opportunities for the growing number of people expected to reside within the GGH over the next 20 years. It is a goal of the GPGGH to develop an integrated transportation network that will allow people choices for easy travel both

within and between urban centres throughout the region. Specifically, the transportation system will offer a balance of transportation choices that reduces reliance upon the automobile and provides multimodal access to jobs. As land development and transportation are interconnected, areas with high employment densities shall be planned to be better connected to transit and vacant/underutilized employment lands shall be used more effectively and efficiently. The Bass Pro Mills Drive MCEA study will support the objectives within the GPGGH through the identification of an efficient transportation network to support growth and development within the Secondary Plan area.

The following documents were also prepared to provide a framework for land use and transportation planning in the City and Region, and are also being considered as part of the MCEA study:

- **The City of Vaughan Transportation Master Plan (2012)**, which identified key transportation issues for the City at the time, such as an auto-oriented urban structure. However, the City has become one of the fastest-growing municipalities in Canada and major Urban Growth Centers within the GGH. To address this growth, the City is undertaking many strategic Master Plan updates, including updates to the City of Vaughan Transportation Master Plan (TMP). The update to the TMP aims to support current and future residents, businesses, and visitors by providing high-quality, attractive, and sustainable travel choices. The existing TMP refers to the presence of discontinuities in the existing road grid, impacting the efficiency of travel within the City. As part of the TMP, the missing Bass Pro Mills Drive connection to the collector road network west of Highway 400 was identified as a major gap in the grid network.
- **The York Region Official Plan (2010)** guides the economic, environmental and community building decisions that assist in the coordination and management of growth within the Region. The York Region Official Plan (YROP) promotes city building with a focus on regional centres and corridors, including innovated urban designs and green building. The study area for this MCEA is designated as an Urban Area within the Regional structure, with a regional street (i.e. Weston Road) and highway (Highway 400) running along the eastern and western boundaries of the study area. The regional street network is composed of a system of urban and rural streets, with more north-south connections than east-west connections. Thus, it is a goal for the Region to investigate establishing continuous alternative east-west corridors to alleviate traffic congestion. While not designated within the regional street network, the extension of Bass Pro Mills Drive will provide an east-west connection to existing regional roads, such as Weston Road, allowing for a more efficient flow of people and goods throughout the urban and rural transportation system.
- **The City of Vaughan Official Plan (2010)** is part of the overall Growth Management Strategy that will shape the future of the City and guide its continued transformation into a vibrant, beautiful and sustainable City. A main goal of the Official Plan is to create a sustainable transportation network that compliments sustainable development, recognizing that land use and transportation are inextricably linked. Within the Official Plan, the study area for this MCEA is comprised of Prestige and General Employment lands. The existing Bass Pro Mills Drive is designated as a Major Collector street and the proposed extension is shown within the Official Plan's Schedule 9 as a proposed new Major Collector Road Link. As such, the proposed extension shall be planned to

carry moderate traffic volumes, continuously facilitate efficient traffic flow and provide effective routing for transit vehicles, bicycles and pedestrians. The incorporation of the proposed Bass Pro Mills Drive extension within the broader transportation network will assist in supporting the City's goal of achieving a connected and continuous grid-like street network, with multiple connections to collector and arterial streets.

Generally speaking, Master Plans, Official Plans and Secondary Plans are high-level reviews to help meet provincial, regional and city objectives. MCEAs are usually derived from recommendations within these high-level reviews/studies, with more focus and detail, before advancement to detailed design and construction as required.

In addition, the City and Region are currently undertaking or have undertaken a number of other transportation studies/projects around the study area including:

- Weston Downs Traffic Study (2016)
- Vaughan Mills Centre Secondary Plan (2014)
- Vaughan Mills Centre Public Realm Strategy and Streetscape Master Plan (ongoing)
- Weston Road Widening Design and Construction (ongoing)
- Langstaff Road Environmental Assessment (ongoing)

A list of transportation planning initiatives that have been or are currently being undertaken by the City and Region are provided at the following links:

- [https://www.vaughan.ca/projects/projects\\_and\\_studies/environmental\\_assessment\\_studies/Pages/default.aspx](https://www.vaughan.ca/projects/projects_and_studies/environmental_assessment_studies/Pages/default.aspx) (City)
- [https://www.york.ca/wps/portal/yorkhome/transportation/yr/environmentalassessmentstudy!/ut/p/z0/fU5LDolwFDyLC5bmNcQP24YYAUNwid2QqhUq0EL7QHt7CwdwNzOZHZAo gSk-y5qj1Ip3nt\\_YoUrpOU2SC8mKXRQTSggahceInK57yID9N\\_gG-R5HRoE9tELxRSidqVasMCBOM9YTixKnVWh0LwKChis7aIPrk4AINUujVe8tvOPWCms XbHF6umUjNHmc18AGjs1WqpeG8m9kaNndfejmB-gp8N0!](https://www.york.ca/wps/portal/yorkhome/transportation/yr/environmentalassessmentstudy!/ut/p/z0/fU5LDolwFDyLC5bmNcQP24YYAUNwid2QqhUq0EL7QHt7CwdwNzOZHZAo gSk-y5qj1Ip3nt_YoUrpOU2SC8mKXRQTSggahceInK57yID9N_gG-R5HRoE9tELxRSidqVasMCBOM9YTixKnVWh0LwKChis7aIPrk4AINUujVe8tvOPWCms XbHF6umUjNHmc18AGjs1WqpeG8m9kaNndfejmB-gp8N0!/) (Region)

### **Existing and Future Traffic Conditions**

In response to your questions/concerns regarding traffic flows within the study area, please refer to the attached figures (attachments 1 to 4), which help demonstrate the traffic volumes for the existing 2020 AM and PM peak, and future 2031 AM and PM peak hours. As illustrated in the attached, a significant increase in study area traffic volumes is expected by 2031. To elaborate, a V/C (Volume/Capacity) ratio in excess of 1 means the volumes exceed the capacity of the roadway. Traffic in the area includes thousands of trips generated by residents going to work and other destinations within and outside Vaughan, trips from outside Vaughan coming into Vaughan to work and other destinations, trips to/from the Vaughan Mills Centre (and surrounding businesses) which is a key destination for the City, trips between regions (north-south and east-west), future trips generated to/from the planned employment areas west of Highway 400 and much more.

### **MCEA Process and Consultation**

The City of Vaughan values the voice of its citizens and is dedicated to promoting an open dialogue with the community. As such, the City will engage and collaborate with stakeholders



and the public at all key study milestones to effectively share information, including the decision-making process, for the purpose of improving transportation choices and creating a more sustainable and transportation-oriented future. The following table reflects the methods in which different interested parties have been and will continue to be engaged throughout the study:

<b>General Public</b>	<ul style="list-style-type: none"> <li>• Direct mailing to those included/requested to be included on the contact list, including email notifications (if provided)</li> <li>• All notifications posted in local newspapers</li> <li>• Mail/Email project information as requested</li> </ul>
<b>Study Area Business Owners/Operators/Tenants</b>	<ul style="list-style-type: none"> <li>• Direct mailing of all notifications and email notifications (if provided)</li> <li>• Invitation to participate in the Stakeholder Group included with Notice of Study Commencement package</li> <li>• Hold individual meetings as necessary</li> </ul>
<b>Study Area Property Owners/Tenants, Developers, Local Interest Groups</b>	<ul style="list-style-type: none"> <li>• Direct mailing of notifications and email notification (where available)</li> <li>• Invitation to participate in the Stakeholder Group included with Notice of Study Commencement package</li> <li>• Hold individual meetings as necessary</li> </ul>
<b>Properties within/in proximity to the Study Area</b>	<ul style="list-style-type: none"> <li>• Canada Post Ad Mail Drop of study notifications (Approximately 4,949 locations, including properties within 200m west of Weston Road)</li> </ul>

Thus far, stakeholders and the public have been engaged through the following:

- Distribution of the Notice of Study Commencement and Invitation to Participate in the Stakeholder Group
- Posting study information on the dedicated project webpage provided on the City's website ([www.vaughan.ca/basspromillsea](http://www.vaughan.ca/basspromillsea))
- Communications with members of the study team, including email and telephone correspondence
- Stakeholder Group Meeting (first one held on September 14, 2020 which was attended by some members of the WDRA – presentation is attachment 5)

Slide 5 of the presentation in attachment 5, gives an overview of the MCEA process. As the study is still within its preliminary stages, there are a number of consultation events still to take place, each of which the WDRA will be notified in advance of, including but not necessarily limited to:

- Public Information Centre 1 – study background, existing conditions, preliminary traffic analysis, problems and opportunities, needs and justification, recommended solution etc. (tentatively scheduled for Winter 2020)
- Public Information Centre 2 – development and evaluation of alternative designs, identification of the preferred design etc. (tentatively scheduled for Spring 2021)
- Stakeholder Group Meetings (to be scheduled in coordination with key study milestones)
- Environmental Study Report 30-day Public Review Period (upon Notice of Study Completion)



It should be noted that all stakeholders from across the City and study area are encouraged to contact the study team at any time during the course of the study to discuss their concerns and/or ask questions in relation to this study. The Project Team is tasked with balancing the input to the study from all perspectives and stakeholders.

The City has noted the WDRA's concern that not all residents will have the means necessary to participate fully in an online consultation event. However, due to COVID-19 and associated physical distancing requirements, provincial and municipal project planning meetings are held exclusively online. This online PIC method has been adopted by other Municipalities and Provincial agencies with a high level of participation, including those located within rural communities. As such, this study will be proceeding in accordance with the City of Vaughan's COVID-19 protocol and social practices for public engagement during the pandemic. The Project Team is happy to hear from residents who wish to have hard copies of presentations or would like to review over a phone call or other communications as appropriate.

We thank you again for the comments made by WDRA during the November 6, 2020 meeting. We look forward to continuing to exchange information with you and other key stakeholders as we move forward with project planning. Should you have any further questions or concerns regarding the Bass Pro Mills MCEA, please do not hesitate to contact the Project Team.

Attachments: 1) PM Peak 2020 (Existing)  
2) AM Peak 2020 (Existing)  
3) PM Peak 2031 (Future)  
4) AM Peak 2031 (Future)  
5) Stakeholders Group Meeting Presentation September 14, 2020

Copy: Selma Hubjer, Manager, Transportation Planning, IPCAM, City of Vaughan  
Peter Cholewa, Project Manager, Stantec Consulting  
Diana Addley, Senior Environmental Planner, Stantec Consulting  
Arash Mirhoseini, Senior Transportation Planner, Stantec Consulting  
Jennifer Robinson, Environmental Planner, Stantec Consulting



**Simulated V/C (Colour)**

Grey	-1 to 0
Green	0 to 0.25
Yellow	0.25 to 0.5
Orange	0.5 to 0.75
Red-Orange	0.75 to 1
Dark Red	1 to 1.79769e+308



**Simulated V/C (Colour)**

Grey	-1 to 0
Green	0 to 0.25
Yellow	0.25 to 0.5
Orange	0.5 to 0.75
Red	0.75 to 1
Dark Red	1 to 1.79769e+308



**Simulated V/C (Colour)**

Grey	-1 to 0
Green	0 to 0.25
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Grey	-1 to 0
Green	0 to 0.25
Yellow	0.25 to 0.5
Orange	0.5 to 0.75
Red-Orange	0.75 to 1
Dark Red	1 to 1.79769e+308

## Robinson, Jennifer

---

**From:** Esedebe, Hilda <Hilda.Esedebe@vaughan.ca>  
**Sent:** Monday, January 11, 2021 1:21 PM  
**To:** [REDACTED]  
**Cc:** Coles, Todd; [REDACTED]; Addley, Diana; Cholewa, Peter; Robinson, Jennifer  
**Subject:** RE: [External] Survey - Vaughan Mills Bass Pro extension

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Hi [REDACTED],

Thanks again for your email and thank you for your participation in the online Public Information Centre for the Bass Pro Mills Environmental Assessment, which closed on January 8, 2021.

We appreciate feedback from the public and representatives from community associations like yours. Your comments have been duly noted and will be part of the public consultation record for the study.

To reiterate, a detailed Transportation/Traffic Analysis for the study area will be completed at later stages of the study, once the recommended design is identified. Results will be shared with stakeholders at that time (including the Weston Downs Rate Payers Association-WDRA).

The City of Vaughan is growing and infrastructure for vehicular traffic, transit and active transportation needs to be in place to support this growth. The City and Region have a number of ongoing studies aimed at growth and improving Vaughan's connectivity including this Bass Pro Mills EA, [Kirby Road missing link](#), [Teston Road missing link](#), [Langstaff Road missing link](#) to mention a few. York Region also has plans to widen Rutherford Road and Weston Road by 2041 (see the [Transportation Master Plan | York Region](#)). Although many of the main roads in the area including Weston Road, Rutherford Road, Langstaff Road and Pine Valley Drive fall under York Region's jurisdiction, the City works with all tiers of government to preserve the interest of citizens as best as possible while reviewing multiple solutions to address growth and transportation needs. The option is however available to the WDRA to forward concerns regarding Regional roads directly to the Region at [roads@york.ca](mailto:roads@york.ca) for further consideration.

Regards,

Hilda Esedebe, P.Eng., MBA, M.Sc.  
Transportation Project Manager  
Infrastructure Planning and Corporate Asset Management  
905-832-8585, ext. 8484 | [hilda.esedebe@vaughan.ca](mailto:hilda.esedebe@vaughan.ca)

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[vaughan.ca](http://vaughan.ca)

-----Original Message-----

**From:** [REDACTED]  
**Sent:** Friday, January 8, 2021 8:59 PM  
**To:** Esedebe, Hilda <Hilda.Esedebe@vaughan.ca>

Cc: Coles, Todd <Todd.Coles@vaughan.ca>; [REDACTED]  
Subject: [External] Survey - Vaughan Mills Bass Pro extension

Hilda,

Further to my survey I submitted today, I would like to "add" the following to my submission:

We do not want any direct connections to the Weston Downs Community.

We would like the City Of Vaughan through your team to Stop encouraging traffic infiltration as a means of dealing with traffic congestion on Rutherford, Weston and Langstaff roads.

We would like the City Of Vaughan to Start caring about neighbourhood communities that already exist, rather than trying to justify the building of more condos and higher density by moving congestion off the main arteries and instead through our existing, neighbourhood communities.

My request is to review the results of the survey in order to provide context as this survey was very misleading !

Thank you, and I look forward to meeting with you to discuss further.

Thank you,  
[REDACTED]

This e-mail, including any attachment(s), may be confidential and is intended solely for the attention and information of the named addressee(s). If you are not the intended recipient or have received this message in error, please notify me immediately by return e-mail and permanently delete the original transmission from your computer, including any attachment(s). Any unauthorized distribution, disclosure or copying of this message and attachment(s) by anyone other than the recipient is strictly prohibited.



## Robinson, Jennifer

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**To:** Esedebe, Hilda  
**Subject:** RE: WDRA letter regarding Municipal Class Environmental Assessment Study Bass Pro Mills Drive

---

**From:** Esedebe, Hilda

**Sent:** Monday, January 11, 2021 1:51 PM

**To:** [REDACTED]

**Cc:** Bevilacqua, Maurizio <[Maurizio.Bevilacqua@vaughan.ca](mailto:Maurizio.Bevilacqua@vaughan.ca)>; Rosati, Gino <[Gino.Rosati@vaughan.ca](mailto:Gino.Rosati@vaughan.ca)>; Ferri, Mario <[Mario.Ferri@vaughan.ca](mailto:Mario.Ferri@vaughan.ca)>; Jackson, Linda <[Linda.Jackson@vaughan.ca](mailto:Linda.Jackson@vaughan.ca)>; Hubjer, Selma <[Selma.Hubjer@vaughan.ca](mailto:Selma.Hubjer@vaughan.ca)>; Musacchio, Vince <[Vince.Musacchio@vaughan.ca](mailto:Vince.Musacchio@vaughan.ca)>; Coles, Todd <[Todd.Coles@vaughan.ca](mailto:Todd.Coles@vaughan.ca)>; DeFrancesca, Rosanna <[Rosanna.DeFrancesca@vaughan.ca](mailto:Rosanna.DeFrancesca@vaughan.ca)>; [REDACTED]

**Subject:** RE: WDRA letter regarding Municipal Class Environmental Assessment Study Bass Pro Mills Drive

Good afternoon [REDACTED],

Thank you for your letter and for your participation in the online Public Information Centre (PIC) for the Bass Pro Mills Environmental Assessment Study, which closed on January 8, 2021.

We appreciate feedback from the public and representatives of community associations like yours. Your comments have been duly noted and will be part of the public consultation record for the study.

To reiterate, further Transportation/Traffic Analysis for the study area will be completed at later stages of the study, once the recommended design is identified. Results will be shared with stakeholders at that time (including the Weston Downs Rate Payers Association-WDRA).

As noted in the online PIC, the City of Vaughan is growing and infrastructure for vehicular traffic, transit and active transportation needs to be in place to support this growth. The City and Region have a number of ongoing studies aimed at growth and improving Vaughan's connectivity including this Bass Pro Mills EA, [Kirby Road missing link](#), [Teston Road missing link](#), [Langstaff Road missing link](#) to mention a few. York Region also has plans to widen Rutherford Road and Weston Road by 2041 (see the [Transportation Master Plan | York Region](#)). Although many of the main roads in the area including Weston Road, Rutherford Road, Langstaff Road and Pine Valley Drive fall under York Region's jurisdiction, the City works with all tiers of government to preserve the interest of citizens as best as possible while reviewing multiple solutions to address growth and transportation needs. You may also email [roads@york.ca](mailto:roads@york.ca) for more information regarding the Regional road network.

A healthy and happy New Year to your and yours as well and looking forward to further consultations on this study.

Regards,

**Hilda Esedebe**, P.Eng., MBA, M.Sc.  
**Transportation Project Manager**  
**Infrastructure Planning and Corporate Asset Management**  
905-832-8585, ext. 8484 | [hilda.esedebe@vaughan.ca](mailto:hilda.esedebe@vaughan.ca)

**City of Vaughan | Infrastructure Development**  
2141 Major Mackenzie Dr., Vaughan, ON L6A 1T1  
[vaughan.ca](http://vaughan.ca)



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**From:** [REDACTED]  
**Sent:** Saturday, January 9, 2021 1:59 AM  
**To:** Esedebe, Hilda <[Hilda.Esedebe@vaughan.ca](mailto:Hilda.Esedebe@vaughan.ca)>; Coles, Todd <[Todd.Coles@vaughan.ca](mailto:Todd.Coles@vaughan.ca)>; DeFrancesca, Rosanna <[Rosanna.DeFrancesca@vaughan.ca](mailto:Rosanna.DeFrancesca@vaughan.ca)>  
**Cc:** Bevilacqua, Maurizio <[Maurizio.Bevilacqua@vaughan.ca](mailto:Maurizio.Bevilacqua@vaughan.ca)>; Rosati, Gino <[Gino.Rosati@vaughan.ca](mailto:Gino.Rosati@vaughan.ca)>; Ferri, Mario <[Mario.Ferri@vaughan.ca](mailto:Mario.Ferri@vaughan.ca)>; Jackson, Linda <[Linda.Jackson@vaughan.ca](mailto:Linda.Jackson@vaughan.ca)>  
**Subject:** [External] WDRA letter regarding Municipal Class Environmental Assessment Study Bass Pro Mills Drive

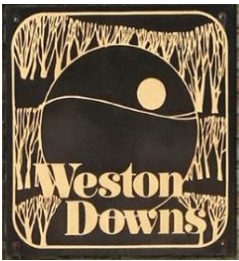
Attached please find a letter from the Weston Downs Ratepayers Association and supporting excerpt.

Best wishes for a Happy and Healthy New Year to each of you and your families.

Stay safe,

[REDACTED]  
Co-president, Weston Downs Ratepayers Association

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# WESTON DOWNS RATEPAYERS ASSOCIATION

[www.westondownra.ca](http://www.westondownra.ca)

January 8, 2021

Hilda Esedebe, P. Eng  
Project Manager  
City of Vaughan  
2141 Major Mackenzie Drive  
Vaughan, Ontario  
L6A 1T1

**RE: Municipal Class Environmental Assessment Study  
Bass Pro Mills Drive**

Dear Ms. Esedebe,

The Weston Downs Ratepayers Association (WDRA) represents a residential community of 1876 homes, bounded by Rutherford Road to the north, Langstaff Road to the south, Weston Road to the east, and the National Estates to the west. On behalf of the WDRA, we would like to again express our concerns with respect to the Bass Pro Mills Drive extension, from Highway 400 to Weston Road, as recommended in the Vaughan Mills Centre Secondary Plan (2014).

We oppose any direct connection or alignment of the proposed Bass Pro Mills Drive extension to any of the streets in Weston Downs. Specifically, we oppose the direct alignment of the Bass Pro Mills Drive extension to any of Weston Downs community entrances on Weston Road. We also oppose the direct connections or alignments of walking or bicycle trails on the east side of Weston Road to our Weston Downs entrances on the west side of Weston Road.

Any such alignment will compound the current traffic issues that the Weston Downs community is experiencing with traffic infiltration. Many residents who live to the north and west of Weston Downs currently cut through our residential streets to circumvent the gridlock on Weston Road and Rutherford Road, during the morning and evening rush hours. It is unfair for the City of Vaughan to mitigate traffic gridlock caused by intensification by encouraging traffic infiltration through established quiet communities.

Please see the attached excerpt Item #4, Report No. 30 of the Committee of the Whole (Public Hearing) which was adopted by the Council of the City of Vaughan on June 25, 2013. Specifically, the recommendation adopted by Council is as follows:

*Staff be directed to eliminate the eastern roadway connection to the proposed intersection at Weston Road and Astona Boulevard, and that staff continue to work with York Region and landowners on the proposed intersection at Weston Road and the Bass Pro Mills Drive extension, and that staff report back on the timing and implementation of the extension of Bass Pro Mills Drive and the extensions of Creditview Road and Westcreek Drive to Rutherford Road as part of the technical report to the Committee of the Whole;*

Please respect the aforementioned City of Vaughan 2013 commitment by ensuring that the Bass Pro Mills Drive extension and any other roads, walkways, or bicycle paths on the east side of Weston Road are not directly aligned or connected to any Weston Downs entrances or roads on the west side of Weston Road. The quality of life of residents in our established Weston Downs community should not be sacrificed for intensification or because of poor planning. Mitigating traffic congestion by diverting traffic from congested arterial roads and instead encouraging traffic infiltration into our residential communities is a blatant disregard for the Weston Downs residents and it will not be accepted.

Thank you for your time and consideration.

Yours truly,

Weston Downs Ratepayers Association

Per:

██████████

Co-president, Weston Downs Ratepayers Association (WDRA)

cc. ██████████, Co-president, WDRA

██████████, Co-president, WDRA

CITY OF VAUGHAN

**EXTRACT FROM COUNCIL MEETING MINUTES OF JUNE 25, 2013**

Item 4, Report No. 30, of the Committee of the Whole (Public Hearing), which was adopted without amendment by the Council of the City of Vaughan June 25, 2013.

*Councillor Racco declared an interest with respect to this item, as it relates to the Northeast Corner of Jane Street and Riverock Gate, Tesmar Holdings Inc., as her daughter is working for a related company and did not take part in the discussion or vote on the foregoing matter.*

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**4 THE VAUGHAN MILLS CENTRE SECONDARY PLAN STUDY  
DRAFT SECONDARY PLAN  
FILE: 26.1  
WARDS 1, 3 & 4**

The Committee of the Whole (Public Hearing) recommends:

- 1) That the recommendation contained in the following report of the Commissioner of Planning and the Director of Development Planning, dated June 11, 2013, be approved;
- 2) That the following resolution submitted by Councillor DeFrancesca, be approved:

Whereas, residents have raised concerns with the proposed intersection at Astona Boulevard and Weston Road;

Whereas, a connection to Weston Road is proposed at the future intersection of the extended Bass Pro Mills Drive and Weston Road;

Whereas, opportunities exist to connect the future Bass Pro Mills Drive to Langstaff Road and Rutherford Road through the future extensions of Creditview Road and Westcreek Drive,

Therefore be it resolved that:

Staff be directed to eliminate the eastern roadway connection to the proposed intersection at Weston Road and Astona Boulevard, and that staff continue to work with York Region and landowners on the proposed intersection at Weston Road and the Bass Pro Mills Drive extension, and that staff report back on the timing and implementation of the extension of Bass Pro Mills Drive and the extensions of Creditview Road and Westcreek Drive to Rutherford Road as part of the technical report to the Committee of the Whole;

- 3) That the following deputations and Communications be received:

1. [REDACTED], Dialog, Toronto Studio, representing the City of Vaughan;
2. [REDACTED] Weston Downs Ratepayers, Blackburn Boulevard, Woodbridge;
3. [REDACTED], Weston Downs Ratepayers;
4. [REDACTED] Blackburn Boulevard, Woodbridge, and Communication C21 dated June 11, 2013;
5. [REDACTED], Magna International, Steeles Avenue East, Brampton;
6. [REDACTED], Village Green Drive, Woodbridge;
7. [REDACTED], Goldberg Group, Avenue Road, Toronto, and Communication C3, dated June 7, 2013, on behalf of Ivanhoe Cambridge;
8. [REDACTED], Velmar Drive, Woodbridge;
9. [REDACTED], Topper Court, Woodbridge;
10. [REDACTED], Radley Street, Woodbridge;
11. [REDACTED], Mellings Drive;
12. [REDACTED], [REDACTED], King Street West, Toronto, on behalf of Canadian National Railways;

CITY OF VAUGHAN

**EXTRACT FROM COUNCIL MEETING MINUTES OF JUNE 25, 2013**

Item 4, CW(PH) Report No. 30 – Page 2

- 13. [REDACTED], Topper Court, Woodbridge; and
- 14. [REDACTED], Flushing Avenue, Woodbridge; and

4) That the following Communications be received:

- C2. [REDACTED], King Street West, Toronto, dated June 7, 2013;
- C8. [REDACTED], Lakeshore Group, Wellington Street West, dated June 10, 2013;
- C9. [REDACTED], Aird & Berlis, Bay Street, Toronto, dated June 10, 2013;
- C10. [REDACTED], Weston Consulting, Millway Avenue, Vaughan, dated June 10, 2013;
- C12. [REDACTED] Aird & Berlis, Bay Street, Toronto, dated June 10, 2013;
- C13. [REDACTED], McMillan, Bay Street, Toronto, dated June 11, 2013;
- C14. [REDACTED], Davies Howe Partners LLP, Spadina Avenue, Toronto, dated June 10, 2013;
- C15. [REDACTED], Brattys LLP, Keele Street, Vaughan, dated June 11, 2013;
- C16. [REDACTED] L. Humphries, Humphries Planning Group Inc., Chrislea Road, Vaughan, dated June 11, 2013; and
- C17. [REDACTED], Parente, Borean LLP, Highway 7, Vaughan, dated June 11, 2013.

**Recommendation**

The Commissioner of Planning and the Director of Policy Planning recommend:

- 1. THAT the draft Vaughan Mills Centre Secondary Plan, prepared by Dialog Inc. forming Attachment 8 to this report, BE RECEIVED; and, that any issues raised at the public hearing, be addressed by the Policy Planning Department in a future Technical Report to Committee of the Whole.

**Contribution to Sustainability**

The contribution to sustainability will be addressed when the Technical Report is considered.

**Economic Impact**

The economic impact will be addressed in the Technical Report to Committee of the Whole.

**Communications Plan**

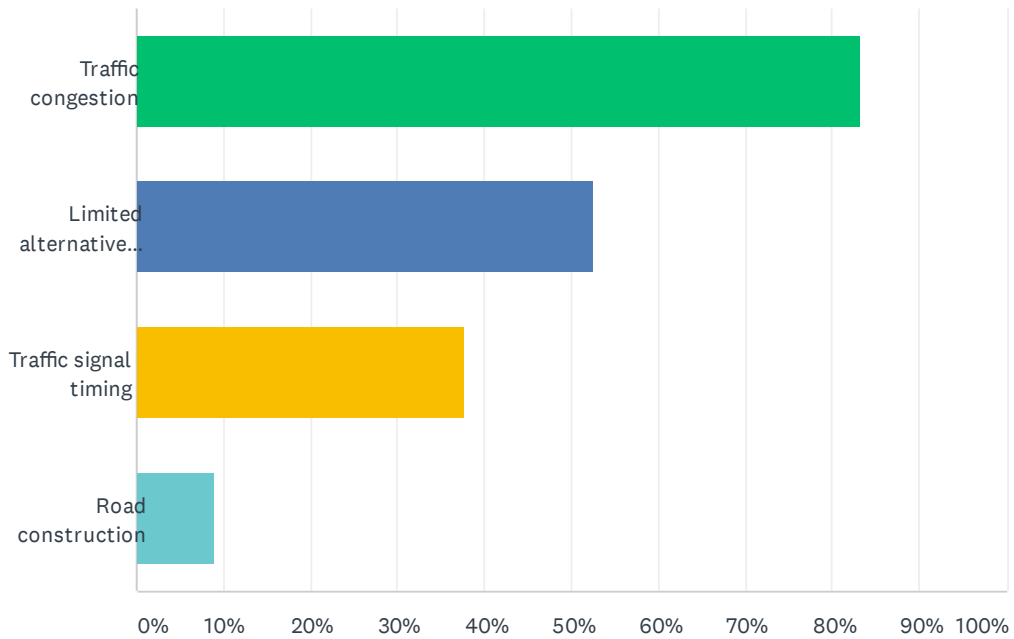
On May 09, 2013, a joint notice of Public Open House and notice of Public Hearing was sent to all landowners within, the Secondary Plan boundary and to 400 m beyond; to Ratepayer Associations; and to individuals who had requested notification. The notice was also placed in the Vaughan Liberal; the Vaughan Citizen; and posted on both the City Page and Vaughan Online websites. The draft Vaughan Mills Centre Secondary Plan was posted on the City's website on May 22, 2013, to allow for public review in advance of this public hearing. An overview of the proposed Secondary Plan was presented by the City's consulting team at the May 22, 2013 Public Open House.

**Purpose**

The purpose of this report is to introduce the draft Secondary Plan for the Vaughan Mills Centre plan area (the Plan) for the purposes of obtaining public comment prior to its finalization. The Secondary Plan will establish a comprehensive framework of planning policies to guide and manage growth and development in the secondary plan area to 2031. This report presents a summary of the draft Secondary Plan and the process followed in its preparation. Copies of the

# Q1 What are your greatest difficulties when driving east-west in the vicinity of the Bass Pro study area? (Select All That Apply)

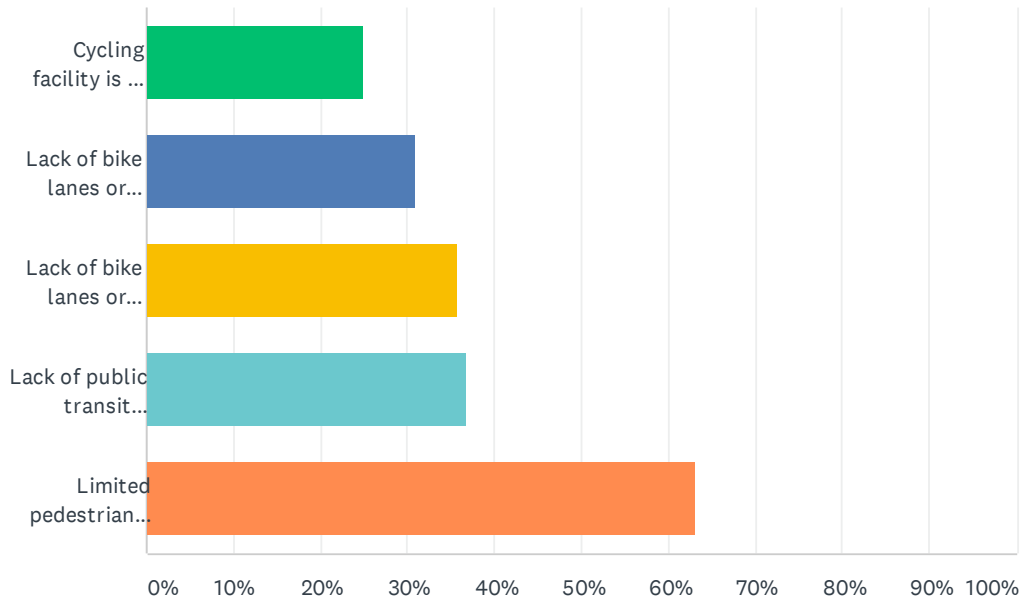
Answered: 101 Skipped: 6



ANSWER CHOICES	RESPONSES	
Traffic congestion	83.17%	84
Limited alternative routes	52.48%	53
Traffic signal timing	37.62%	38
Road construction	8.91%	9
Total Respondents: 101		

## Q2 What are your greatest difficulties when using alternative modes of transportation in the vicinity of the Bass Pro study area? (Select All That Apply)

Answered: 84 Skipped: 23

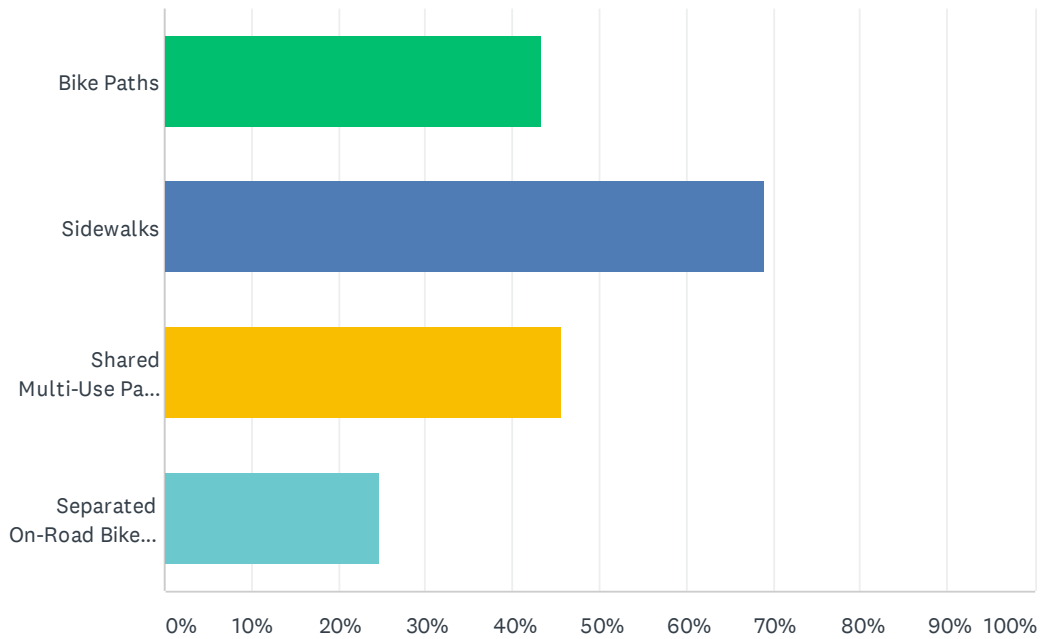


ANSWER CHOICES	RESPONSES	
Cycling facility is on the roadway	25.00%	21
Lack of bike lanes or cycling facilities on the roadway	30.95%	26
Lack of bike lanes or cycling facilities within the boulevard	35.71%	30
Lack of public transit services	36.90%	31
Limited pedestrian friendly routes	63.10%	53
Total Respondents: 84		



### Q3 What Active Transportation improvements would you most like to see through the extension of Bass Pro Mills Drive (Select All That Apply)

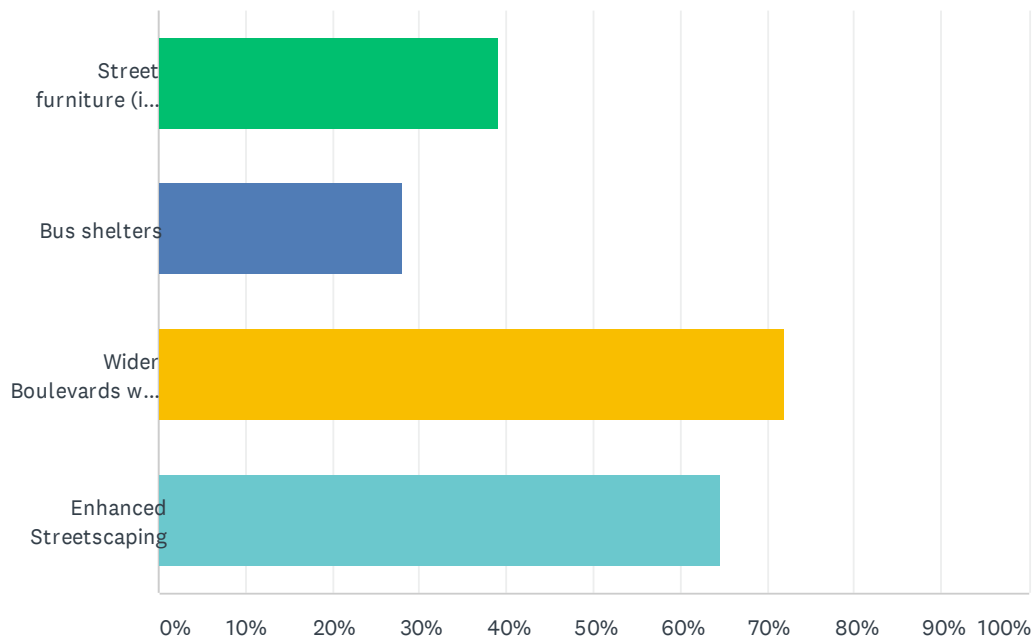
Answered: 81 Skipped: 26



ANSWER CHOICES	RESPONSES	
Bike Paths	43.21%	35
Sidewalks	69.14%	56
Shared Multi-Use Paths (for both pedestrians and cyclists)	45.68%	37
Separated On-Road Bike Paths	24.69%	20
Total Respondents: 81		

## Q4 What Community improvements would you most like to see through the extension of Bass Pro Mills Drive (Select All That Apply)

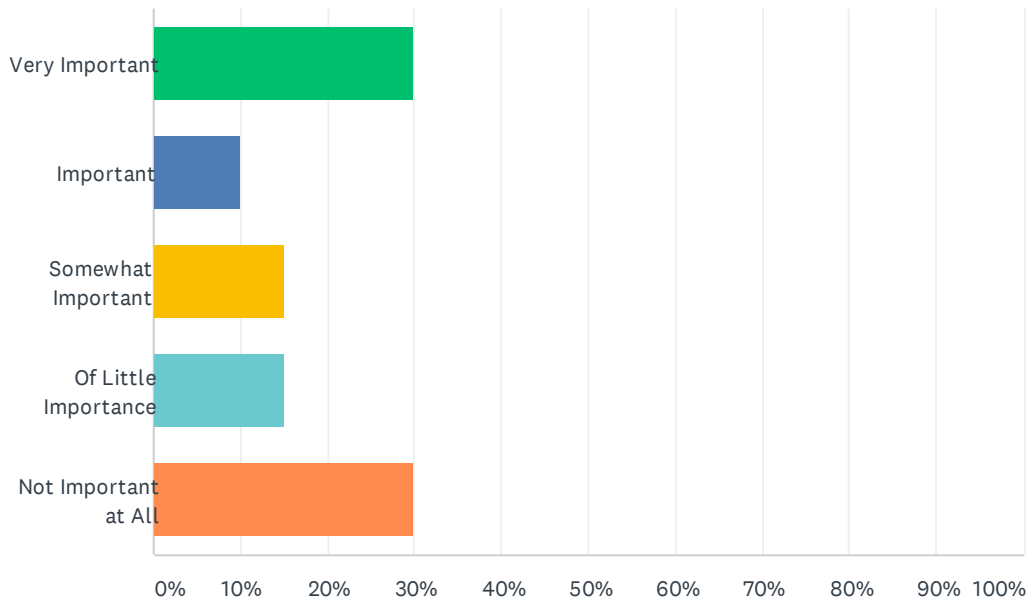
Answered: 82 Skipped: 25



ANSWER CHOICES	RESPONSES	
Street furniture (i.e. benches, bike racks)	39.02%	32
Bus shelters	28.05%	23
Wider Boulevards with street trees	71.95%	59
Enhanced Streetscaping	64.63%	53
Total Respondents: 82		

## Q5 How important is it to you to have a pedestrian/cycling friendly connection between Weston Road and Jane Street?

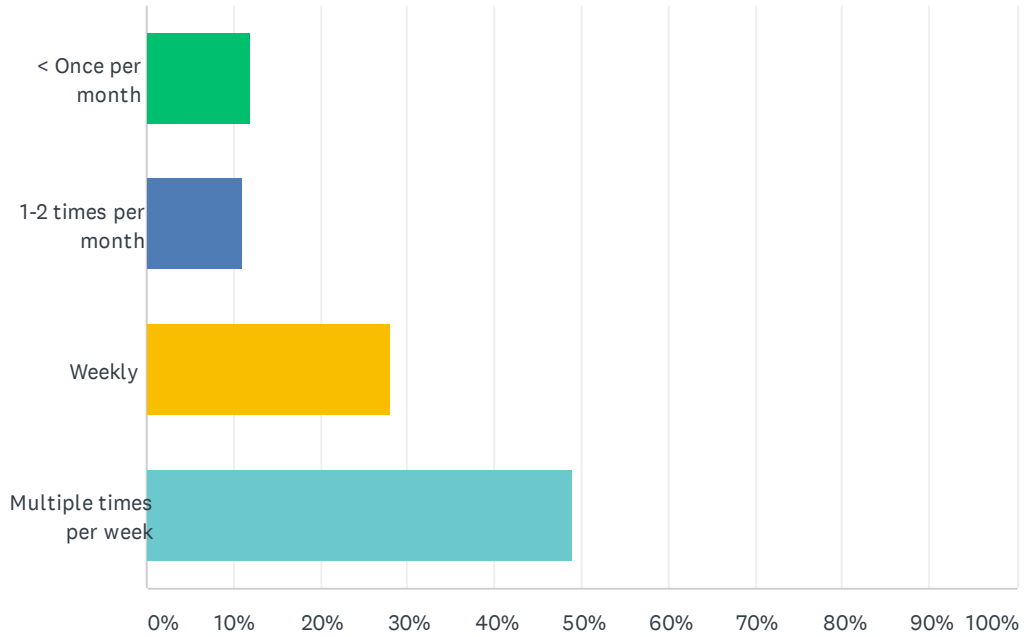
Answered: 100 Skipped: 7



ANSWER CHOICES	RESPONSES	
Very Important	30.00%	30
Important	10.00%	10
Somewhat Important	15.00%	15
Of Little Importance	15.00%	15
Not Important at All	30.00%	30
<b>TOTAL</b>		<b>100</b>

## Q6 How often do you travel east-west to Vaughan Mills Mall?

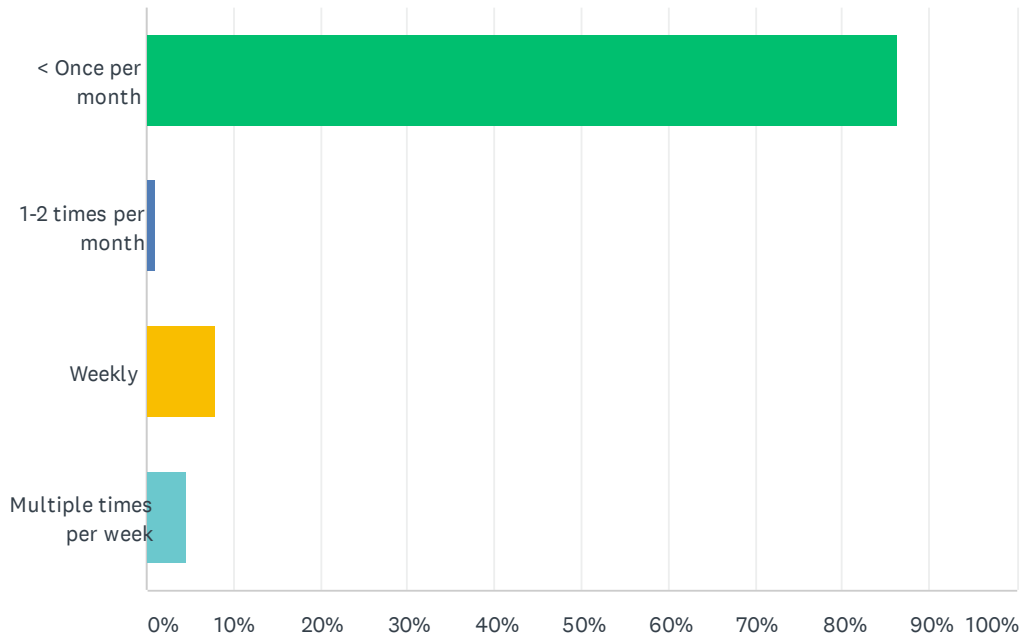
Answered: 100 Skipped: 7



ANSWER CHOICES	RESPONSES	
< Once per month	12.00%	12
1-2 times per month	11.00%	11
Weekly	28.00%	28
Multiple times per week	49.00%	49
<b>TOTAL</b>		<b>100</b>

## Q7 How often do you use public transportation?

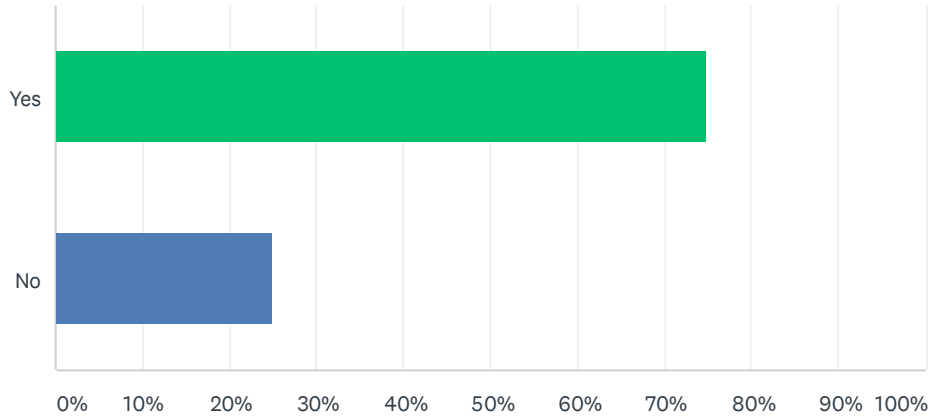
Answered: 88 Skipped: 19



ANSWER CHOICES	RESPONSES	
< Once per month	86.36%	76
1-2 times per month	1.14%	1
Weekly	7.95%	7
Multiple times per week	4.55%	4
<b>TOTAL</b>		<b>88</b>

## Q8 Would you like to receive future notifications about the study?

Answered: 96 Skipped: 11



ANSWER CHOICES	RESPONSES	
Yes	75.00%	72
No	25.00%	24
TOTAL		96

**Q9 Your opinion is important to the Study Team. In the area below, please provide any comments or feedback that you wish to share with the Study Team.**

Answered: 82 Skipped: 25

Bass Pro Mills Extension - Online Public Information Centre 1

#	RESPONSES	DATE
1	I am concerned that the traffic congestion along Weston Rd and Rutherford Rd will become heavier than they were in pre-pandemic times, which will lead to even more drivers who cut through Weston Downs to reach Langstaff Rd, Weston Rd or Rutherford Rd, and make it that much more difficult for residents of this neighbourhood to get in and out of it. I believe the problem would be significantly compounded by extending Astona Blvd and strongly urge you to consider alternative options.	1/11/2021 10:51 AM
2	My biggest concern is environmental - the disruption and danger to the environment and wildlife	1/9/2021 12:11 PM
3	I feel this extension is not necessary and will create additional expense for our city which could be avoided. I do not support building this extension.	1/9/2021 9:34 AM
4	The Vaughan mills extension can not attach to any subdivision entrance streets west of Weston road e.g Weston downs. To do so would create increased traffic through flow in already heavy traffic neighborhoods and would be unsafe.	1/9/2021 9:14 AM
5	It is critical that any future developments/improvements do not allow direct infiltration of additional traffic into Weston Downs, which already has a serious traffic infiltration problem. Any additional vehicle traffic, which, based on the current proposal, will come from highway 400, Vaughan Mills mall and the new development, will have a huge negative impact on the quality of life for residents in this area. In addition to creating roads that are unsafe for children and residents. Without question ... Traffic MUST be diverted away from this area. If this is not done you will have created a monster. This should be a win/win and not a win/lose situation. Your survey was leading in that you did not provide an option for other input.	1/9/2021 8:11 AM
6	Extension is going to impact surrounding neighborhoods greatly.	1/9/2021 12:54 AM
7	So much congestion that there should be NO more roads to make things worse	1/8/2021 11:09 PM
8	I do not support the proposed Bass Pro Mills extension. It does little to address the traffic issues cited in this very study. It will only add to the traffic issues in this area by putting more cars into the primary area shown in this study. This study does not provide any indication that adding this new roadway extension will address the traffic issues in the future. It only seems to create an additional access point for Vaughan Mills Plaza. It does not even address the issue of traffic between Weston and Jane. Bass Pro does not have easy access to Jane Street. Anyone travelling along Bass Pro Mills to access Jane street is aware that the traffic there is high and already congested, this proposal will not help. Why does the Survey not have any question to ask if I support the extension. This is a poorly constructed and thought out survey designed only to appear to create support for this plan. Very biased and skewed. Be better.	1/8/2021 9:28 PM
9	This study while attempting to present itself as a well-considered evaluation of all options is so limited in scope that it can not properly serve the needs of Vaughan. It is much too limited, it does address the impact of enabling more cars to travel east-west, and the resulting impact on north-south roadways along Weston and Jane and at the intersection. It does not appear to address the primary reason for the east-west traffic. The primary traffic source seems to be along the 400 and it will continue to create the most traffic in the area. Adding another east-west route does little to solve this issue in this area. By having such a limited scope, this solution seems to only have the effect of improving access to Vaughan Mills mall. The study emphasizes the benefit of access to private property but the only private property in the primary zone is for the developer. What will happen to this east west traffic as it crosses over the 400? There is no direct access to Jane Street. It is a convoluted route. To address traffic congestion, why not improve the north-south routes such widening the two-lane portion of Islington, Create a proper north-south corridor along Pine Valley by connecting it between Langstaff and Rutherford. These would ease the north-south traffic on 400 and the resulting traffic congestion created around its off-ramps. If the intent is to improve access for active users why not limit this to a transitway for quick bus access to the subway or put in place a bikepath and walkway? These are referenced as possibilities not as committed to the plan. Adding an additional road for east-west car travel will only increase the amount of car traffic onto Weston Road and Jane St. It will not ease the traffic congestion. In addition, this survey did not ask if we favour the option? It only asks about alternatives for the option. It implies that this is a done deal and you are only looking for input so that you can claim that users asked for certain features on this new road, it does not give us the option to object to such a poorly thought out plan.	1/8/2021 9:17 PM



## Bass Pro Mills Extension - Online Public Information Centre 1

10	This extension is not needed. The traffic in this area is already congested enough there is no need to provide more. Save tax payers money. This is not the time to be entertaining such a project during a pandemic.	1/8/2021 9:11 PM
11	There should be no connection to the roadways in Weston Downs. There should be more north south roads in the area west of the 400 to allow traffic flow from Hwy 7 to Rutherford to help with heavy traffic on Weston Road as well as an additional access east west to allow traffic to flow over the 400 . ALL cut through traffic flowing through Weston Downs from the north or south or east should be blocked .	1/8/2021 8:58 PM
12	My concern has a Weston downs tx player is the amount of traffic that will be created via Astona into our residential community. If Bass Pro Drive is to be created it should connect to Weston road north of Astona and prohibiting cars from turning south bound on to Weston Rd.	1/8/2021 8:35 PM
13	This extension will come onto an already congested weston road and bring people through the weston downs community to go west and create an artery to bypass Rutherford. Our community is meant for families and children to walk, run and play. Our community already is being used to bypass Rutherford. I expect that better planning as a whole to happen.	1/8/2021 7:52 PM
14	No bike bridge required ! Waste of taxpayers money and this was not part of the plan set out and approved by council back in 2012	1/8/2021 7:49 PM
15	What's will be taken to protect the integrity of neighbourhoods to the west of the study area and prevent traffic infiltration by drivers wishing to bypass Rutherford Rd when going/coming to/from the mall or highway 400?	1/8/2021 6:57 PM
16	Offset any road connections to Weston Road so that they do NOT connect to Weston Downs. You the City are the cause of traffic infiltration into our community. We have children that have been hit by cars and it is no longer safe or pleasurable to walk on our community sidewalks as hundreds of cars cut through our community to avoid congestion on Weston and Rutherford Road. The City does NOT care about the quality of life of existing residents but rather is more concerned about building more condos and building more traffic capacity by moving traffic congestion into quiet neighbourhood communities. The City is reducing our quiet enjoyment and quality of life by intentionally promoting traffic infiltration into Weston Downs to deal with traffic congestion on the Rutherford Road, Weston Road and Langstaff Road.	1/8/2021 5:55 PM
17	The focus of this survey seems to have completely missed the mark on the elements that are most important and impactful to residents living in the immediate vicinity of these changes. I live in Weston Downs directly across Weston road from the proposed change. None of the 5 family members living in our home use public transportation or travel by bicycle, nor do we have any plans to do so even if these amenities are built. Our primary concern is traffic infiltration into our subdivision by people passing through to bypass traffic on either Weston road or Rutherford. If this new road links directly to our subdivision, this issue will only get worse and make our community roads less safe. Where, in this proposal, are items to address these concerns which have been raised by the Weston Downs Homeowners association? Where, in this proposal, are items to address limiting new high density residential building in the area until other improvements along Weston and Rutherford can be built? Are there any other plans to address traffic throughput issues along Rutherford and Weston Road? Putting benches and trees on this new road does nothing to address these real concerns from current residents in the immediate vicinity that have had to put up with the issues caused by rampant development without adequate infrastructure. No further development should be allowed in the vacant lands on which this road is proposed until a wholistic solution is put in place. In addition, this vacant land should not be rezoned to allow residential of any sort.	1/8/2021 5:37 PM
18	We do not want any direct connections to the Weston Downs Community. Stop encouraging traffic infiltration as a means of dealing with traffic congestion on Rutherford, Weston and Langstaff roads. Start caring about neighbourhood communities that already exist, rather than trying to justify the building of more condos and higher density by moving congestion off the main arteries and instead through our existing, neighbourhood communities.	1/8/2021 5:34 PM
19	Weston downs is already being used as a throughway, a roadway from Vaughan Mills should not be connected to a Weston's downs entrance. Access from the mall to Weston downs should be prohibited.	1/8/2021 4:54 PM
20	Whatever road connection(s) from Bass Pro to Weston Rd. is implemented, this should be done in a way that discourages traffic infiltration into Weston Downs Community. Traffic in the community has already reached a point of saturation. The intersection at Weston and	1/8/2021 4:08 PM

## Bass Pro Mills Extension - Online Public Information Centre 1

Rutherford needs to be further improved to accommodate the volume of traffic that over the past few years has increased tremendously. Perhaps additional turning lanes should be considered. To further relive congestion on Weston Rd., serious consideration must be given to northbound access to the 400 corridor at Langstaff, presently access is limited to southbound only.

21	The overall plan is excellent. As residents of Weston Downs we already have an issue of drivers using our quiet neighbourhood as a shortcut, mainly at peak travel times. Our main concern for the project is for it to be designed in such a way as not to cause an influx of traffic in our residential area.	1/8/2021 3:51 PM
22	Don't build an extension to connect v mills to Weston, Weston is already the 2nd worst street in history - next to Rutherford.	1/8/2021 3:41 PM
23	We do not need more congestion on Weston Rd. Dumping onto Weston Rd and then infiltrating Weston Downs, is not when we want or need.	1/8/2021 3:37 PM
24	Traffic infiltration into the Weston Downs subdivision is already terrible and unsafe for children and pedestrians. Please ensure that future traffic will not do gymnastics of turns into the neighbourhood.	1/8/2021 3:31 PM
25	Weston road is already so congested you cannot connect this road until you fix North south congestion issues on Weston road. This will detrimentally worsen congestion on Weston road and increase air and noise pollution. This is not a problem that needs solving, there is a road going east west at the north side of Vaughan Mills on Rutherford and south of vaughan mills on Langstaff and neither are busy enough warranting an addition east west road. PRIORITIZE NORTH SOUTH ON WESTON ROAD ISSUES FIRST.	1/8/2021 3:26 PM
26	Weston road is already so congested you cannot connect this road until you fix North south congestion issues on Weston road. This will detrimentally worsen congestion on Weston road and increase air and noise pollution. This is not a problem that needs solving, there is a road going east west at the north side of Vaughan Mills on Rutherford and south of vaughan mills on Langstaff and neither are busy enough warranting an addition east west road. PRIORITIZE NORTH SOUTH ON WESTON ROAD ISSUES FIRST.	1/8/2021 3:25 PM
27	I currently live on Valeria Blvd and there is already an immense volume of traffic on our street, furthermore making it almost impossible to leave or enter our driveway. This causes a great concern for the safety of my family and the neighbours who are constantly battling speeding vehicles. We have witnessed numerous accidents by drivers who do not live in the area and are in a rush to avoid traffic signals on Weston Rd and Langstaff. Being a realtor for 31 years, I know that by opening a road to Weston Rd from Vaughn mills shopping centre, will cause an influx of new vehicles using Valeria as a shortcut and further enhancing our already apparent problem.	1/8/2021 2:58 PM
28	The traffic congestion comes from people trying to get out of Vaughan Mills during rush hour. If the traffic could be directed to Highway 400 rather than Rutherford Road - there would be less traffic congestion. Directing traffic to Weston Road where there are huge traffic issues would be a bad idea. If you were to add two more lanes - both north and south - then maybe there wouldn't be an issue if you gave access to Weston Road. Weston Downs already has an issue with cars using the subdivision as a short cut through the Weston Road and Rutherford Road intersection. This would make that much worse.	1/8/2021 2:56 PM
29	Im not for the extension of Bass Pro Mills road. There comes a time when population growth and business in area should stay as is. Develop other areas that don't have development and can grow. We talk about the environment, but mean while want to maximize use of every square foot with buildings, roads etc. Since I moved in this area in 2001 there has always been complaints about traffic. What has changed nothing except still complaints about traffic because tons of more development, population growth, lots of condo towers, more townhomes, more all high density, so of course more traffic due to more vehicles. More vehicles more noise and pollution. Humans, wild life etc need some open space not to be show congested. We dont need a study to analyze traffic, just drive along weston, rutherford, hwy 7 etc. Noise level has increased, pollution level, etc,if we are worried about environment why do we keep on growing. Pinevalley was blocked due to some environment issue really??? I think noise, pollution increase affects everyone, from humans to wild life etc. If we really care, I would not create further expansion and growth in this. area.	1/8/2021 2:48 PM
30	The road between Weston Rd and Vaughan Mills should NOT line up with any existing road in	1/8/2021 2:45 PM

## Bass Pro Mills Extension - Online Public Information Centre 1

	Weston Downs.	
31	I( am in favour of extending the road to weston road.	1/8/2021 2:43 PM
32	DO NOT have any impact on traffic through Weston Downs Community. Traffic is horrendous already	1/8/2021 2:33 PM
33	There should be no access from the extension of Bass Pro Mills Drive site into the Weston Downs community. Weston Downs via Astona Blvd. and any other access routes from Weston Road should be protected from any increase in vehicle traffic into its community!	1/8/2021 2:28 PM
34	The extension into the weston downs community should not be considered a viable alternative. There is significant congestion presently and by adding this artery you increase congestion significantly.	1/8/2021 2:13 PM
35	Do not want any connecting route to Astonia Blvd or Topper Court off Weston are from Bass Pro Extension	1/8/2021 1:59 PM
36	why would you try to put more traffic on Weston road when it cannot handle the traffic it has now. if if you add another lane. this makes no sense	1/8/2021 1:53 PM
37	Do not extend the road, the traffic in the neighbour is already crazy. To Weston? Are you nuts? Weston was a parking lot at 5PM before COVID	1/8/2021 1:43 PM
38	The extension of Bass Pro Mills Drive will only create more traffic congestion on an already congested Weston Rd. Furthermore, it will allow for further congestion into the Weston Downs community as commuters try to avoid the traffic of Weston Rd. As a resident of Weston Downs, we do not want further access into our already congested roads as commuters try to bypass the traffic. This has already been proven by the numerous traffic studies that the City has conducted with taxpayers money. This extension would not alleviate traffic but instead shift it to other roads. I'm not sure adding bike lanes on congested roads would be a wise decision as people travel these roads in high speeds and this would be unsafe for cyclists.	1/8/2021 12:55 PM
39	Do not open this up to Astonia? Or into weston downs community. We have enough traffic!	1/8/2021 12:40 PM
40	Within Weston downs there is already a lot of traffic congestion plus getting onto Weston road from Astona has been a problem for years due to congestion	1/8/2021 12:38 PM
41	No extention	1/8/2021 12:31 PM
42	Traffic is already a nightmare in the area of Weston Downs. This extension will only intensify it. I am hopeful the planners will review the current situation in Weston Downs before moving forward.	1/8/2021 12:27 PM
43	There should be no infiltration into the Weston Downs community as we are dealing with major traffic infiltration already. To avoid this, access to Weston road should be made north of Astona Blvd. and a road division median should be placed to avoid u turns which would prevent vehicles from turning around and accessing Weston Downs. Keep vehicular traffic on Regional Roads and not small community streets. The City should work with the Region to widen both Weston and Rutherford Roads to decrease traffic times and keep cars on major Regional road networks. We do NOT want the extension to have access to our community and every effort should be made to avoid infiltration into Weston Downs as was promised by council years ago.	1/8/2021 12:25 PM
44	I'm not in favour of this extension plan. I have concerns with traffic flow increasing through the Weston Downs community from Astona through Village Green to Velmar to Rutherford. This plan is not beneficial to the Weston Downs community.	1/8/2021 12:22 PM
45	We don't want infiltration into Weston Downs through Astona. A median should be placed to avoid any U Turns and keep vehicles on Weston Road.	1/8/2021 12:15 PM
46	Don't open to Weston road. It will create more traffic in the neighboring subdivision.	1/8/2021 11:51 AM
47	The arterial connecting Weston road to bass pro mills drive should not be a direct through way to any subdivision on the west side of Weston road or it will increase congestion in the community which is already at a maximum, a safety hazard for residents, traffic violations and speeding.	1/8/2021 11:44 AM
48	I live in the subdivision west of Vaughan Mills (Weston Downs) I would like to see further restrictions in place on limiting vehicular access into area due to people driving thru	1/8/2021 11:39 AM

## Bass Pro Mills Extension - Online Public Information Centre 1

subdivision to get thru to Rutherford and Langstaff. Its been a issue for over 30 years and the City has done nothing to satisfy the residents in the neighbourhood.

49	This extension will only increase traffic in the area. When do you finally realize that is all it is doing!!!	1/8/2021 11:08 AM
50	we are lacking green space in our community, ideally using this space for a walking path and separate biking path with trees on both sides of the road would be ideal	1/7/2021 9:58 PM
51	Question 7 - There is probably a lot of people who don't use transit at all b/c it's so infrequent, unavailable, etc. but no option for that question. There is not enough information provided on criteria weighting and evaluating to understand how the City came to identify the preferred solution. Outcome appears to be skewed to engineered solutions and not account for natural ecosystem services/green infrastructure. Further your show two study areas the blue and the red when some options only affect the red area and others don't affect the red and spread out into the blue area? Examples are below. How can Surface Water be the least preferred under do nothing? I don't understand how an option with no treatment requirements can score less than the option to extend Bass Pro Mills which creates the need for surface water and storm water management and contributes to increased quantity and decreased quality? Unless no weighting has been placed on the ability of the land to provide these services naturally in an undeveloped state? How is LID factored into this evaluation to mitigate the need for engineered solutions with larger capital investment for surface and stormwater treatment e.g. stormwater ponds? How is this evaluation consistent with current Ministry of Environment guidance for LID? Same arguments apply under Technical Stormwater/drainage section. Please ensure fair weighting has been given to natural vs. engineered solutions for surface and storm water management and treatment. Under Air Quality and Climate Change there seems to be the same sort of weighting issues. I do not understand how either of these options could be the least preferred under the 'Do Nothing' option. Study appears to only consider impacts of air pollution from vehicles. How is weighting given to leaving current lands in their natural state e.g. carbon and pollution sequestration for existing wetlands? Do we get more climate resilience from the land in it's natural state or from developing it? Does this evaluation support Green Directions Vaughan, 2019 Sustainability Plan? Is there Green Infrastructure? How are future potential changes considered from COVID-19 that suggest telecommuting to work is here to stay? Concerned that traffic brought over from Vaughan Mills will have nowhere to go and will go through Weston Downs and other surrounding neighbourhoods. How will this be elevated Valeria is already used as a pass through in West Downs and Ansley Grove in the subdivision to the south. Vice versa once traffic gets to Jane St. where will it go w/o approval of the Langstaff EA over the train yards? Condo developments at Jane and Rutherford are excessive. Rutherford is already a nightmare from Jane to Keele and on Hwy 7 as well. This needs to be addressed or those bottle necks will be amplified and local neighbourhoods will be impacted negatively.	1/7/2021 1:07 PM
52	If the Bass Pro Mill extension proceeds, there must be time restrictions for traffic in the morning rush period & evening rush period to prevent traffic flow from the Bass Pro Mill road into Astona leading into the subdivision. As many use this as alternative route to either Rutherford or Langstaff. The question is how to manage traffic during the rush traffic periods of those travelling from Rutherford & langstaff leading to Bass Pro Mill road through the subdivision.	1/6/2021 11:08 AM
53	Speed of travel by automobile must be #1 priority through city of Vaughan Must extend Bass Pro Mills to Weston to alleviate Rutherford Stop wasting money on cycling projects that are not being used Will not use and allow my kids to bike on/near congested roads. Will use trails for biking. Build proper parking infrastructure (multi-level garage) at VMC if you want people to actually take the subway. Driving to park at 407/Jane or Pioneer Village still means traffic levels will remain unchanged in Vaughan	1/5/2021 1:17 PM
54	I'm supportive of shared multi-use paths (for cyclists and pedestrians), however I don't believe that they are required on both sides of the roadway. Understanding that Vaughan Mills mall is a tourist destination and that the automobile will most likely still be the dominant form of transportation, it is recommended that accommodations for them are also made and not neglected in order to reduce traffic (for example don't eliminate vehicular lanes in order to accommodate multi-use paths on both sides of the road).	1/4/2021 10:58 AM
55	I am in full support of the extension as it will alleviate the extreme congestion on Rutherford at all times and especially during peak times. I believe it would attract more businesses to the area if there was less congestion because drivers would be more likely to travel to the area if they knew that they wouldn't be stuck in that horrible traffic.	1/3/2021 12:58 PM

Bass Pro Mills Extension - Online Public Information Centre 1

56	I live in Weston Downs. I am seriously concerned about the impact that these plans will have on the through traffic in my subdivision. I have small children and the traffic is already very heavy and careless during peak times. I am worried for their safety and wish to minimize impact to where they live.	1/3/2021 12:41 PM
57	As a resident of Weston Downs (specifically Village Green Drive), my concern is specifically regarding non-resident traffic infiltration into the community. The intersection of Weston Rd. and Rutherford, as well as the timing of traffic lights (north/southbound on Weston Rd, and east/westbound on Rutherford), has been very problematic for years. Astona, Village Green Drive, Velaria, Orr, Babak (along with other streets) have been used for years as a through way to avoid the traffic that has been created by poor overall design of commercial development, turn lanes, traffic lights along Weston Rd. and Rutherford. The quality of life, as well as safety of the residents of the community continue to be compromised. I do not see anything in the studies or plans that make reference of what specifically will be implemented to ensure there is NO traffic infiltration into the community (more specifically the names of the streets mentioned above) from the Bass Pro Mills Extension. If there is any type of roadway that allows for the movement of traffic into the community, it will happen. The City needs to be sensitive to all of it's resident and not make improvements at the expense of the residents of a specific part of the community. Travel time improvements and alleviating the congestion at Weston Rd. and Rutherford can not come at the expense and safety of our community. Again, what SPECIFICALLY is in the plan to ensure this does not happen?	1/3/2021 10:41 AM
58	I avoid the area because of traffic congestion. If I need to go to the mall etc., I either go first thing when opening or towards closing time. The road by the dealerships does help as I can avoid Jane St/Rutherford and Jane St to Hwy 400,	1/3/2021 10:34 AM
59	This is a good idea. Traffic on Rutherford is high and this will help. Just moved to Vaughan from Toronto and this is a well taken care off and upkeep is great transit options are very limited and most neighbourhoods within in Vaughan are almost built as self contained places you need a car to get in and out of.	1/3/2021 9:34 AM
60	Please Widen the section of Weston road north of langstaff and sound of Rutherford .. this area is also heavily congested and with development will be worse .. please also Consider this when developing the land next to Vaughan Mills.	1/3/2021 9:03 AM
61	Alleviate traffic on Weston road it is a pit stop and also Rutherford Road, people who do not live in WestonDowns are using our subdivision as thorough way.	12/28/2020 2:33 PM
62	Don't bother with bike lanes, pedestrian walkways, etc in this area. This is not a walking or biking area. Please keep cost down and just extend roadway.	12/28/2020 11:09 AM
63	Just wondering if you conducted a traffic study on Weston Road and how congested it gets. Also will you be putting a do not enter sign on Astona as all the traffic coming off the 400 will be cutting through Weston Downs.	12/28/2020 9:28 AM
64	I live just across from the primary study area in the Weston Downs subdivision. I am extremely concerned that the proposed changes will cause increase traffic congestion through my subdivision. I don't see any evaluation or consultation planned to understand the impact on the existing residents in the community. Weston Downs already has a substantial issue with traffic congestion due to residents from outside the community travelling through the streets to bypass congestion on Rutherford and Weston roads. The Weston Downs Ratepayers association has made a number of proposals to council to address these concerns which have not been adopted. If the proposed new street links directly to the subdivision, this will lead to further congestion. Widening Weston and Rutherford Road, coordinating traffic signals and reducing cars using subdivisions as a bypass are all elements of the solution that must be incorporated in improving traffic congestion in the area. These items are much more critical than bicycle lanes and street scaping, yet I don't see them addressed at all.	12/22/2020 4:48 PM
65	Open Pine Valley between Rutherford and Langstaff as it is one million times more important to eleveliate congestion. The 407 exit on Pine Valley Brings too much traffic to Weston which makes the interest ruin north of Astona too heavily used to have another extension feed onto Weston. Pine Valley is a priority to open as the fact it is not being extended is creating too much traffic to Weston Road to compensate for Pine Valley being closed for access. There is no room for more cars irrespective of what your studies indicate drive the road yourselves and you will see it is accident prone and many near misses that are not on your study. It is not in the public's interest to do this project before Pine Valley. There are also proposed multi level	12/20/2020 12:43 PM

## Bass Pro Mills Extension - Online Public Information Centre 1

buildings in the Weston downs area that make this project a contributor to further traffic issues. You must revisit old agreements to keep Pine Valley closed to benefit the few when the city's population is now in need of that road for the safety of the community.

66	Not a big fan of this particular initiative. The city should fix other traffic issues/improvements before tackling this one. This only helps vaughan mills. Doesn't help the general public or neighbouring communities. They should look at extending pine valley from langstaff to rutherford and look at connecting langstaff between creditstone and keele and fixing all the traffic lights so they work in unison. Too many traffic lights on Rutherford. Look at eliminating exit at Wonderland from rutherford. As well, bigger concern is if they build more condos on that sight which will lead to more traffic. The bass pro street should not connect at Astona as this will lead to further traffic infiltration into Weston Downs.	12/20/2020 11:43 AM
67	Please connect bass pro Mills to Weston road as it will help ease congestion in the area and will provide an alternate route to many.	12/20/2020 9:37 AM
68	I don't agree that extending Bass Pro Mills is going to significantly reduce traffic on Rutherford. It seems to me that much of the traffic on Rutherford is related to access to Highway 400 or Rutherford GO. Extending Bass Pro will be beneficial if it is going to include access to highway 400. How about enhancing highway access at Langstaff as well? Linking Weston and Jane in order to reduce traffic on Rutherford is just going to increase traffic on both of those streets, as everyone must then travel north to get back onto Rutherford. I have always thought Bass Pro should be extended to Weston. I support the project in that regard. Also, I have hoped that the surrounding property would be developed. However, I only see extending Bass Pro as facilitating access to Vaughan Mills Mall at this point in time and see little greater community impact in terms of traffic reduction.	12/20/2020 9:15 AM
69	The extension will help get rid of some congestion on weston road and will create another way to get to jane street and shopping	12/20/2020 8:35 AM
70	Traffic congestion relief is needed on Rutherford Rd	12/15/2020 9:54 PM
71	I think it is very important to examine roundabouts along the extension but also within the surrounding Vaughan Mills precinct. The overconcentration of traffic lights along Rutherford has made that road unbearably slow, and at every intersection the turning lanes make it so wide as to be hostile to pedestrians and more dangerous to cars. The entire approach to widening roads and then adding ever more traffic lights is flawed, and as a result we have roads that are both slower and more dangerous than outside North America (or even in, say, Waterloo Region). On a less busy roadway like Bass Pro Mills, I hope planning staff will consider roundabouts rather than lights for environmental efficiency, safety, and driving experience. And a side point, though of perhaps less import, Bass Pro Mills is a bizarre name for a road. It seems like corporate sponsorship of municipal roadways and would be an embarrassing address. Perhaps there can be a better name.	12/15/2020 11:33 AM
72	I understand the extension of Bass Pro Mills will decrease some traffic on Rutherford but it will only create a huge bottleneck from where that extension meets Weston Road north on Weston Road to Rutherford. Weston Road between Langstaff and Rutherford is already bad and this will only make it worse (drive the bottleneck slightly north). Need to understand where most cars who will use the new extension will be heading when they reach Weston Road (i.e. stay on Weston and travel north of Rutherford or take Weston to turn and head West on Rutherford road). Anyone wanting to go north to Rutherford and then west on Rutherford will cut through Weston Downs (to Velmar or Babak to get onto Rutherford and avoid the disaster that already is and will be even more in the future - intersection of Weston and Rutherford). Cutting through the subdivision will be a definite concern to the community as there are already high volumes doing this in pm rush.	12/13/2020 6:18 PM
73	You've identified connectivity as the key issue, and that's great. I live at Weston Road and Rutherford Road, and this area is in desperate need of connectivity to the mall and to VMC. It would be great if the consideration given to increasing pedestrian and cycling routes kept in mind future expansion to VMC. I see VMC and Vaughan Mills as one large downtown area for Vaughan. It would be silly to not think about future connections to downtown. I would like to have the opportunity for me and my family to be able to bike and walk from my house in the area to VMC in the future. One other point, I know some consideration will need to be given how pedestrian and cycling routes will traverse Highway 400. Whatever is done, overbuilding a road or path over the highway will be tough. It would be great to incorporate paths alongside the highway as well so there are north-south options in addition to east-west routes. In addition, and I recognize this study won't get into this detail, with the land that opens up	12/12/2020 9:50 AM

## Bass Pro Mills Extension - Online Public Information Centre 1

because of this road extension, I sincerely hope the larger plan takes into account a mix of condos and commercial that isn't low-rise or industrial in nature. Low-rise industrial and commercial buildings are already there. We don't need more low-rise hotels or tile/home reno companies in the area. Office spaces, attracting employers and residents should be the priority along with greater transit options to get to the Vaughan Mills transit hub.

74	I live on the east side of highway 400 near Rutherford Rd in Maple. Since the modification to the center island preventing cars from making a left turn entering the Swiss Chalet plaza on the south east side of Rutherford and Weston, I have been blocked from finding a way into the plaza. The Bass pro extension would greatly improve my access to this location. I like riding my bike and riding on Rutherford Rd between Jane and Weston is dangerous. This alternative east west route was greatly needed when Vaughan Mills was constructed. Time has shown that matters have gotten much worse with a high amount of congestion at this intersection.	12/11/2020 1:08 PM
75	The information presented did not quantify the number of trips on this 1km section of road and how this 1km road would be able to reduce congestion on Rutherford Road. In order to be able to evaluate the information presented, we should be provided with more information including the origin/destination of trips on Rutherford Road. These survey questions directed us to assume that the extension of Bass Pro Mills Drive is the only solution. I thought this PIC was to receive feed back on the 5 alternatives but you did not seek any feedback on the other options. Intuitively a 1 km road (or very long driveway) that connects a residential area to a shopping mall will do nothing to reduce trips on Rutherford Road. This is not a necessary connection since these are choice trips made in off peak periods and are very infrequent. It has been suggested that this extension as necessary to improve congestion and yet, it is only 1km long and is terminating at a residential area. If you are truly interested in reducing congestion on E-W routes you would be looking at an alternate route that extends more than 1 block. A motorist would not get off Rutherford to take this link for 1 block. However, if somehow, motorists could stay off Rutherford longer then maybe this route would be attractive. If this connection was put into Waze, it likely would direct motorists to get on Bass Pro at Jane and travel to Weston and then direct motorists to travel through the Weston Downs community to get to Rutherford or Langstaff. So after spending all this money on the extension you would move motorists off the arterial road network and into a residential community. This is poor planning. If the objective is to get people on to other forms of transportation, the option of a pedestrian/bicycle path, should have been presented. A 1 km walk/ride to connect the residential community to the mall is much better use of our taxpayer dollars. Existing conditions shows that these trips are not being made today on Rutherford Road because one is risking their life riding next to cars, however if a safe walk/ride option was available, this may be a preferred mode choice. If you build it, they will come.	12/8/2020 7:52 PM
76	Main concern is the amount of traffic the new route will bring to the neighborhood bounded by Weston Rd to the west Langstaff Rd to the north and Rutherford road to the south. Safety concerns, excessive noise and air pollution would also increase dramatically to this area.	12/7/2020 9:32 PM
77	I live in Weston Downs and am concerned that this extension will add to the traffic infiltration in our community. I would fix other east west routes before attempting this project. I would sooner extend Langstaff from Creditstone To Keele. This Bass Pro extension will lead to Weston Rd which is congested now. If it has to be built, this street should not connect to Astona Blvd. This new street should have an offset entrance preferably north of Asonta in an effort to discourage drivers who want to cut thru Weston Downs. If not, this infiltration into Weston Downs will get worse. I also do not want to see any more condos or townhouses built on those subject lands.	12/7/2020 9:19 AM
78	Please extend bass pro mills to Weston road. I live in Weston downs and It'll help with traffic and make commuting to the mall easier.	12/5/2020 5:33 AM
79	Stop wasting time and get on with building this very important road connection and tell those idiots at York Region to start building the Langstaff Bridge road over the rail corridor. We should be proactive and not reactive. Maybe the City should take over the EA's & construction of all regional projects? Mr Mayor and Regional Councillors its time we fix this structure as it is no longer benefitting the City. Look at how long it took them to build Major Mackenzie Drive, thank God for MTO otherwise it would not have ever been reconstructed. The City of Vaughan should take it all over. I love my City but we need to build the infrastructure first or at the same time that we are approving all of this new Development. The traffic is killing us. The subway should also be extended to the hospital with a stop at the mall. Please ensure that this project protects for this future subway expansion to Major Mackenzie Drive. I am thinking about my children and it is too bad that we did not invest in public transit in the early years of Vaughan's	12/4/2020 4:57 PM

## Bass Pro Mills Extension - Online Public Information Centre 1

growth. I love the fact that you are thinking about in boulevard cycling lanes. I love this and hope I get my fat ass on a bike once in a while and cycle on a safe boulevard route up to Kleinburg / Humber Valley or down to Woodbridge or Toronto waterfront or somewhere up north as we old people need some exercise on the weekend or bike to work some day before I retire!! Take care and thank you for the opportunity.

80	Providing a highway 400 northbound ramp would greatly reduce traffic on Rutherford Rd.	12/4/2020 3:01 PM
81	The Vaughan Mills Secondary Plan, Schedule F notes several planned connections tying into the Bass Pro Mills overpass. Please ensure that through the design stages and final ESR of this EA that the minor and major collector roads (to the north and south of the new Bass Pro Mills Road) are appropriately protected for. This will ensure that through future block plan and site specific application stages, the future connections will be implemented.	12/3/2020 6:28 PM
82	I am a home owner in Weston Downs. We already have an issue with heavy volumes of traffic cutting through the sub division from Weston road to Rutherford road in the PM, and the reverse in the AM. It is important that the extension of Bass Pro Mills drive to Weston road does NOT connect directly to Astona road. Only LOCAL traffic (home owners, guests, and vehicles carrying people doing work IN the subdivision) should be allowed to access Weston Downs via Astona road in the PM. The same restrictions will be needed to prevent traffic from accessing Weston Downs from Rutherford road via Velmar or Babak in the AM.	12/3/2020 5:00 PM



**Q10 Please provide contact information of which you would like to receive future updates regarding the study to (i.e. Mailing Address, Email etc.)**

Answered: 62 Skipped: 45

Bass Pro Mills Extension - Online Public Information Centre 1

#	RESPONSES	DATE
1	[REDACTED]	1/9/2021 12:11 PM
2	[REDACTED]	1/9/2021 10:56 AM
3	[REDACTED]	1/9/2021 9:34 AM
4	[REDACTED]	1/9/2021 9:14 AM
5	[REDACTED]	1/9/2021 8:11 AM
6	[REDACTED]	1/9/2021 12:54 AM
7	[REDACTED]	1/8/2021 9:51 PM
8	[REDACTED]	1/8/2021 9:28 PM
9	[REDACTED]	1/8/2021 9:17 PM
10	[REDACTED]	1/8/2021 9:11 PM
11	[REDACTED]	1/8/2021 7:52 PM
12	[REDACTED]	1/8/2021 7:49 PM
13	[REDACTED]	1/8/2021 6:57 PM
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16	[REDACTED]	1/8/2021 5:34 PM
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25	[REDACTED]	1/8/2021 2:48 PM
26	[REDACTED]	1/8/2021 2:45 PM
27	[REDACTED]	1/8/2021 2:43 PM
28	[REDACTED]	1/8/2021 2:28 PM
29	[REDACTED]	1/8/2021 1:59 PM
30	[REDACTED]	1/8/2021 1:53 PM
31	[REDACTED]	1/8/2021 1:43 PM
32	[REDACTED]	1/8/2021 12:27 PM
33	[REDACTED]	1/8/2021 12:25 PM
34	[REDACTED]	1/8/2021 12:15 PM
35	[REDACTED]	1/8/2021 11:44 AM
36	[REDACTED]	1/8/2021 11:39 AM
37	[REDACTED]	1/8/2021 11:08 AM

Bass Pro Mills Extension - Online Public Information Centre 1

38	[REDACTED]	1/7/2021 9:58 PM
39	[REDACTED]	1/7/2021 1:07 PM
40	[REDACTED]	1/6/2021 11:08 AM
41	[REDACTED]	1/4/2021 10:58 AM
42	[REDACTED]	1/3/2021 12:58 PM
43	[REDACTED]	1/3/2021 12:41 PM
44	[REDACTED]	1/3/2021 10:41 AM
45	[REDACTED]	1/3/2021 9:34 AM
46	[REDACTED]	1/3/2021 9:03 AM
47	[REDACTED]	12/22/2020 4:48 PM
48	[REDACTED]	12/20/2020 12:43 PM
49	[REDACTED]	12/20/2020 11:43 AM
50	[REDACTED]	12/20/2020 9:37 AM
51	[REDACTED]	12/20/2020 9:05 AM
52	[REDACTED]	12/15/2020 11:33 AM
53	[REDACTED]	12/13/2020 6:18 PM
54	[REDACTED]	12/11/2020 1:08 PM
55	[REDACTED]	12/8/2020 7:52 PM
56	[REDACTED]	12/7/2020 9:32 PM
57	[REDACTED]	12/7/2020 9:19 AM
58	[REDACTED]	12/7/2020 9:02 AM
59	[REDACTED]	12/4/2020 3:01 PM
60	[REDACTED]	12/4/2020 7:58 AM
61	[REDACTED]	12/3/2020 6:39 PM
62	[REDACTED]	12/3/2020 5:00 PM



@ [Redacted]

Commented on Post by cityofvaughan "Bass Pro Mills Drive has proposed plans to extend providing a new ma..."

12 hours ago



[Redacted] Let's make it worse? It won't matter because this will get bogged down in council for years.

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In response to Comment "That will clog up... and create more gridlock."



@ [Redacted]

Commented on Post by cityofvaughan "Bass Pro Mills Drive has proposed plans to extend providing a new maj..."

2 hours ago



Horrible idea. Weston. rd from Langstaff to Rutherford is already a gridlock from 4-6pm and this will make it worse. I get it's a short period of time but Bass Mills is only busy during the Christmas Season one time a year not 52weeks a year!

[View Thread](#)



@ [Redacted]

Commented on Post by cityofvaughan "Bass Pro Mills Drive has proposed plans to extend providing a new maj..."

4 hours ago



What will be paved over? Wetlands? Why was this not done originally...where there stricter environmental regs in place?

[View Thread](#)



@ [Redacted]

Commented on Post by cityofvaughan "Bass Pro Mills Drive has proposed plans to extend providing a new maj..."

4 hours ago



People are missing the point. It will give another option for those heading east and west. The traffic on Weston road and Rutherford will actually be reduced.

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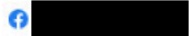
Commented on Post by cityofvaughan "Bass Pro Mills Drive has proposed plans to extend providing a new maj..."

5 hours ago



Maybe you should leave your office once and awhile so you could see the mess you have already created at Weston and Rutherford.

[View Thread](#)



Commented on City of Vaughan Wall Post "Bass Pro Mills Drive has proposed plans to extend providing a new ..."

5 hours ago



you mean Weston Road. That's the on ramp to the 400 southbound. It needs to be done to take pressure off Rutherford so that people can get to Weston Road from Basspro mills Dr.

[View Thread](#)



In response to Comment "Pretty sure that road... opened over 7yrs ago"



Commented on Post by cityofvaughan "Bass Pro Mills Drive has proposed plans to extend providing a new maj..."

6 hours ago



west rd is already the worst road in Woodbridge during rush hr

[View Thread](#)



In response to Comment "That will clog up... and create more gridlock."



Commented on Post by cityofvaughan "Bass Pro Mills Drive has proposed plans to extend providing a new maj..."

6 hours ago



How about you guys look at making the speed limits reasonable instead of putting them so low that you get a ticket for going a reasonable speed on an open road. Example. Teston road.

[View Thread](#)



Commented on Post by cityofvaughan "Bass Pro Mills Drive has proposed plans to extend providing a new maj..."

6 hours ago



This is a fantastic idea. And while you're at it, please put a northbound ramp onto the 400 to stop the clogging on Rutherford between 5-7pm.

[View Thread](#)



Commented on Post by cityofvaughan "Bass Pro Mills Drive has proposed plans to extend providing a new maj..."

6 hours ago



No it won't. People who frequent the Mall mostly live outside Woodbridge. If anything it will help reduce Rutherford congestion. Weston Road from 4-7 has always been a busy road. Pre-pandemic, of course. My question is what will that open space occupy, between Weston and the 400, south of Rutherford.

[View Thread](#)



In response to Comment "That will clog up... and create more gridlock."



Commented on Post by cityofvaughan "Bass Pro Mills Drive has proposed plans to extend providing a new maj..."

7 hours ago





yes please!!!

[View Thread](#)









In response to Comment "Maybe a ramp to... to cram onto Rutherford"

  [Redacted] 7 hours ago


Commented on Post by cityofvaughan "Bass Pro Mills Drive has proposed plans to extend providing a new maj..."





That will clog up Weston Rd even more and create more gridlock.

[View Thread](#)    



  [Redacted] 8 hours ago

Commented on Post by cityofvaughan "Bass Pro Mills Drive has proposed plans to extend providing a new maj..."

 🙄🙄🙄 Yes they need a northbound ramp as well





[View Thread](#)    



In response to Comment "Maybe a ramp to... to cram onto Rutherford"

  [Redacted] 8 hours ago

Commented on Post by cityofvaughan "Bass Pro Mills Drive has proposed plans to extend providing a new maj..."





Absolutely agree! 👍, great feedback...



[View Thread](#)    

  [Redacted] 8 hours ago

Commented on Post by cityofvaughan "Bass Pro Mills Drive has proposed plans to extend providing a new maj..."





Maybe a ramp to the northbound 400 also, so everyone isn't trying to cram onto Rutherford

[View Thread](#)    

  [Redacted] 8 hours ago

Commented on Post by cityofvaughan "Bass Pro Mills Drive has proposed plans to extend providing a new maj..."

No

[View Thread](#)    



[Redacted]

Commented on Post by cityofvaughan "Bass Pro Mills Drive has proposed plans to extend providing a new maj...

8 hours ago



This is a fantastic idea!

[View Thread](#)



[Redacted]

Commented on City of Vaughan Wall Post "Bass Pro Mills Drive has proposed plans to extend providing a new ...

8 hours ago



This message has been removed from facebook.com

Pretty sure that road was supposed to continue to Rutherford When Vaughanmills opened over 7yrs ago

[View Thread](#)



[Redacted]

Commented on Post by cityofvaughan "Bass Pro Mills Drive has proposed plans to extend providing a new maj...

9 hours ago



An extension to Weston Road. Why not? Great to alleviate the Rutherford and Langstaff traffic.

[View Thread](#)







**Online Public Information Centre  
#2 Summary Report**

Bass Pro Mills Drive MCEA, from  
Highway 400 to Weston Road

160540006

October 12, 2021

Prepared for:

City of Vaughan

Prepared by:

Stantec Consulting Ltd.




## ONLINE PUBLIC INFORMATION CENTRE #2 SUMMARY REPORT

This document entitled Online Public Information Centre #2 Summary Report was prepared by Stantec Consulting Ltd. ("Stantec") for the account of The City of Vaughan (the "Client"). Any reliance on this document by any third party is strictly prohibited. The material in it reflects Stantec's professional judgment in light of the scope, schedule and other limitations stated in the document and in the contract between Stantec and the Client. The opinions in the document are based on conditions and information existing at the time the document was published and do not take into account any subsequent changes. In preparing the document, Stantec did not verify information supplied to it by others. Any use which a third party makes of this document is the responsibility of such third party. Such third party agrees that Stantec shall not be responsible for costs or damages of any kind, if any, suffered by it or any other third party as a result of decisions made or actions taken based on this document.

Prepared by \_\_\_\_\_  
  
(signature)

**Jenn Robinson**

Approved by \_\_\_\_\_  
  
(signature)

**Diana Addley**



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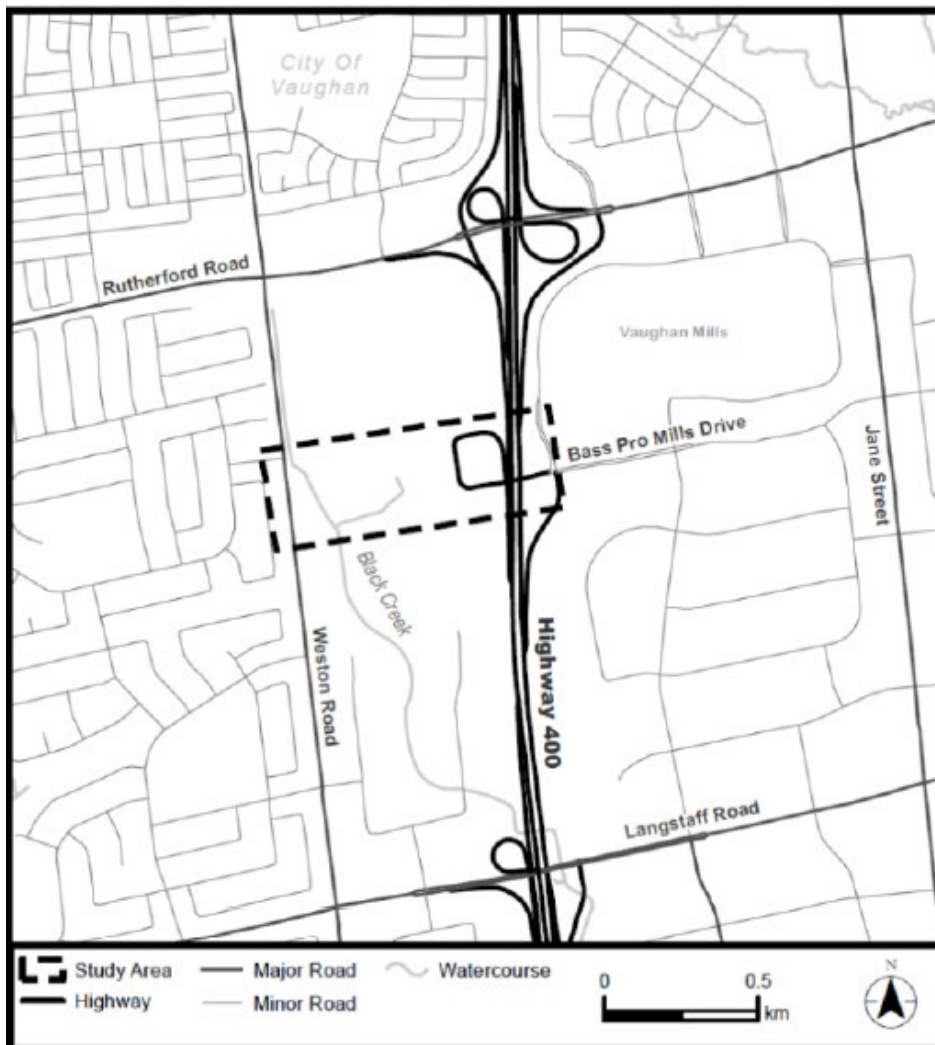


## 1.0 INTRODUCTION

Online Public Information Centre (PIC) 2 was held from August 19, 2021 to September 16, 2021 to present and solicit feedback on the study background, outcomes of Online PIC 1 (held from December 3, 2020 to January 8, 2021), the preliminary findings of the traffic analysis and environmental investigations, evaluation of alternative alignments and cross-sections, Technically Recommended Design and next steps in the study process. Due to COVID-19 and associated physical distancing requirements, PIC 2 was hosted online via the study website ([Vaughan.ca/BassProMillsEA](http://Vaughan.ca/BassProMillsEA)).

The study area for this is generally situated between Highway 400 and Weston Road, within the City of Vaughan, as shown on the Key Plan in **Figure 1** below:

**Figure 1: Key Plan**



## 2.0 PURPOSE

Online PIC 2 forms part of the overall consultation plan for the Bass Pro Mills Drive Municipal Class Environmental Assessment (Class EA) and was designed to inform and engage City residents and other stakeholders on study background, outcomes from the first PIC, the preliminary findings of the traffic analysis and environmental investigations, evaluation of alternative alignments and cross-sections, Technically Recommended Design and next steps in the study process.

## 3.0 LOCATION, DATE AND TIME

The Online PIC was hosted on the study website ([Vaughan.ca/BassProMillsEA](http://Vaughan.ca/BassProMillsEA)) from August 19, 2021 through September 16, 2021. A recorded presentation was available for online participants to listen to and/or read study information. Opportunities to offer feedback and ask questions were provided through an online survey provided at the end of the presentation, or by contacting the study team directly via telephone or email.

## 4.0 NOTIFICATION

The Notice of Online PIC 2 provided information about the PIC, including the date that the materials and online survey would become available for review online, where to access the materials and online survey, and a requested end date to provide feedback to the study team.

The Notice of Online PIC 2 was distributed to the public, agencies, utilities, stakeholders and First Nation communities through the methods outlined below in **Table 1**.

**Table 1: Notification Distribution**

Method of Distribution	Date of Distribution
Posted in the Thornhill Liberal local newspaper	August 12, 2021 September 2, 2021
Posted in the Vaughan Citizen local newspaper	August 12, 2021 September 2, 2021
Hardcopies mailed via Canada Post Ad Mail to 3,393 residents within a 200m radius of the study area.	August 18, 2021
Hardcopies mailed directly via Canada Post to residents backing directly onto Weston Road (from Astona Boulevard to 500m south of Astona Boulevard), external agencies, as well as members of the public who expressed an interest in the study, where addresses were available.	August 12, 2021



## ONLINE PUBLIC INFORMATION CENTRE #2 SUMMARY REPORT

Hardcopies mailed directly via Canada Post to First Nation communities	August 12, 2021
Emailed electronic copy to the contact list	August 12, 2021
Emailed electronic copy of notice, along with a Public Service Announcement, to the Technical Advisory Committee (TAC) and Stakeholder Group	August 12, 2021

A copy of the Notice of Online PIC 2 is available for review within **Appendix A**.

In addition to the above, the City of Vaughan published several social media ‘pushes’ to their Facebook, Twitter, Instagram and LinkedIn accounts on the dates below, encouraging the public to have their say on the proposed extension. While the August 19, 2021 social media push provided a link to the City’s Public Service Announcement (PSA), all other pushes provided direct links to the study website.

- August 19, 2021
- August 26, 2021
- September 2, 2021
- September 9, 2021
- September 16, 2021

The City’s PSA was published on August 19, 2021 through the City of Vaughan news website (<https://www.vaughan.ca/news>) and social media pushes on this day, encouraging the public to “Help shape the extension of Bass Pro Mills Drive” and to “Provide feedback and help inform the City’s proposed extension plan” by participating in Online PIC 2 through the provided links to the study website. The announcement also provided background information on the study, such as the recommendations made by the Vaughan Mills Centre Secondary Plan (VMCSP, 2014) and the City’s Transportation and Mobility Objectives that were outlined in the 2018 - 2022 Term of Council Service Excellence Strategic Plan. Additionally, banners highlighting the dates and website location of Online PIC 2 were also published using the City’s electronic banner system within and outside of City Hall throughout the duration of the comment period.

A copy of the wording and dates of the social media pushes, PSA and banners are available for review within **Appendix A**.

## 5.0 REFERENCE MATERIALS AND HANDOUTS

The following City of Vaughan Bass Pro Mills Extension Class EA displays were presented as part of Online PIC 2:

- Title Page
- PIC 2 Objectives
- Study Overview



## ONLINE PUBLIC INFORMATION CENTRE #2 SUMMARY REPORT

- Municipal Class Environmental Process
- Online Public Information Centre 1
- What Have We Heard?
- Simulation of Future Transportation Options
- Transportation Network Improvements
- Vaughan Mills Centre Secondary Plan
- Transportation Network Assessment (1)
- Transportation Network Assessment (2)
- Transportation Network Assessment (3)
- Roundabout Screening
- Natural Environment – Background
- Natural Environment – Site Surveys
- City of Vaughan Design Criteria
- Evaluation Process
- Alternative Cross-Sections (1)
- Alternative Cross-Sections (2)
- Alternative Cross-Sections (3)
- Alternative Cross-Sections (4)
- Evaluation of Alternative Cross-Sections
- Recommended Cross-Section
- Alternative Alignments
- Evaluation of Alternative Alignments
- Technically Recommended Design (3A) (1)
- Technically Recommended Design (3A) (2)
- Recommended Design – Highway 400 Bridge
- Proposed Black Creek Culvert
- Potential Future Black Creek Culvert Locations
- Headwater Drainage Feature Management Proposed Approach
- Environmental Impacts & Mitigation Measures
- Next Steps
- Thank you for attending!

A copy of the Online PIC 2 displays and accompanying script are included in **Appendix B**.



## 6.0 FORMAT

In light of COVID-19 and associated physical distancing requirements, the PIC was held online via the study website and included a pre-recorded presentation and an online survey through which to provide feedback. A summary of the online survey questions, and associated feedback received is provided in **Section 8.0**.

The Articulate Storyline presentation platform was used to encourage interaction throughout the presentation, allowing users to pause the presentation, or fast forward to sections of the presentation that interested them most and/or rewind to review information in more detail. The text of each narrated presentation slide was offered as part of the online PIC to provide both a visual and audio experience, and to help to ensure that accessibility needs were accommodated.

All attendees were encouraged to provide their feedback by September 16, 2021. As part of the Online PIC review and comment period, members of the study team were available to answer questions and/or respond to concerns submitted through the comment form provided on the study website, as well as via mail, phone, and email.

The information provided during Online PIC 2, including the displays and narration are provided within **Appendix B**, and a copy of the online survey can be reviewed within **Appendix D**.

## 7.0 PARTICIPATION

Statistics were gathered during the PIC period (i.e., August 19, 2021 to September 16, 2021) to determine the number of viewers of the online presentation. The website visit statistics were broken down into the following two categories:

- 1) **Unique Visitors** – the total number of people that visited the site. The same person visiting the site multiple times during the PIC time period is only counted once.
- 2) **Number of Visits** – the total number of visits by browsing session. If a visitor viewed another page on the site within 30 minutes of their last pageview, it is counted as the same visit. If a





## ONLINE PUBLIC INFORMATION CENTRE #2 SUMMARY REPORT

visitor returns to the study website 30 minutes after their last pageview, it is counted as a separate visit.

The data gathered for each category is illustrated below within **Table 2** and **Table 3**. It should be noted that while a daily value can be determined for the number of visits, the unique visitor statistic is only available on a monthly basis.

**Table 2: Number of Unique Visitors to the Study Website**

Unique Visitors	
August 2021	192 visitors
September 2021	117 visitors
<b>Total:</b>	<b>309 visitors</b>

**Table 3: Number of Visits to the Study Website**

Number of Visits	
August 19, 2021 to August 31, 2021	534 visits
September 1, 2021 to September 16, 2021	268 visits
<b>Total:</b>	<b>802 visits</b>

A copy of the raw statistics from the cPanel platform can be reviewed within **Appendix C**.

## 8.0 FEEDBACK

All Online PIC 2 participants were encouraged to provide feedback through a link to the online survey offered at the end of the PIC presentation. In addition, the study team contact information was provided as part of the Online PIC 2 notice, the study website, and the online presentation.

As part of the online survey, participants were asked to answer a series of questions and share their thoughts in relation to the evaluations of alternative cross-sections and alignments, the Technically Recommended Design, the format of the Online PIC and any additional comments they wished to provide. Participants were also encouraged to submit additional comments by emailing the study team directly. In total, 2 survey responses, 1 telephone call, 2 emails from the general public were received following the notification of Online PIC 2 and during the comment period. There were no comments received through the City's social media pushes.

Based on the online survey responses, telephone conversation, and email responses, the following key comment themes were noted:



## ONLINE PUBLIC INFORMATION CENTRE #2 SUMMARY REPORT

- Concern regarding traffic infiltration into adjacent residential communities.
- Concern regarding increased traffic on Weston Road.
- Concern for pedestrian/cyclist safety, especially near the Highway 400 ramp.
- Agreement that the extension will provide much needed alleviation of east-west traffic congestion on Rutherford Road.
- Positive feedback regarding the provision of active transportation facilities within the Technically Recommended Design.
- General satisfaction with the way information has been presented on the Online PIC 2 platform.

A copy of the online survey, as well as all comments and survey responses received is included within **Appendix E**. It should be noted that all names and addresses from respondents were included on the study mailing list.

### 9.0 NEXT STEPS

All comments received as part of Online PIC 2 have been reviewed and considered by members of the study team and will continue to be considered as the Class EA study progresses.

Next steps will include reviewing and considering feedback received as part of Online PIC 2, confirming the Preferred Design and preparing the Environmental Study Report (ESR). Upon finalization of the ESR, a Notice of Study Completion will be issued to initiate a 30-day public review period.

Consultation will continue to be ongoing, and all interested parties who have been requested to be included on the mailing list will be notified of key study milestones, including the Notice of Study Completion.



# Appendix A

## Notification Materials



# NOTICE OF ONLINE PUBLIC INFORMATION CENTRE 2

## MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT STUDY

### Bass Pro Mills Drive, from Highway 400 to Weston Road

#### THE STUDY

The City of Vaughan (City) is undertaking a Municipal Class Environmental Assessment (Class EA) study to assess the need to extend Bass Pro Mills Drive, from Highway 400 to Weston Road, as recommended in the Vaughan Mills Centre Secondary Plan (2014).

These recommendations were made to:

- provide a new east-west multi-modal connection between Highway 400 and Weston Road;
- help alleviate traffic congestion along Rutherford Road;
- support future growth and development within the plan area;
- create new multi-modal transportation connections; and,
- develop a safe and comfortable environment for active transportation users.

#### THE PROCESS

The study is being completed in accordance with the planning and design process for Schedule 'C' projects, as outlined in the Municipal Engineers Association (MEA) Municipal Class EA guidelines (October 2000, amended 2007, 2011 and 2015), which is approved under the Ontario Environmental Assessment Act. Upon study completion, an Environmental Study Report (ESR) will be made available for public review and comment. The ESR will document the study, consultation process and decision-making rationale.

#### ONLINE PUBLIC INFORMATION CENTRE 2

The City of Vaughan values the voice of its citizens and is dedicated to promoting a dialogue with the community that is open, transparent, accessible, and inclusive. Consultation with citizens is a key component of the study and as part of this Class EA process.

Online Public Information Centre 1 (PIC 1) for this study was held between December 3, 2020 and January 8, 2021, to present and obtain feedback on the study background, existing study area conditions, the evaluation of alternative solutions and next steps in the study process. A second PIC has been arranged to present and obtain feedback on the outcomes of PIC 1, the traffic analysis and environmental investigations, the evaluations of alternative alignments and cross-sections, the Technically Recommended Design, and the next steps in the study process. In light of COVID-19 and associated physical distancing requirements, PIC 2 will be hosted online. **Online PIC 2 will be available for your review on the project website at [Vaughan.ca/BassProMillsEA](https://vaughan.ca/BassProMillsEA) on August 19, 2021.** A narrated presentation will be available as part of the online PIC, as well as an online survey, until **September 16, 2021.**

#### CONTACT US

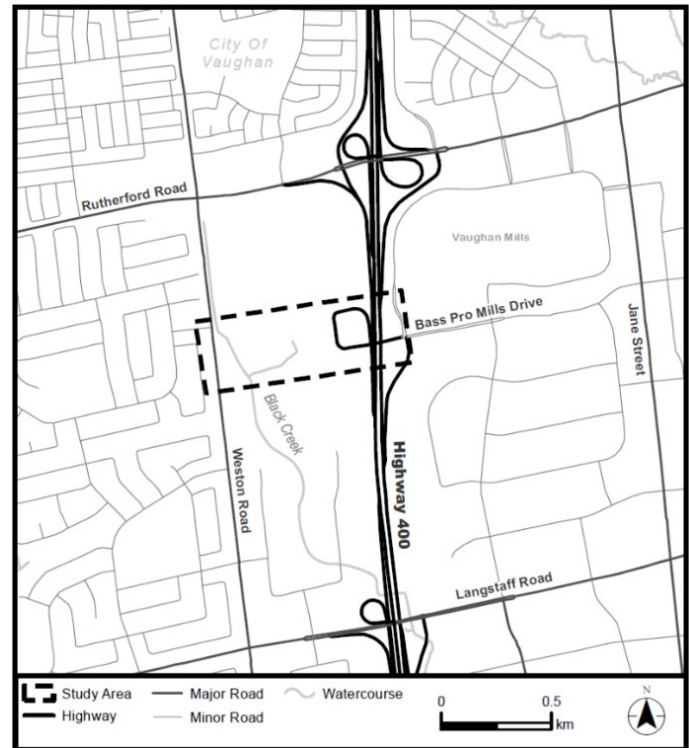
To join the study mailing list or to share comments, please contact:

**Hilda Esedebe, P.Eng.**  
**City of Vaughan**  
**Project Manager**  
2141 Major Mackenzie Dr.  
Vaughan, ON L6A 1T1  
T: 905-832-2281, ext. 8484

E: [Hilda.Esedebe@vaughan.ca](mailto:Hilda.Esedebe@vaughan.ca)

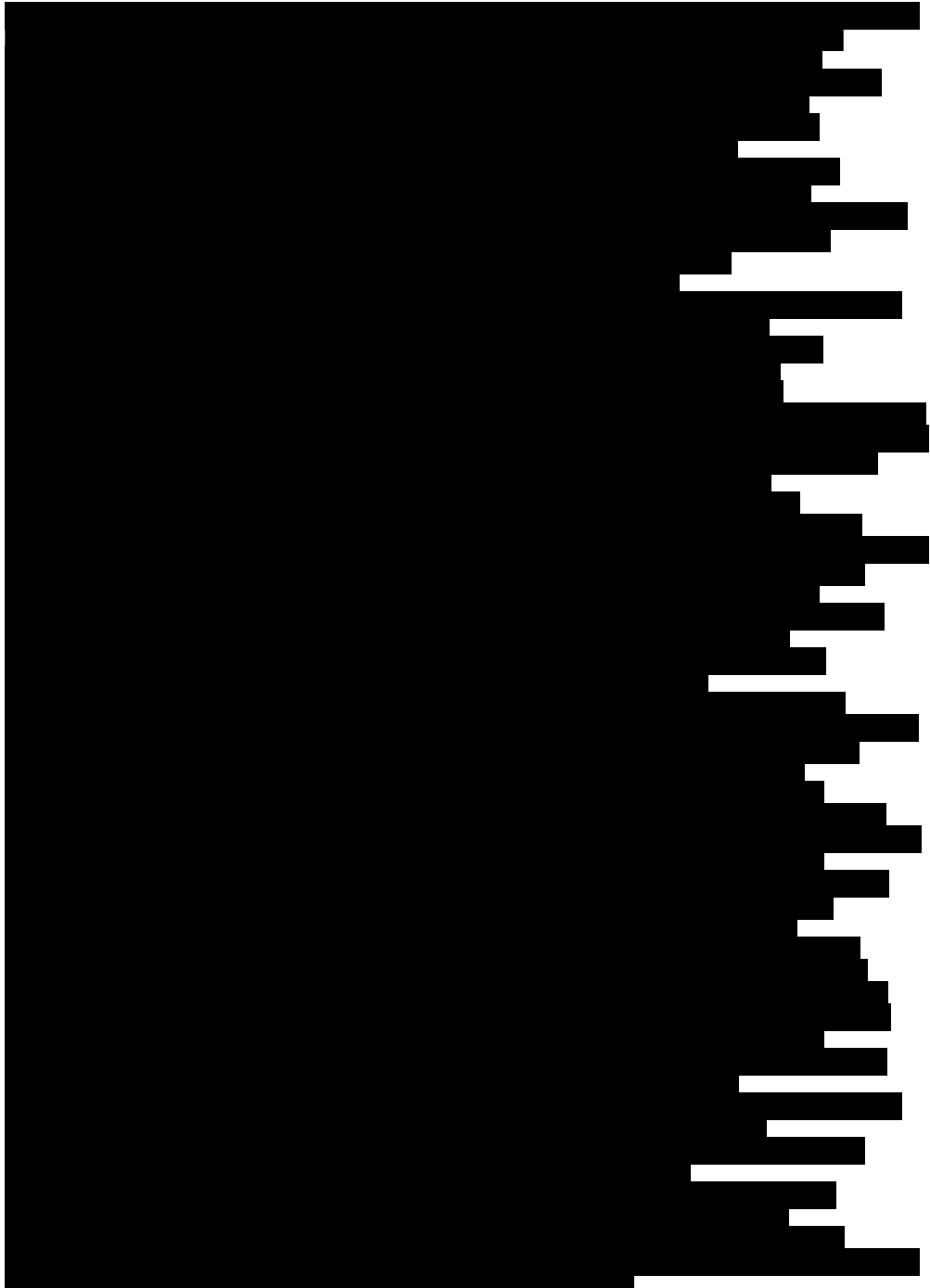
**Diana Addley**  
**Stantec Consulting Ltd.**  
**Senior Environmental Planner**  
300W-675 Cochrane Drive  
Markham, ON L3R 0A8  
T: 905-415-6401

E: [Diana.Addley@stantec.com](mailto:Diana.Addley@stantec.com)



Personal information on this form is collected under the authority of the Municipal Act, 2001 and will be used for the purpose of administering the Bass Pro Mills Environmental Assessment Study (between Highway 400 and Weston Road). Questions about this collection can be directed to the Manager, Transportation Planning, City of Vaughan, 2141 Major Mackenzie Drive, Vaughan, ON L6A 1T1, 905-832-8585.

**From:** [Robinson, Jennifer](#)  
**To:** [Robinson, Jennifer](#)  
**Cc:** [Hilda Esedebe](#); [Cholewa, Peter](#); [Addley, Diana](#)  
**Bcc:**



**Subject:** Notice of Online PIC 2 - MCEA Class EA, Bass Pro Mills Drive (Hwy 400 to Weston Rd)  
**Date:** Thursday, August 12, 2021 3:26:00 PM  
**Attachments:** [160540006\\_basspromills\\_notice\\_PIC2\\_20210727.pdf](#)

---

Hello,

Please see the attached Notice of Online Public Information Centre (PIC) 2 for the **Bass Pro Mills Drive Municipal Class Environmental Assessment (EA) Study**. As indicated within the attached notice, the City of Vaughan is undertaking this EA Study for the proposed extension of Bass Pro Mills Drive, between Highway 400 and Weston Road. The purpose of this notice is to inform of the Online Public Information

Centre that has been arranged to present and solicit feedback on the outcomes of PIC 1, the traffic analysis and environmental investigations, the evaluations of alternative alignments and cross-sections, the Technically Recommended Design and the next steps in the study process. As part of this online PIC, a recorded presentation and comment form will be available for your review and feedback on the project website ([Vaughan.ca/BassProMillsEA](http://Vaughan.ca/BassProMillsEA)) from **August 19, 2021 to September 16, 2021**.

Should you have any questions or concerns, please do not hesitate to contact us.

Regards,

**Jenn Robinson**

Environmental Planner, Transportation GTA

OSEC, Whitby Office

[Jennifer.Robinson@stantec.com](mailto:Jennifer.Robinson@stantec.com)

Stantec



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**CITY OF VAUGHAN  
NOTICE OF PUBLIC MEETING  
REGARDING PROPOSED AREA SPECIFIC DEVELOPMENT CHARGE  
BY-LAWS - EDGELEY POND AND PARK AND BLACK CREEK CHANNEL  
WORKS**

**COMMITTEE OF THE WHOLE MEETING  
Tuesday, September 14, 2021 at 1:00 P.M.  
Electronic Meeting**

TAKE NOTICE that at the Committee of the Whole meeting of September 14, 2021, a public meeting will be held by The Corporation of the City of Vaughan pursuant to Section 12 of the Development Charges Act, 1997, R.S.O., 1997 for the purpose of receiving input from the public on the following matters:

**Area Specific Development Charge Background Study and Proposed  
Development Charge By-Law - Edgeley Pond and Park and Black Creek  
Channel Works**

In order that sufficient information is available to the public, a copy of the background study and proposed by-law will be made available for viewing as of August 30, 2021 on the City of Vaughan website at [www.vaughan.ca/developmentcharges](http://www.vaughan.ca/developmentcharges).

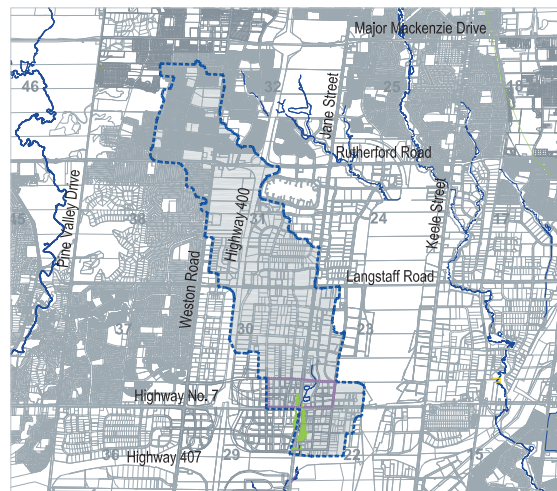
This is an Electronic Meeting. The Council Chamber will not be open to the public. Any member of the public may attend the public meeting to make an oral submission via electronic participation. If you wish to speak to this item, please pre-register by contacting Access Vaughan at 905-832-2281 or [clerks@vaughan.ca](mailto:clerks@vaughan.ca) by noon on the last business day before the meeting on Monday, September 13, 2021.

Alternatively, written submissions may be e-mailed to [clerks@vaughan.ca](mailto:clerks@vaughan.ca), and must be received in the Office of the City Clerk no later than 12:00 noon on Monday, September 13, 2021 in order to receive advance circulation. For further information call 905-832-8585 ext. 8482.

The Agenda, including any applicable Staff Report(s) on the above matters will also be posted at [www.vaughan.ca](http://www.vaughan.ca) on Tuesday, September 7, 2021.

This Notice is provided in accordance with the Development Charges Act, 1997, R.S.O., 1997 and Ontario Regulation 82/98.

**Todd Coles  
City Clerk**



AREA SPECIFIC  
DEVELOPMENT CHARGE  
Edgeley Pond and Black Creek  
Channel Works

KEY MAP

■ BENEFITING AREAS - MAP 1  
■ BENEFITING AREAS - MAP 2  
■ BENEFITING AREAS - MAP 3

NOT TO SCALE



June 8, 2016



**NOTICE OF ONLINE PUBLIC INFORMATION CENTRE 2  
MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT STUDY  
Bass Pro Mills Drive, from Highway 400 to Weston Road**

**THE STUDY**

The City of Vaughan (City) is undertaking a Municipal Class Environmental Assessment (Class EA) study to assess the need to extend Bass Pro Mills Drive, from Highway 400 to Weston Road, as recommended in the Vaughan Mills Centre Secondary Plan (2014). These recommendations were made to:

- provide a new east-west multi-modal connection between Highway 400 and Weston Road;
- help alleviate traffic congestion along Rutherford Road;
- support future growth and development within the plan area;
- create new multi-modal transportation connections; and,
- develop a safe and comfortable environment for active transportation users



**THE PROCESS**

The study is being completed in accordance with the planning and design process for Schedule 'C' projects, as outlined in the Municipal Engineers Association (MEA) Municipal Class EA guidelines (October 2000, amended 2007, 2011 and 2015), which is approved under the Ontario Environmental Assessment Act. Upon study completion, an Environmental Study Report (ESR) will be made available for public review and comment. The ESR will document the study, consultation process and decision-making rationale.

**ONLINE PUBLIC INFORMATION CENTRE 2**

The City of Vaughan values the voice of its citizens and is dedicated to promoting a dialogue with the community that is open, transparent, accessible, and inclusive. Consultation with citizens is a key component of the study and as part of this Class EA process.

Online Public Information Centre 1 (PIC 1) for this study was held between December 3, 2020 and January 8, 2021, to present and obtain feedback on the study background, existing study area conditions, the evaluation of alternative solutions and next steps in the study process. A second PIC has been arranged to present and obtain feedback on the outcomes of PIC 1, the traffic analysis and environmental investigations, the evaluations of alternative alignments and cross-sections, the Technically Recommended Design, and the next steps in the study process. In light of COVID-19 and associated physical distancing requirements, PIC 2 will be hosted online. Online **PIC 2 will be available for your review on the project website at [Vaughan.ca/BassProMillsEA](http://Vaughan.ca/BassProMillsEA) on August 19, 2021**. A narrated presentation will be available as part of the online PIC, as well as an online survey, until **September 16, 2021**.

**CONTACT US**

To join the study mailing list or to share comments, please contact:

**Hilda Esedebe, P.Eng.**  
**City of Vaughan**  
**Project Manager**  
2141 Major Mackenzie Dr.  
Vaughan, ON L6A 1T1  
T: 905-832-2281, ext. 8484  
E: [Hilda.Esedebe@vaughan.ca](mailto:Hilda.Esedebe@vaughan.ca)

**Diana Addley**  
**Stantec Consulting Ltd.**  
**Senior Environmental Planner**  
300W-675 Cochrane Drive  
Markham, ON L3R 0A8  
T: 905-415-6401  
E: [Diana.Addley@stantec.com](mailto:Diana.Addley@stantec.com)

Personal information on this form is collected under the authority of the Municipal Act, 2001 and will be used for the purpose of administering the Bass Pro Mills Environmental Assessment Study (between Highway 400 and Weston Road). Questions about this collection can be directed to the Manager, Transportation Planning, City of Vaughan, 2141 Major Mackenzie Drive, Vaughan, ON L6A 1T1, 905-832-8585.

This Notice was first issued August 12, 2021.



## NOTICE OF ONLINE PUBLIC INFORMATION CENTRE 2 MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT STUDY Bass Pro Mills Drive, from Highway 400 to Weston Road

### THE STUDY

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**Diana Addley**  
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Markham, ON L3R 0A8  
T: 905-415-6401  
E: [Diana.Addley@stantec.com](mailto:Diana.Addley@stantec.com)

Personal information on this form is collected under the authority of the Municipal Act, 2001 and will be used for the purpose of administering the Bass Pro Mills Environmental Assessment Study (between Highway 400 and Weston Road). Questions about this collection can be directed to the Manager, Transportation Planning, City of Vaughan, 2141 Major Mackenzie Drive, Vaughan, ON L6A 1T1, 905-832-8585.

This Notice was first issued August 12, 2021.

## NEWS

# 'GET INVOLVED'

## PARK COMING TO PETER RUPERT AVENUE AND RUTHERFORD ROAD AREA

DINA AL-SHIBEEB  
[dibrahim@yrmg.com](mailto:dibrahim@yrmg.com)

Vaughan citizens have until Sunday, Aug. 15 to bring their insights for "final design" of the new 7.7-hectare park coming to Peter Rupert Avenue and Rutherford Road, the city said July 29 on its website, urging people to "get involved."

The park will be segmented in different zones, one for children to play, another for event staging and concerts and one area for connections to trails and greenspace.

It will be built on approx-



Metroland file photo

You have until Aug. 15 to have your say on the new 7.7-hectare park coming to the city.

imately 28.5 hectares (70 acres) of greenspace that includes parkland, trails, stormwater ponds and the Cook Woodlot, "allowing for beautiful nature views," the City said July 29.

"This central location will also have direct connections to the Rutherford GO station and several nearby communities."

The city also described the proposed park as "dynamic, welcoming and inspiring gathering place

with a wide range of amenities for the entire community to enjoy."

Citizens can visit [vaughan.ca/PeterRupert-Park](http://vaughan.ca/PeterRupert-Park) to participate in the online self-guided public open house to:

- Learn about and provide feedback on the proposed park concept plans.
- Find out more about the design process and work completed to date.
- Hear about the next steps for the project.

# 'NOW IS THE TIME TO GET VACCINATED': SCHOOL BOARD

DINA AL-SHIBEEB  
[dibrahim@yrmg.com](mailto:dibrahim@yrmg.com)

York Region District School Board (YRDSB)'s official Twitter account is active with messages urging students to get vaccinated and making them aware of services around them to get the shot.

On July 22, the YRDSB also retweeted its message, saying how "COVID-19 infection can have serious consequences for all age groups, including young people."

"Now is the time to get vaccinated," it added.

"By working together to make sure everyone who is eligible to get a vaccine can get one, we can reduce the number of cases of COVID-19 in our community

and our school."

So far, the YRDSB has drawn criticism for introducing the hybrid model of education for next year. Its counterpart, the Catholic district school board, has axed the hybrid model for its elementary schools, but not for secondary. The YRDSB, however, stated that if enough students opt for in-person learning, there won't be any hybrid education.

Students aged 12 to 17 who haven't received their first dose "should get one as soon as possible or get your second dose if your last dose was at least 21 days ago."

For those who require assistance booking their appointment or with transportation to get to a clinic,

they can call Access York at 1-877-464-9675.

If anyone has questions or concerns about COVID-19 vaccinations, we encourage you to speak with your health care provider or call the York Region Health Connection line at 1-800-361-5653 (TTY: 1-866-512-6228) and speak with a public health nurse. They can provide you with reliable, up-to-date information to help you make an informed decision.

A total of 1,552,984 vaccines have been administered to York Region residents living anywhere in Ontario.

As of July 21, 845,804 York Region residents have received at least one dose and 707,180 have received two doses. That amounts to 81.5 per cent of adults 18 years and older have received one dose, while 69.5 per cent of adults are fully vaccinated with two doses.





## NOTICE OF ONLINE PUBLIC INFORMATION CENTRE 2 MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT STUDY Bass Pro Mills Drive, from Highway 400 to Weston Road

### THE STUDY

The City of Vaughan (City) is undertaking a Municipal Class Environmental Assessment (Class EA) study to assess the need to extend Bass Pro Mills Drive, from Highway 400 to Weston Road, as recommended in the Vaughan Mills Centre Secondary Plan (2014). These recommendations were made to:

- provide a new east-west multi-modal connection between Highway 400 and Weston Road;
- help alleviate traffic congestion along Rutherford Road;
- support future growth and development within the plan area;
- create new multi-modal transportation connections; and,
- develop a safe and comfortable environment for active transportation users



### THE PROCESS

The study is being completed in accordance with the planning and design process for Schedule 'C' projects, as outlined in the Municipal Engineers Association (MEA) Municipal Class EA guidelines (October 2000, amended 2007, 2011 and 2015), which is approved under the Ontario Environmental Assessment Act. Upon study completion, an Environmental Study Report (ESR) will be made available for public review and comment. The ESR will document the study, consultation process and decision-making rationale.

### ONLINE PUBLIC INFORMATION CENTRE 2

The City of Vaughan values the voice of its citizens and is dedicated to promoting a dialogue with the community that is open, transparent, accessible, and inclusive. Consultation with citizens is a key component of the study and as part of this Class EA process.

Online Public Information Centre 1 (PIC 1) for this study was held between December 3, 2020 and January 8, 2021, to present and obtain feedback on the study background, existing study area conditions, the evaluation of alternative solutions and next steps in the study process. A second PIC has been arranged to present and obtain feedback on the outcomes of PIC 1, the traffic analysis and environmental investigations, the evaluations of alternative alignments and cross-sections, the Technically Recommended Design, and the next steps in the study process. In light of COVID-19 and associated physical distancing requirements, PIC 2 will be hosted online. Online **PIC 2 will be available for your review on the project website at [Vaughan.ca/BassProMillsEA](http://Vaughan.ca/BassProMillsEA) on August 19, 2021.** A narrated presentation will be available as part of the online PIC, as well as an online survey, until **September 16, 2021.**

### CONTACT US

To join the study mailing list or to share comments, please contact:

**Hilda Esedebe, P.Eng.**  
City of Vaughan  
Project Manager  
2141 Major Mackenzie Dr.  
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T: 905-832-2281, ext. 8484  
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This Notice was first issued August 12, 2021.

# It's time to vote!

We received hundreds of truly inspiring nominations from newcomers coast to coast who have made a positive impact since their arrival in Canada. Now it's your turn to help chose this year's Top 25 Canadian Immigrant award winners!

13<sup>TH</sup> ANNUAL  
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CANADIAN  
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### THE STUDY

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- provide a new east-west multi-modal connection between Highway 400 and Weston Road;
- help alleviate traffic congestion along Rutherford Road;
- support future growth and development within the plan area;
- create new multi-modal transportation connections; and,
- develop a safe and comfortable environment for active transportation users



### THE PROCESS

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1ST CONSUMER SHOW IN 18 MONTHS!

SEPTEMBER 10-12

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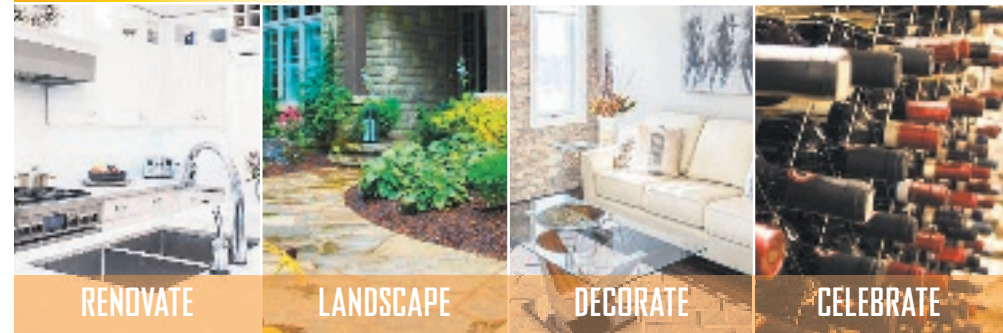
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# Public Service Announcement

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## Help shape the extension of Bass Pro Mills Drive

*Provide feedback and help inform the City's proposed extension plan*

**VAUGHAN, ON (Aug. 19, 2021)** – What should be considered for the proposed extension of Bass Pro Mills Drive? The City of Vaughan is hosting another virtual Public Information Centre as part of the Bass Pro Mills Extension Municipal Class Environmental Assessment (MCEA) Study. You can get involved and help shape the future of this road. The session, which includes a narrated presentation and comment form, is available until Thursday, Sept. 16 at [vaughan.ca/BassProMillsEA](https://vaughan.ca/BassProMillsEA).

As recommended in the [Vaughan Mills Centre Secondary Plan \(2014\)](#) (PDF), the study will assess the need to extend Bass Pro Mills Drive from Highway 400 to Weston Road. These recommendations were made to:

- provide a new east-west transportation network between Highway 400 and Weston Road, including a new route for York Region Transit.
- help distribute east-west traffic and alleviate congestion along Rutherford Road to the north.
- support future growth and development within the plan area.
- develop a safe and effective environment for active transportation users.

As part of the process, the Bass Pro Mills Extension MCEA Study will define problems and opportunities, identify solutions and determine a design for recommended improvements that align with the area's natural, social, economic and cultural environment. Upon study completion, an Environmental Study Report will be created.

Public engagement is vital to the development and success of this study. Through previous input opportunities, including a survey and an initial Public Information Session, the City gathered feedback on alternative solutions and shared key findings. Now, citizens are invited to get involved in the next stage of the study by participating in a second virtual Public Information Centre until Thursday, Sept. 16. In doing so, participants will:

- learn more about the project, including key findings collected and planned next steps.
- share thoughts on current challenges, potential solutions, active transportation opportunities and more for the study area.
- review alternative alignments and the recommended design for the road.
- understand potential impacts, such as traffic and environmental impacts, and proposed mitigation measures.

Get started at [vaughan.ca/BassProMillsEA](https://vaughan.ca/BassProMillsEA).

To be a fully connected and integrated community, the City is committed to undertaking initiatives like the proposed Bass Pro Mills Extension to improve the municipal road network, promote the development of

# Public Service Announcement

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transit and increase cycling and pedestrian-friendly infrastructure. This project also supports Vaughan's Transportation and Mobility objective – a priority outlined in the [2018-2022 Term of Council Service Excellence Strategic Plan](#).

-30-

**ABOUT VAUGHAN:** The City of Vaughan is one of Canada's fastest growing cities with a population of more than 335,000. Incorporated in 1991, Vaughan includes the communities of Concord, Kleinburg, Maple, Thornhill and Woodbridge. This culturally diverse municipality is located in the heart of York Region and the Greater Toronto Area.

**MEDIA CONTACT:** Teresa Fazari, Corporate and Strategic Communications  
T. 905-832-2281, ext. 8185 E. [teresa.fazari@vaughan.ca](mailto:teresa.fazari@vaughan.ca)

[vaughan.ca](http://vaughan.ca)



# Social Media

## Bass Pro Mills EA Study – Public Information Centre

**DATE:** Aug. 19 to Sept. 16, 2021

**EXTERNAL LEAD:** Teresa Fazari

**PCE LEAD REVIEW:** Catherine Vettese

### ORGANIC SOCIAL CAMPAIGN

Date	Platform	Message	Image
Aug. 19	Twitter Facebook Instagram	<p>What should be considered for the proposed extension of Bass Pro Mills Drive? Until Thursday, Sept. 16, participate in a virtual Public Information Centre to have your say on the City's extension plans. Learn more: <a href="#">PSA LINK</a></p> <p>Character count: 223 Friendly URL: <a href="http://vaughan.ca/BassProMillsEA">vaughan.ca/BassProMillsEA</a></p>	
Aug. 19	LinkedIn	<p>What should be considered for the proposed extension of Bass Pro Mills Drive? The City is hosting a virtual Public Information Centre, which includes a narrated presentation and comment form, as part of the Bass Pro Mills Extension Municipal Class Environmental Assessment Study. Until Thursday, Sept. 16, you can get involved and help shape the future of this road. Learn more: <a href="#">PSA LINK</a></p> <p>Character count: 410</p>	
Aug. 26	Twitter	<p>The City is carrying out a study for the extension of Bass Pro Mills Drive from Highway 400 to Weston Road. Alternative road designs, active transportation options and more are being considered. Have your say at <a href="http://vaughan.ca/BassProMillsEA">vaughan.ca/BassProMillsEA</a></p> <p>Character count: 237</p>	
Aug. 26	Facebook	<p>The City is carrying out a study for the extension of Bass Pro Mills Drive from Highway 400 to Weston Road. Alternative road designs, active transportation options, a new intersection and more are being considered. Until Sept. 16, participate in a virtual Public Information Centre to have your say. Get started at <a href="http://vaughan.ca/BassProMillsEA">vaughan.ca/BassProMillsEA</a></p> <p>Character count: 340</p>	
Aug. 26	Instagram	<p>The City's study to extend Bass Pro Mills Drive from Highway 400 to Weston Road is underway. Alternative road designs, active transportation</p>	

# Social Media

		<p>options, a new intersection and more are being considered. Until Sept. 16, participate in a virtual Public Information Centre to have your say. Get involved at <a href="http://vaughan.ca/BassProMillsEA">vaughan.ca/BassProMillsEA</a></p> <p>Character count: 328</p>	
Sept. 2	Twitter	<p>Have your say on the City's extension plan for Bass Pro Mills Drive, between Highway 400 and Weston Road. Visit <a href="http://vaughan.ca/BassProMillsEA">vaughan.ca/BassProMillsEA</a> until Sept. 16 to learn more about the study and help shape the future of this road.</p> <p>Character count: 223</p>	
Sept. 2	Facebook	<p>The Bass Pro Mills Drive extension study includes alternative solutions and recommendations for an extension between Highway 400 and Weston Road. This includes road designs, active transportation options, a new intersection and more. How should this area look? Visit <a href="http://vaughan.ca/BassProMillsEA">vaughan.ca/BassProMillsEA</a> to have your say.</p> <p>Character count: 309</p>	
Sept. 2	Instagram	<p>Have your say on the City's extension plan for Bass Pro Mills Drive. The study includes alternative solutions and recommendations for an extension between Highway 400 and Weston Road, including road designs, active transportation options, a new intersection and more. Get started at <a href="http://vaughan.ca/BassProMillsEA">vaughan.ca/BassProMillsEA</a></p> <p>Character count: 308</p>	
Sept. 9	Twitter	<p>There's still time to get involved! The City has initiated the Bass Pro Mills Drive extension study, which considers alternative solutions for the extension of the road from Highway 400 to Weston Road. Provide input until Sept. 16 at <a href="http://vaughan.ca/BassProMillsEA">vaughan.ca/BassProMillsEA</a></p> <p>Character count: 223</p>	
Sept. 9	Facebook Instagram	<p>The City has initiated a Bass Pro Mills Drive extension study to consider alternative solutions for the extension of the road between Highway 400 and Weston Road. There's still time to get involved! Provide input on proposed alignments, cross-sections, the recommended design and more until Sept. 16 at <a href="http://vaughan.ca/BassProMillsEA">vaughan.ca/BassProMillsEA</a></p> <p>Character count: 292</p>	

# Social Media

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Sept. 16	Twitter	<p>#Reminder: today is the last day to participate in the virtual Public Information Centre to review and comment on the City's Bass Pro Mills Drive extension study. Get started at <a href="http://vaughan.ca/BassProMillsEA">vaughan.ca/BassProMillsEA</a></p> <p>Character count: 209</p>	
Sept. 16	Facebook Instagram	<p>#Reminder: today is the last day to participate in the virtual Public Information Centre to review and comment on the City's Bass Pro Mills Drive extension study. Plans are in place to extend the road from Highway 400 to Weston Road. Get started at <a href="http://vaughan.ca/BassProMillsEA">vaughan.ca/BassProMillsEA</a></p> <p>Character count: 274</p>	

## NOTES:

- **Tags:** use .@TAG if the tag is at the start of the post. If the '.' is not inserted, it'll post as a private DM (not public). Tags will also need to be inserted manually per platform.
- **Hashtags:** ensure hashtags are official and widely used. Event-specific hashtags need to be pre-approved
- **URL:** use friendly URLs only, i.e. [vaughan.ca/news](http://vaughan.ca/news) vs [www.vaughan.ca/news/pages/default.aspx](http://www.vaughan.ca/news/pages/default.aspx)
- **Character count:** stick to 280 characters for Twitter and up to 400 characters for Facebook, Instagram and LinkedIn
- **Paid sponsored posts:** images for paid ads must have little to no text, or else it will get denied. Less than 1/3 of the image can have text.



**Help shape the extension  
of Bass Pro Mills Drive**

**Get involved!**

**Virtual Public Information Centre  
[vaughan.ca/BassProMillsEA](http://vaughan.ca/BassProMillsEA)**





# Help shape the extension of Bass Pro Mills Drive

Provide feedback and help inform  
the City's proposed extension plan

# **Appendix B**

## **Online PIC 2 Presentation (Displays and Script)**



## **Bass Pro Mills Extension - Online Public Information Centre 2**

### ***1.1 Online Public Information Centre 2***



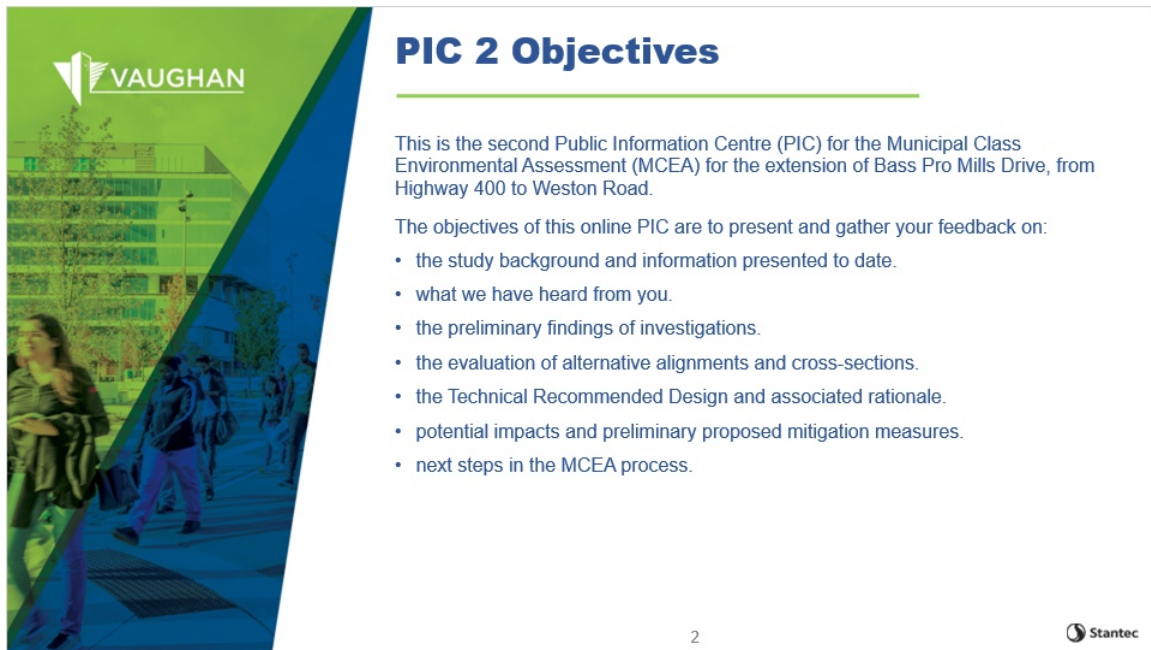
Hello, and thank you for joining us for this Online Public Information Centre for the Bass Pro Mills Extension Schedule C Municipal Class Environmental Assessment study.

This video presentation will provide you with an overview of this Municipal Class Environmental Assessment (or MCEA) study. A PDF copy of this presentation is also available for your review by clicking the Resources tab located at the top right of this presentation.

This is the second and final Public Information Centre for this study. In light of COVID-19 and associated physical distancing requirements, this PIC is being hosted online.

Please stay tuned until the end of the presentation to discover ways in which you can provide valuable feedback to the project team.

## 1.2 PIC 2 Objectives




### PIC 2 Objectives

This is the second Public Information Centre (PIC) for the Municipal Class Environmental Assessment (MCEA) for the extension of Bass Pro Mills Drive, from Highway 400 to Weston Road.

The objectives of this online PIC are to present and gather your feedback on:

- the study background and information presented to date.
- what we have heard from you.
- the preliminary findings of investigations.
- the evaluation of alternative alignments and cross-sections.
- the Technical Recommended Design and associated rationale.
- potential impacts and preliminary proposed mitigation measures.
- next steps in the MCEA process.

2 

As noted, this is the second and final Public Information Centre (or PIC) for this Municipal Class Environmental Assessment study for the proposed extension of Bass Pro Mills Drive, from Highway 400 to Weston Road.

The objectives of this online PIC are to present and gather your feedback on:

- The study background and information presented at PIC 1
- What we have heard from you
- The preliminary findings of investigations
- The evaluation of alternative alignments and cross-sections
- The Technical Recommended Design and associated rationale
- Potential impacts and preliminary proposed mitigation measures
- Next steps in the MCEA process

### 1.3 Study Overview




## Study Overview




The proposed extension of Bass Pro Mills Drive is envisaged to support future development in the study area, including the employment and intensification plans developed as part of the Vaughan Mills Centre Secondary Plan (VMCSP).

3

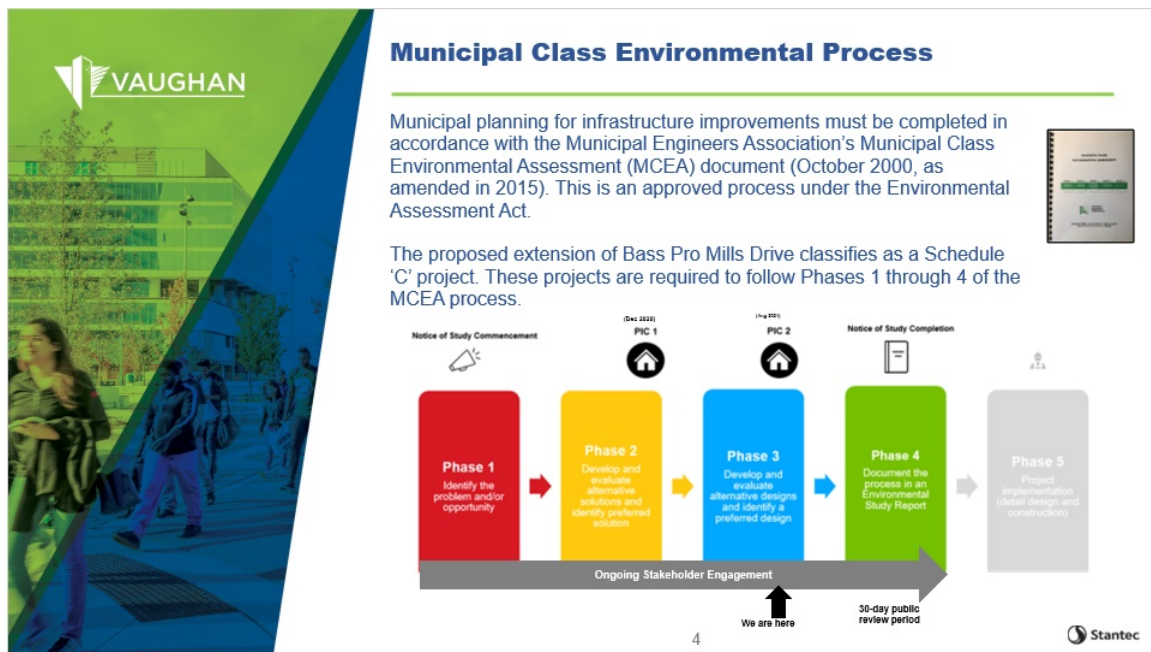


The City of Vaughan is undertaking this Municipal Class Environmental Assessment (or MCEA) study for the proposed extension of Bass Pro Mills Drive, from Highway 400 westerly to Weston Road. This extension would provide a new major collector roadway that unites neighbourhoods from Weston Road to Jane Street, redistributes east-west traffic and alleviates congestion on Rutherford Road.

An enhanced boulevard could accommodate new York Region Transit amenities, a pedestrian friendly multi-use trail, as well as on-street cycling facilities.

The proposed extension of Bass Pro Mills Drive is envisaged to support future development in the study area, which is outlined within the figure on this slide, including the employment and intensification plans developed as part of the Vaughan Mills Centre Secondary Plan (VMCSP).

## 1.4 Municipal Class Environmental Process



Municipal planning for infrastructure improvements must be completed in accordance with the Municipal Engineers Association's Municipal Class Environmental Assessment (MCEA) document, which is an approved process under the Environmental Assessment Act.

The proposed extension of Bass Pro Mills Drive classifies as a Schedule 'C' project. These projects are required to follow Phases 1 through 4 of the MCEA process.

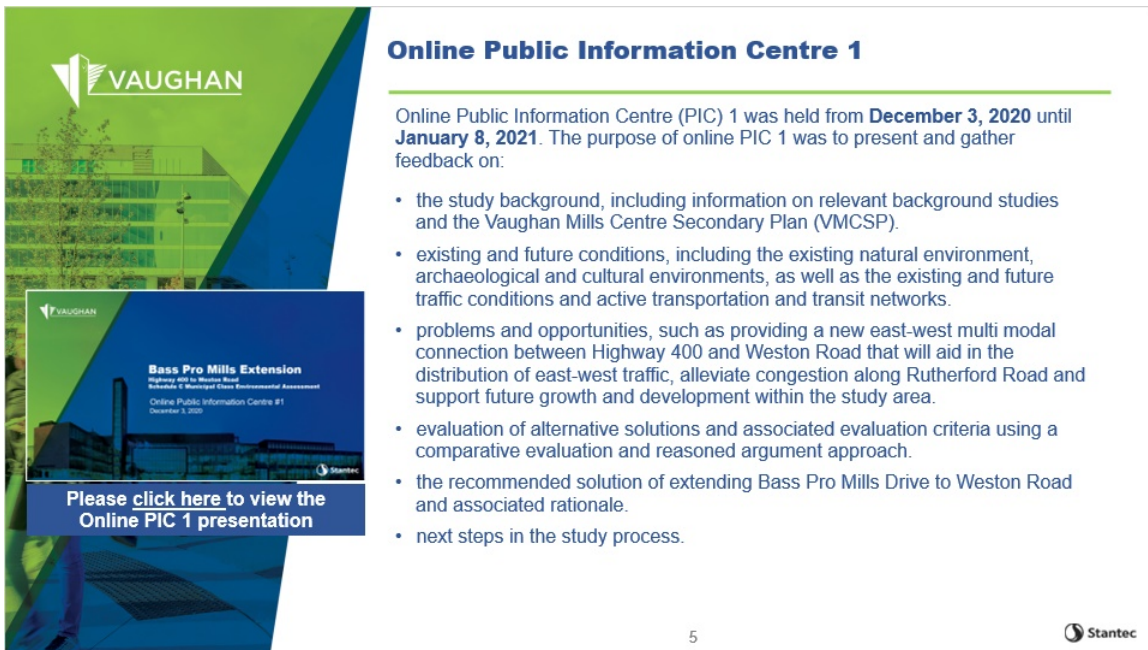
The four phases of the Municipal Class Environmental Process are:

- Phase 1 – Identify the problem and/or opportunity
- Phase 2 – Develop and evaluate alternative solutions and identify a preferred solution
- Phase 3 – Develop and evaluate alternative designs and identify a preferred design
- Phase 4 – Document the process in an Environmental Study Report, which is available for a 30-day public review period before the project is able to move on to the next and final phase of the MCEA process, Project Implementation.

As indicated within the timeline figure, consultation with the public and community stakeholders is carried out throughout the study process and you are encouraged to participate and share your ideas with members of the study team at any time.

At this time, we are currently within Phase 3 of the study process, and we will confirm the preferred design once we have reviewed and considered the feedback received during this Online PIC.

## 1.5 Online Public Information Centre 1




**Online Public Information Centre 1**

Online Public Information Centre (PIC) 1 was held from **December 3, 2020** until **January 8, 2021**. The purpose of online PIC 1 was to present and gather feedback on:

- the study background, including information on relevant background studies and the Vaughan Mills Centre Secondary Plan (VMCSP).
- existing and future conditions, including the existing natural environment, archaeological and cultural environments, as well as the existing and future traffic conditions and active transportation and transit networks.
- problems and opportunities, such as providing a new east-west multi modal connection between Highway 400 and Weston Road that will aid in the distribution of east-west traffic, alleviate congestion along Rutherford Road and support future growth and development within the study area.
- evaluation of alternative solutions and associated evaluation criteria using a comparative evaluation and reasoned argument approach.
- the recommended solution of extending Bass Pro Mills Drive to Weston Road and associated rationale.
- next steps in the study process.

Please click here to view the Online PIC 1 presentation

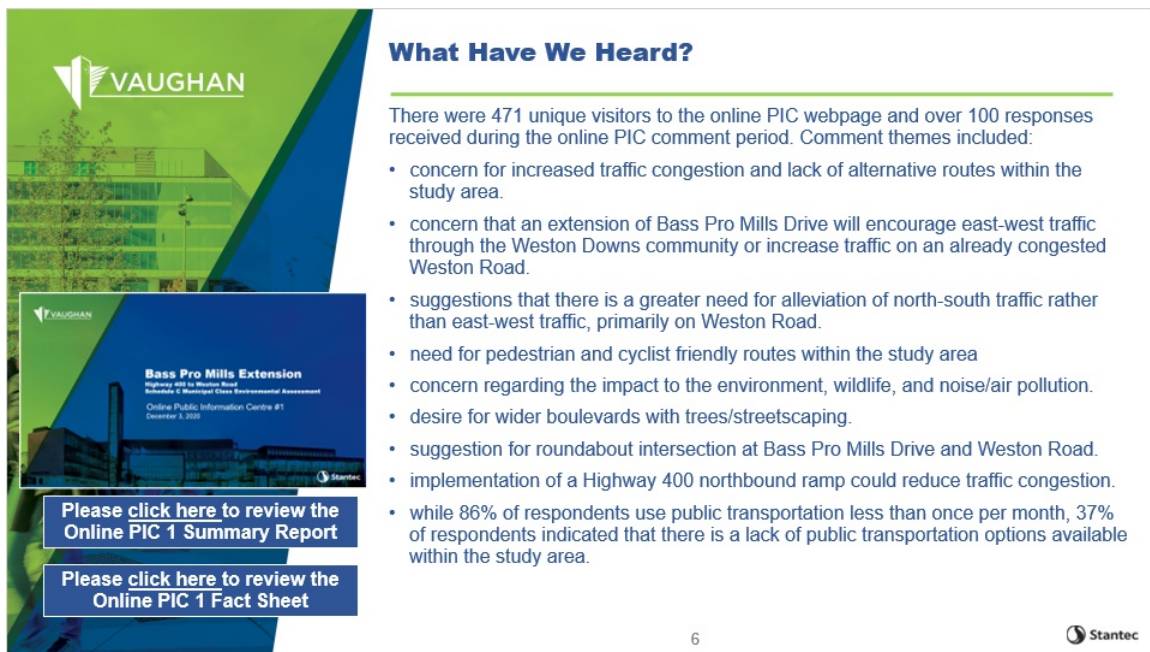
5 

As noted, this is the second and final PIC for this study. Online PIC 1 was held from December 3, 2020 until January 8, 2021 via a recorded presentation on the project website. The purpose of Online PIC 1 was to present and gather feedback on:

- The study background, including the Vaughan Mills Centre Secondary Plan (VMCSP) and other relevant policy directives
- Existing natural and technical conditions within the study area, as well as existing and future transportation conditions
- The Problem and Opportunity statement for the project
- The evaluation of Alternative Solutions and the recommended solution, which is to extend Bass Pro Mills Drive from its existing terminus at Highway 400, westerly to Weston Road
- Next steps in the study process

Please use the link on the left side of the slide to review the materials presented as part of Online Public Information Centre 1.

## 1.6 What Have We Heard?




**What Have We Heard?**

There were 471 unique visitors to the online PIC webpage and over 100 responses received during the online PIC comment period. Comment themes included:

- concern for increased traffic congestion and lack of alternative routes within the study area.
- concern that an extension of Bass Pro Mills Drive will encourage east-west traffic through the Weston Downs community or increase traffic on an already congested Weston Road.
- suggestions that there is a greater need for alleviation of north-south traffic rather than east-west traffic, primarily on Weston Road.
- need for pedestrian and cyclist friendly routes within the study area
- concern regarding the impact to the environment, wildlife, and noise/air pollution.
- desire for wider boulevards with trees/streetscaping.
- suggestion for roundabout intersection at Bass Pro Mills Drive and Weston Road.
- implementation of a Highway 400 northbound ramp could reduce traffic congestion.
- while 86% of respondents use public transportation less than once per month, 37% of respondents indicated that there is a lack of public transportation options available within the study area.

Please click here to review the Online PIC 1 Summary Report

Please click here to review the Online PIC 1 Fact Sheet

6 


There were 471 unique visitors to the Online PIC webpage and over 100 responses received during the Online PIC comment period. The main comment themes we heard included:

- Concern for increased traffic congestion and lack of alternative routes within the study area.
- Concern that an extension of Bass Pro Mills Drive will encourage east-west traffic through the Weston Downs community or increase traffic on an already congested Weston Road.
- Suggestions that there is a greater need to alleviate north-south traffic, rather than east-west traffic, primarily on Weston Road.
- The desire for pedestrian and cyclist friendly routes within the study area, such as through separated bike lanes, sidewalks, multi-use paths and wider boulevards with street trees and other landscape features.
- Concerns related to the potential to impact natural environmental features or increases in noise and air pollution.
- A suggestion for a roundabout intersection at Bass Pro Mills Drive and Weston Road to help alleviate traffic congestion; and
- A lack of public transportation options available within the study area.

Please use the link on the left side of this slide to review the Online Public Information Centre 1 Summary Report and Fact Sheet.




### 1.7 Simulation of Future Transportation Options



#### Simulation of Future Transportation Options

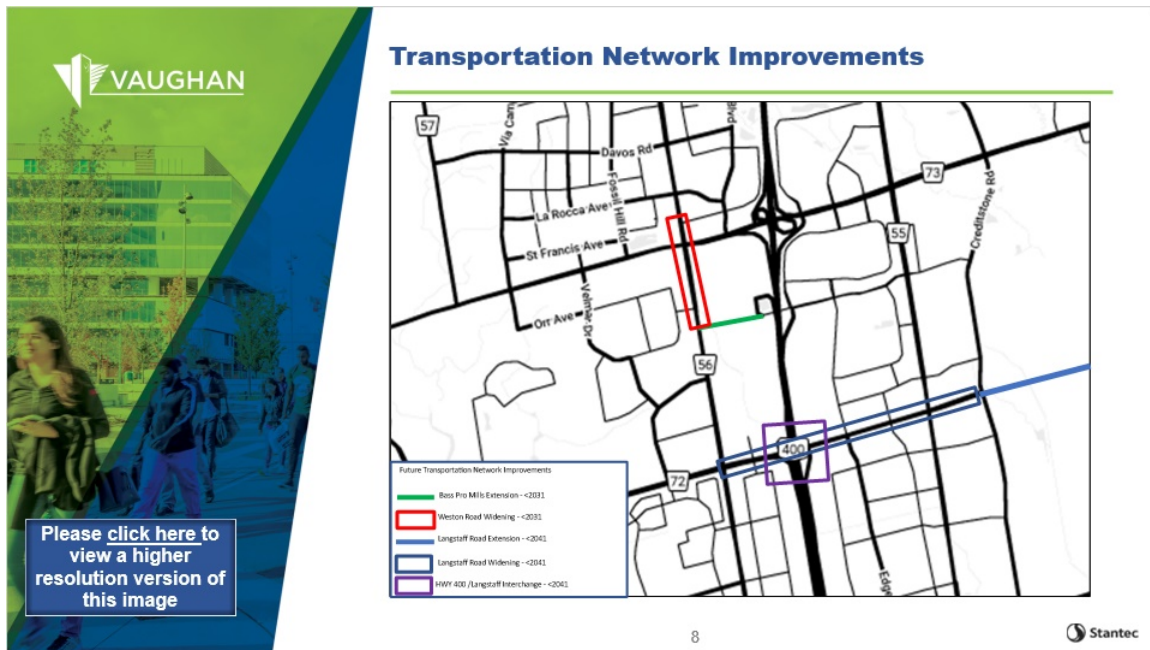
<p><b>Option 0</b></p> <ul style="list-style-type: none"> <li>• Future Base Conditions (2031 and 2041 horizons)</li> </ul>	<ul style="list-style-type: none"> <li>• 2041 horizon show impacts from Langstaff Road extension to Highway 7 and its widening between Weston Road and east of Jane Street</li> </ul>
<p><b>Option A</b></p> <ul style="list-style-type: none"> <li>• Future Conditions with Bass Pro Mills Drive extension (2031 and 2041 horizons)</li> </ul>	<ul style="list-style-type: none"> <li>• Includes Bass Pro Mills Drive Extension and Weston Road Widening north of Bass Pro Mills Extension to Hawkview Boulevard</li> </ul>
<p><b>Option B</b></p> <ul style="list-style-type: none"> <li>• Future Conditions with Bass Pro Mills Drive extension and VMCS (2031 and 2041 horizons)</li> </ul>	<ul style="list-style-type: none"> <li>• Includes 2014 Vaughan Mills Centre Secondary Plan road network and trips</li> </ul>

7 

To assess future transportation operations within the study area, three future options were developed and reviewed, including:

- Option 0 - Future Base Conditions, which assessed future traffic operations in the 2031 and 2041 horizon years. As part of this option, existing conditions were maintained; however, the approved infrastructure plan that was considered in this analysis was the Langstaff Road extension to Highway 7, and its associated widening between Weston Road east of Jane Street.
- Option A – Future Conditions with the extension of Bass Pro Mills Drive in the 2031 and 2041 horizon years. As part of this option, travel conditions with the proposed extension of Bass Pro Mills Drive, as well as the future widening of Weston Road, north of Bass Pro Mills Drive to Hawkview Boulevard, were assessed.
- Option B – Future Conditions with the extension of Bass Pro Mills Drive and full build out of the 2014 Vaughan Mills Centre Secondary Plan in the 2031 and 2041 horizon years. This option is similar to Option A, although the future Vaughan Mills Centre Secondary Plan internal road network and estimated trips generated by that proposed development were considered in the analysis.

## 1.8 Transportation Network Improvements





Several road modifications are planned in the vicinity of the study area that will enhance the transportation network's connectivity by providing additional east-west connections and additional access to and from Highway 400. The location of these future transportation network improvements are shown on this slide.

By 2031, Weston Road, between Bass Pro Mills Drive and Hawkview Boulevard, is expected to be widened to include an additional travel lane in each direction. The approximate location of this area is highlighted in red on this slide. In addition, the proposed extension of Bass Pro Mills Drive is anticipated to be constructed by 2031. The location of this proposed project is highlighted in green on this slide.

By 2041, the following roadway modifications are anticipated to be constructed:

- The extension of Langstaff Road to Highway 7 (highlighted in light blue);
- The widening of Langstaff Road, from west of Weston Road easterly to Creditstone Road (outlined in blue); and
- Additional ramps at the interchange of Langstaff and Highway 400, that will provide access from Langstaff Road to Highway 400 northbound and from Highway 400 southbound to Langstaff Road (outlined in purple).

### 1.9 Vaughan Mills Centre Secondary Plan





#### Vaughan Mills Centre Secondary Plan

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Vehicular trips associated with the 2014 Vaughan Mills Centre Secondary Plan (VMCSP) road network were modelled as part of **Option B** (Future Conditions with Bass Pro Mills Drive extension and VMCSP (2031 and 2041 horizons) in study area.

VMCSP Future Trips	AM Peak		PM Peak	
	Inbound	Outbound	Inbound	Outbound
<b>East of Highway 400</b>	852	828	1,183	1,277
<b>West of Highway 400</b>	2,839	468	996	2,970
<b>Total</b>	3,691	1,296	2,179	4,247

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As noted on Slide 7, the number of trips generated by the road network proposed as part of the 2014 Vaughan Mills Centre Secondary Plan were included in the analysis of Option B, which assessed the future traffic conditions with the extension of Bass Pro Mills Drive and implementation of the 2014 VMCSP for the 2031 and 2041 horizon years.

The future development of the Vaughan Mills Centre Secondary Plan east of Highway 400 would use the existing road network.

The internal road network conceptually outlined as part of the 2014 Vaughan Mills Centre Secondary Plan, and located west of Highway 400, was added to the road network for the analysis of Option B.

Based on the findings of the analysis, the main access points to this potential development would be at Rutherford Road and Bass Pro Mills Drive. In addition, the total vehicular trips estimated for the future Vaughan Mills Centre Secondary Plan was approximately 5,000 in the AM peak hour and 6,400 in the PM peak hour.

### 1.10 Transportation Network Assessment



#### Transportation Network Assessment

**Transportation Analysis – Study Area Network Delay Performance**

- Transportation simulation results showed that Option A (Bass Pro Mills Drive Extension and Weston Road Widening) will result in overall reduction in delay and operational improvements in the area.
- The proposed improvements in Option A will result in approximately 10% improvement in 2031 and above 22% improvement in 2041.
- Option A provides better connectivity and greater capacity in the road network.


Horizon Year	Peak Hour	Delay Values			Delay Reduction %	
		Option 0	Option A	Option B	Option A	Option B
2031	AM	125	113	130	-10%	4%
	PM	149	133	136	-11%	-9%
2041	AM	152	113	135	-26%	-11%
	PM	137	106	117	-22%	-15%

The network-wide impacts of each option were determined as part of the traffic analysis, and the delay values shown on this slide provide the quantitative measurements of aggregate changes within the transportation network associated with each option.

The results of the analysis showed that Option A, which assumed implementation of the extension of Bass Pro Mills Drive and widening of Weston Road, will result in an overall reduction in travel delays in the area. The proposed improvements associated with Option A will also result in an over 10% improvement in operations in 2031, and over 22% in 2041.

The operational improvements forecasted for 2041 is due to the improved connectivity and capacity provided within the transportation road network.

### 1.11 Transportation Network Assessment




## Transportation Network Assessment

**Transportation Analysis – Study Area Network Delay Performance**

Intersection	Future Base 2031				Option A 2031				Option B 2031			
	AM Peak Hour		PM Peak Hour		AM Peak Hour		PM Peak Hour		AM Peak Hour		PM Peak Hour	
	Delay(s)	LOS	Delay(s)	LOS	Delay(s)	LOS	Delay(s)	LOS	Delay(s)	LOS	Delay(s)	LOS
Weston Road at Rutherford Road	118	F	133	F	113	F	140	F	136	F	123	F
Weston Road at Astona Blvd	20	C	81	F	27	C	79	E	23	C	61	E
Weston Road at Bass Pro Mills Drive	NA	NA	NA	NA	50	D	83	F	45	D	57	E
Weston Road at Greenpark Crestmount	82	F	130	F	86	F	28	C	114	F	30	C
Weston Road at Langstaff Road	112	F	165	F	89	F	114	F	137	F	151	F

A comparison of Level of Service (LOS) results along Weston Road shows improvements in operations while further traffic will be served by Option A (Bass Pro Mills Drive Extension and Weston Road Widening)

Level of Service (LOS)	Delay (seconds)	Assessment
A	≤10s	Acceptable
B	>10s and ≤20s	Acceptable
C	>20s and ≤35s	Acceptable
D	>35s and ≤55s	Acceptable
E	>55s and ≤80s	Increased Delays
F	>80s	High Delay


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A comparison of the operational Level of Service in 2031 at signalized intersections located along Weston Road is provided in this slide. The Level of Service (or LOS) is a way to measure the free flow of traffic based on the performance of signalized intersection operations, and is used to determine how well a transportation facility is operating from a traveler’s perspective and shows an average of how long it will take a vehicle from the time it reaches a signal to the time it leaves the intersection.

Level of Service is expressed in terms of traffic delays and is represented by the letters A through F. Level of Service of ‘A’ represents free-flow traffic conditions with the lowest delays, while Level of Service of ‘F’ represents a breakdown in traffic flow with stop-and-go conditions, where drivers will face the greatest extended delays. In general, a Level of Service of A, B, C or D, shown in green within the table shown on this slide, represents an acceptable Level of Service. Levels of Service E or F, shown in red on this slide, indicate when motorists will start to feel increased delays or significantly higher delays, respectively. The LOS groups A to D, colored in green, are acceptable. LOS E is when the drivers start to experience congestion and increased delays. LOS F highlighted in red indicates high delays.

A comparison between baseline and future options shows traffic congestion will improve in Option A while further traffic will be served.

### 1.12 Transportation Network Assessment



## Transportation Network Assessment

Transportation Analysis – Study Area Network Delay Performance

Intersection	Future Base 2041				Option A 2041				Option B 2041			
	AM Peak Hour		PM Peak Hour		AM Peak Hour		PM Peak Hour		AM Peak Hour		PM Peak Hour	
	Delay(s)	LOS	Delay(s)	LOS	Delay(s)	LOS	Delay(s)	LOS	Delay(s)	LOS	Delay(s)	LOS
Weston Road at Rutherford Road	163	F	125	F	103	F	100	F	148	F	105	F
Weston Road at Astona Blvd	51	D	30	C	59	E	12	B	30	C	11	B
Weston Road at Bass Pro Mills Drive	NA	NA	NA	NA	57	E	23	C	38	D	36	D
Weston Road at Greenpark/ Crestmount	113	F	28	C	111	F	15	B	76	E	15	B
Weston Road at Langstaff Road	270	F	242	F	113	F	114	F	112	F	130	F


A comparison of Level of Service (LOS) results along Weston Road in 2041 shows improvements in operations while further traffic will be served by Option A (Bass Pro Mills Drive Extension and Weston Road Widening)

Level of Service (LOS)	Delay (seconds)	Assessment
A	≤10s	Acceptable
B	>10s and ≤20s	Acceptable
C	>20s and ≤35s	Acceptable
D	>35s and ≤55s	Acceptable
E	>55s and ≤80s	Increased Delays
F	>80s	High Delay

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The Level of Service expected in 2041 is shown on this slide. The preliminary results show that Option A generally provides a better Level of Service along Weston Road in relation to the three options.


### 1.13 Roundabout Screening



## Roundabout Screening

A roundabout intersection at Bass Pro Mills Drive and Weston Road was considered; however, is not recommended because:

- additional spatial and right of way requirements.
- operational challenges related to the unequal approach traffic volumes that can increase delays and queue lengths.
- pedestrian crossing and cyclist challenges.



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In response to the feedback received as part of Online Public Information Centre 1, the study team reviewed the feasibility of a roundabout intersection at Bass Pro Mills Drive and Weston Road; however, a roundabout is not recommended at this location because:

- It would require additional spatial and right of way requirements.
- Operational challenges are expected in relation to the unequal traffic volumes at the roundabout approaches, which can increase delays and queue lengths; and
- It would introduce challenges for pedestrians and cyclists when crossing the intersection.

### 1.14 Natural Environment - Background



**Legend**

- Watercourse (Permanent)
- ▭ Subject Property
- ▭ Study Area
- ▭ ELC
- ELC Code
- CVC\_1 - Business Sector
- CVC\_2 - Light Industry
- CVI\_1 - Transportation
- CVR\_3 - Single Family Residential
- MASM1-12 - Common Reed Mineral Shallow Marsh Type
- ME - Meadow
- SA - Shallow Water

#### Natural Environment - Background



**2020 Background Review**

**Terrestrial Environment**

- Lies within TRCA Regulated Area
- Shallow marsh community present
- Potential habitat for bats and bird Species at Risk (SAR)
- Candidate Significant Wildlife Habitat for some species

**Aquatic Environment**

- Black Creek permanent watercourse
- Warm thermal regime
- Assumed to provide direct fish habitat
- No aquatic SAR documented in/near this reach of Black Creek

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As part of this study, a desktop environmental review was completed in 2020 to confirm the existing conditions within the study area. Based on the findings of this review, the following was noted:

- With respect to the terrestrial environment, the study area lies within Toronto Region Conservation Authority Regulated Area. A shallow marsh community is present within the south-central portion of the study area. Potential habitat for some species at risk was noted, as well as potential candidate significant wildlife habitat for some species.
- With respect to the aquatic environment, Black Creek is present within the west portion of the study area, and is assumed to provide direct fish habitat. However, no aquatic Species at Risk were documented in/near this reach of Black Creek.

## 1.15 Natural Environment - Site Surveys



### Natural Environment – Site Surveys

#### Site Observations

- Site visits in spring and summer 2021
- No breeding birds, amphibians and/or species at risk identified

#### Headwater Drainage Features

Three features observed in the study area:

- 1) Black Creek
- 2) Wetland Vegetation Community
- 3) Connecting Channel




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To further investigate the findings of the review undertaken in 2020, the study team conducted field surveys from publicly accessible areas in spring and summer of 2021, the findings of which did not identify any breeding birds, amphibians and/or species at risk within the study area. However, three headwater drainage features were identified within the study areas including:

- The Black Creek tributary;
- A Wetland Vegetation Community; and
- A channel connect the wetland to Black Creek




### 1.16 City of Vaughan Design Criteria



**City of Vaughan Design Criteria**

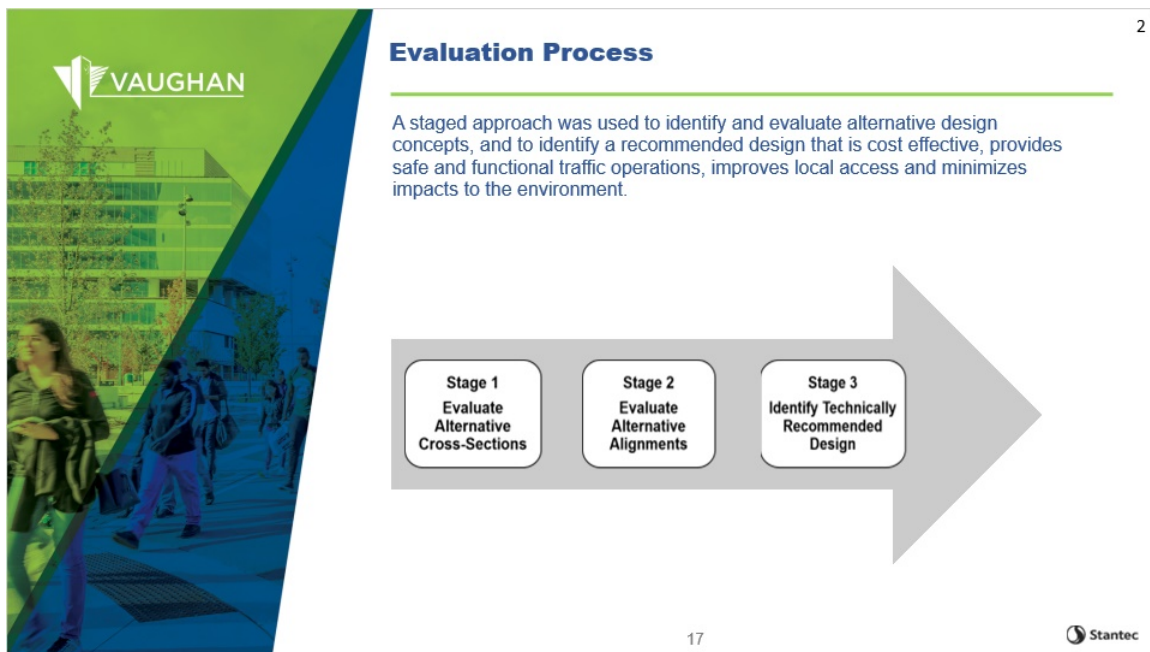
Criteria	City of Vaughan Major Collector Roadway
Design Speed	70 km/hour
Posted Speed	50 km/hour
Through Lane Width	3.3 m
Curb Lane Width	3.5 m
Buffer between Cyclists and Clearways	Minimum 0.5 m
Sidewalk Width	Minimum 1.5 metre and 2.0 metre adjacent to
Cycle Tracks	1.8 metre one-way, 3.0 metre two-way

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Design criteria for future Major Collector Roadways have been established by the City of Vaughan. These include:

- A design speed of 70 km/hr and posted speed limit of 50 km/hr
- 3.3 metre wide through lanes and 3.5 metre wide curb lanes
- A 0.5 metre wide buffer between cyclists and clearways
- A minimum 1.5 metre wide sidewalk, which is increased to a minimum 2.0 metres when the sidewalk is adjacent to the curb
- A minimum 1.8 metre wide one-way cycle track and minimum 3.0 metre wide two-way cycle track

## 1.17 Evaluation Process



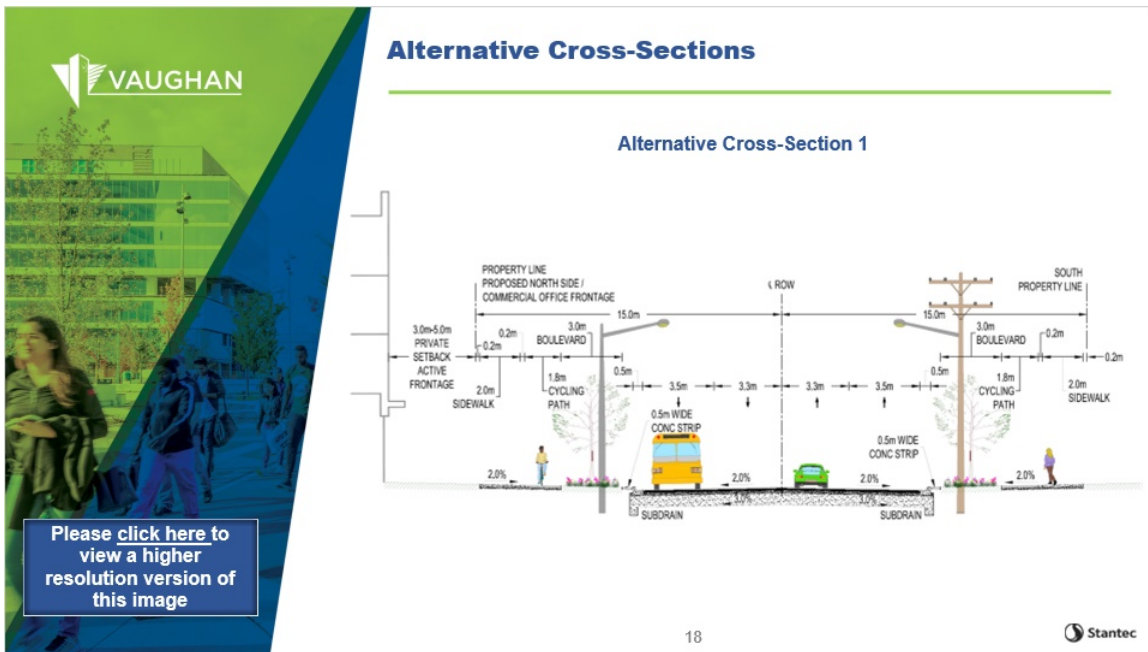
As noted earlier within this presentation, we are in Phase 3 of the Municipal Class Environmental Assessment process, where alternative designs are developed and assessed using a staged approach to identify and evaluate alternative design concepts, and to identify a technically recommended design that is cost effective, provides safe and functional traffic operations, improves local access, and minimizes impacts to the environment. This approach involved the following three stages:

- Stage 1 – Evaluate Alternative Cross-Sections
- Stage 2 – Evaluate Alternative Alignments
- Stage 3 – Identify a Technically Recommended Design

The alternatives are evaluated using a reasoned argument approach, which describes both the advantages of each alternative in response to a defined set of evaluation criteria.

Opportunities to incorporate mitigation measures to offset potential adverse impacts are also considered within this process.

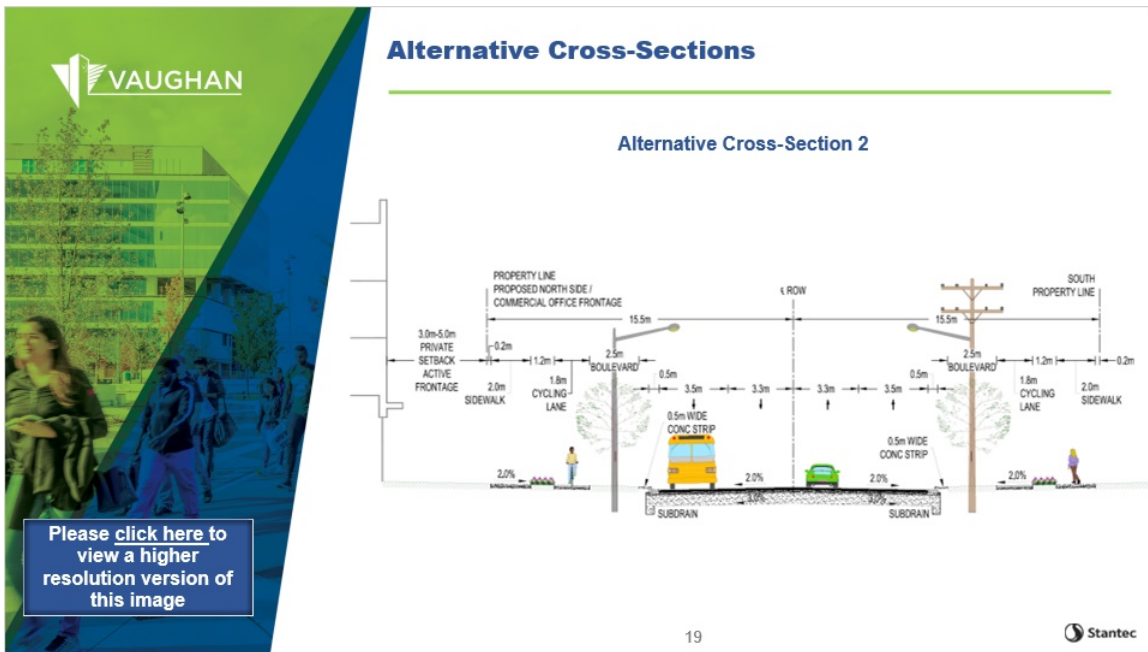
### 1.18 Alternative Cross-Sections



Four Alternative Cross-Sections were developed as part of Stage 1 of the evaluation process.

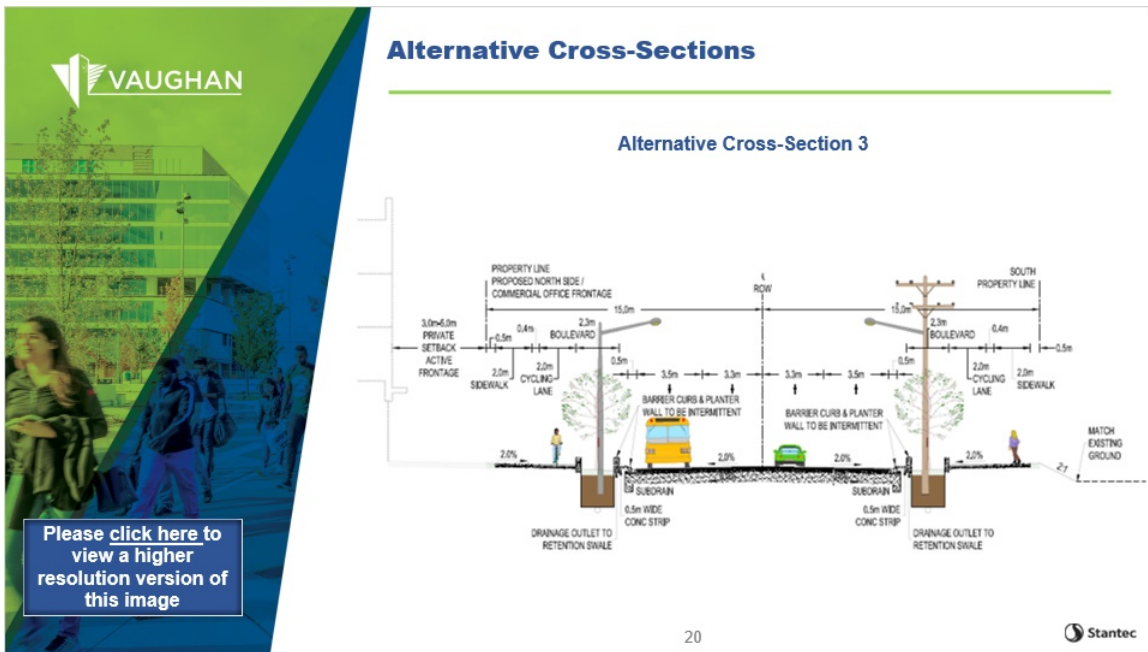
Alternative Cross-Section 1 consists of a 30 metre right-of-way, with a 2.0 metre wide sidewalk and 1.8 metre wide cycling path on either side of the roadway. While there is no formal separation between pedestrians and cyclists, cyclists are separated from vehicular traffic by a 3.0 metre boulevard, which would facilitate streetlighting, utilities, and streetscaping opportunities.

### 1.19 Alternative Cross-Sections



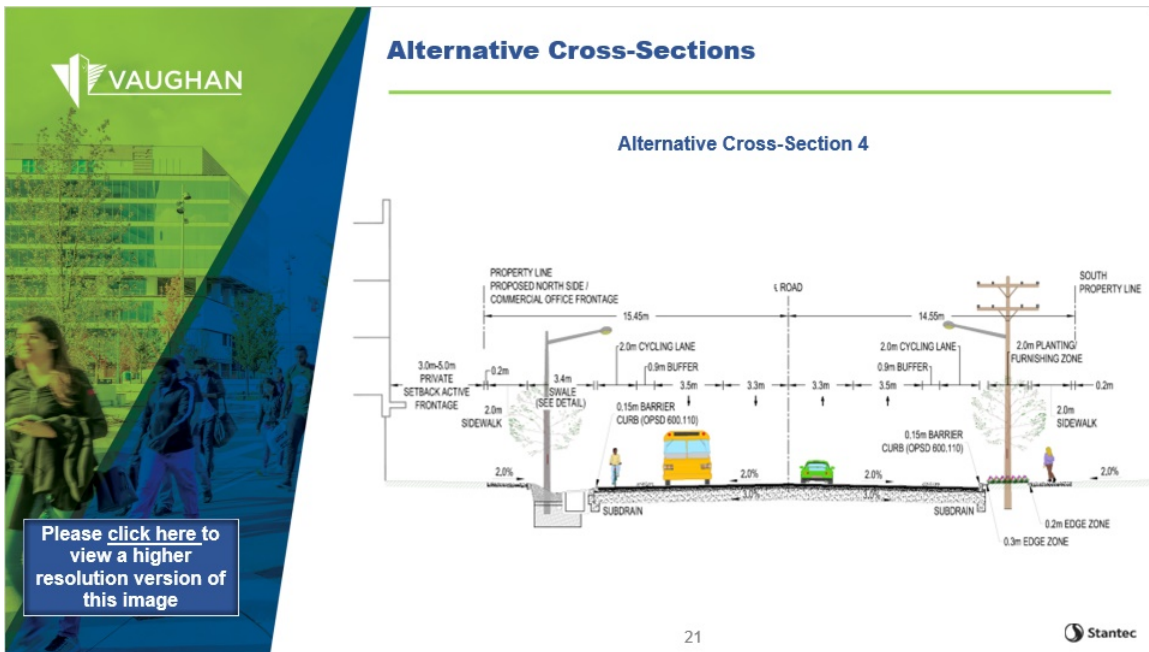
Alternative Cross-Section 2 consists of a 31 metre wide right-of way, with a 2.0 metre wide sidewalk and 1.8 metre wide cycling path on either side of the roadway. A 2.5 metre wide boulevard is provided as part of this option, and offers a 1.2 metre landscape buffer between the sidewalk and cycling path. Cyclists and pedestrians are also separated from vehicles by the 2.5 metre boulevard, which would also facilitate streetlighting, utilities, and streetscaping.

### 1.20 Alternative Cross-Sections



Alternative Cross-Section 3 consists of a 30 metre right-of-way with a 2.0 metre wide sidewalk and 2.0 metre wide cycling path on either side of the roadway. A 0.4 metre wide paved buffer is offered between the sidewalk and cycling path within a 2.3 metre boulevard. Cyclists are separated from vehicles by the 2.3 metre boulevard, which would also facilitate a bio-retention swale, streetlighting, utilities, and streetscaping.



### 1.21 Alternative Cross-Sections



Please [click here](#) to view a higher resolution version of this image

Alternative Cross-Section 4 consists of a 30 metre right-of-way with a 2.0 metre wide sidewalk on either side of the roadway. Cycling facilities are provided on either side of the roadway within 2.0 metre wide cycling lanes that are separated from vehicular traffic by a 0.9 metre wide concrete buffer. A 3.4 metre wide swale would be incorporated within the north side of the right-of-way, and 2.0 metre wide planting/furnishing zones would be provided on the south side.

### 1.22 Evaluation of Alternative Cross-Sections

Please [click here](#) to view the detailed Evaluation of Alternative Cross-Sections

#### Evaluation of Alternative Cross-Sections

Criteria Category	1	2	3	4
Technical	<span style="color: green;">●</span>	<span style="color: orange;">●</span>	<span style="color: yellow;">●</span>	<span style="color: red;">●</span>
Transportation	<span style="color: orange;">●</span>	<span style="color: green;">●</span>	<span style="color: green;">●</span>	<span style="color: red;">●</span>
Socio-Economic	<span style="color: red;">●</span>	<span style="color: yellow;">●</span>	<span style="color: green;">●</span>	<span style="color: orange;">●</span>
Natural Environment	<span style="color: orange;">●</span>	<span style="color: red;">●</span>	<span style="color: green;">●</span>	<span style="color: yellow;">●</span>
<b>Overall Conclusion</b>	Not Recommended	Not Recommended	<b>Technically Recommended Design</b>	Not Recommended



Least Preferred ● → ● → ● → ● Preferred

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The table shown on this slide presents a summary of the evaluation of the four alternative cross-sections. Based on this comparative evaluation, the four alternatives were ranked from least to most preferred in categories considering technical, transportation, socio-economic and natural environment measures. Based on the findings of this evaluation, Alternative Cross-Section 3 was recommended.

A copy of the detailed evaluation of alternative cross-sections can be accessed by clicking the link provided at the bottom of this slide.

### 1.23 Recommended Cross-Section

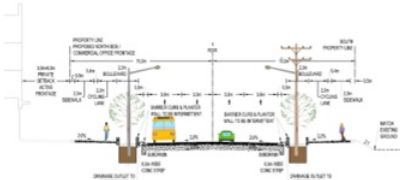



#### Recommended Cross-Section


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Alternative Cross-Section 3 was selected because it:

- is consistent with City design standards and vision for the community and public realm, as well as the recommendations of the VMCSPP.
- has a moderate capital cost and operations and maintenance costs.
- offers safe and comfortable environment for both cyclists and pedestrians.
- provides opportunities to create a pedestrian friendly space through streetlighting, wayfinding, accessible street furniture, etc.
- has high potential to accommodate municipal infrastructure, utilities and streetlighting.
- has high potential to accommodate green infrastructure through the implementation of a retention swale on both sides of the ROW.



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In summary, Cross-Section 3 is recommended because it:

- Is consistent with City of Vaughan design standards and vision for the community and public realm, as well as the recommendations of the Vaughan Mills Centre Secondary Plan.
- Has a moderate capital cost and operations and maintenance cost.
- Offers a safe and comfortable environment for both cyclists and pedestrians.
- Provides opportunities to create a pedestrian friendly space through streetlighting, wayfinding, accessible street furniture, etc.
- Has high potential to accommodate municipal infrastructure, utilities and streetlighting.
- Has high potential to accommodate green infrastructure through the implementation of a retention swale on both sides of the right-of-way.



## 1.24 Alternative Alignments

**Alternative Alignments**

**Alignment Alternative 'A'**

**Alignment Alternative 'B'**

Please [click here](#) to view higher resolution version of the Alternative Alignments

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Once the recommended cross-section design and width were confirmed, the study team proceeded to Stage 2 of the evaluation process; evaluate alternative alignments.

Two alternative alignments were subjected to a detailed evaluation as part of this study.

Alignment Alternative A generally provides a straight and direct connection from the existing terminus of Bass Pro Mills Drive westerly to Weston Road, and Alignment Alternative B traverses offers a connection through the study area from the existing terminus of Bass Pro Mills Drive northwesterly to Weston Road, opposite Astona Boulevard.

A higher resolution copy of the Alternative Alignments can be viewed by clicking the link provided at the bottom of this slide.

### 1.25 Evaluation of Alternative Alignments




**Evaluation of Alternative Alignments**

Criteria Category	Alternative A (Straight Connection to Weston Rd)	Alternative B (Astona Blvd Connection)
<b>Technical</b>	Preferred	Least Preferred
<b>Cultural Heritage</b>	Preferred	Least Preferred
<b>Socio-Economic</b>	Preferred	Least Preferred
<b>Natural Environment</b>	Preferred	Least Preferred
<b>Overall Conclusion</b>	<b>Recommended</b>	Not Recommended

**Alignment Alternative A** is recommended because:

- Avoids direct connection to adjacent residential areas, less traffic infiltration anticipated
- Aligns with City's vision set forth in VMCSPP
- Lower potential environmental impacts (archaeological, natural heritage)
- Lower anticipated capital, operations and maintenance costs
- Less impacts to private property

Please click here to view the detailed Evaluation of Alternative Alignments

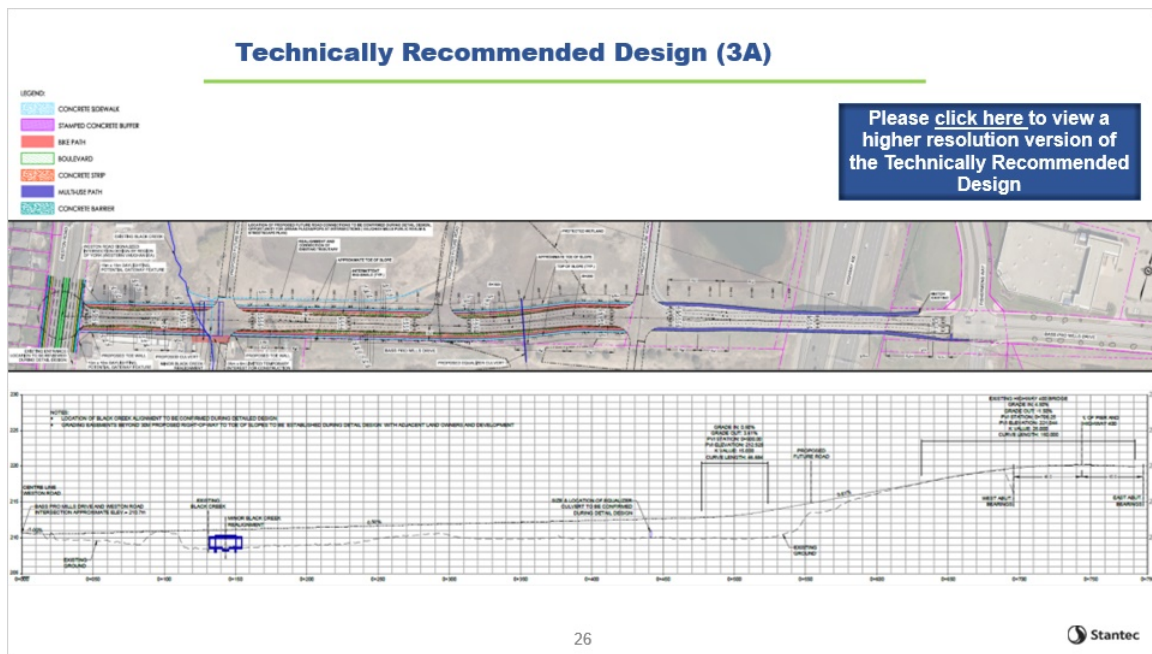
25 

The table shown on this slide presents a summary of the comparative evaluation of the two alternative alignments. Based on the findings of this evaluation, Alignment A is recommended because it:

- Avoids a direct connection to adjacent residential areas;
- Aligns with the City of Vaughan' vision set forth within the Vaughan Mills Centre Secondary Plan;
- Has a lower potential to impact the environment;
- Has a lower anticipated capital, operations and maintenance costs; and
- Impacts less private property.

A copy of the detailed evaluation of alternative alignments can be viewed by clicking the link provided at the bottom of this slide.

### 1.26 Technically Recommended Design (3A)



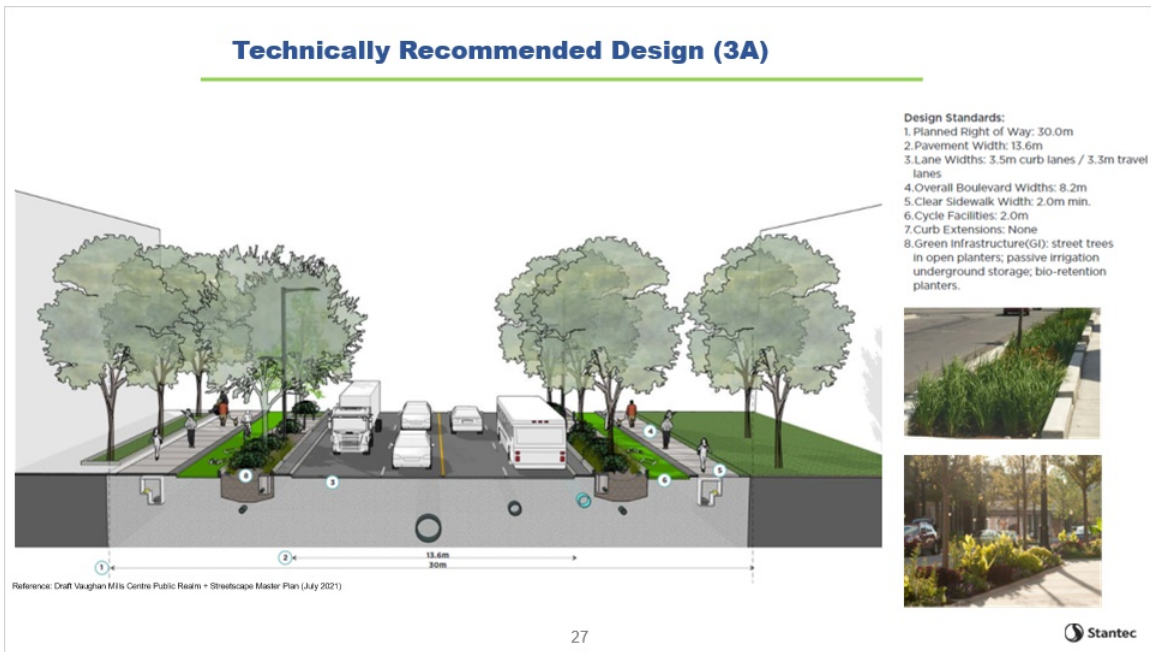
Stage 3 of the evaluation process includes the identification of the Technically Recommended Design. As shown on this slide, the Technically Recommended Design includes Alternative Cross-Section 3 combined with Alternative Alignment A.

The Plan and Profile for Technically Recommended Design 3A includes:

- A 30 metre right-of-way, including 4 lanes of vehicular travel, with 3 lanes in each direction
- A 2.0 metre wide sidewalk and 2.0 metre wide cycling path on either side of the roadway.
- A 0.4 metre wide paved buffer between the sidewalk and cycling path
- A 2.3 metre boulevard with an intermittent bio-retention swale, streetlighting, utilities, and streetscaping
- A potential gateway feature located at the proposed new roadway intersection with Weston Road; and
- A new culvert crossing Black Creek.

A higher resolution version of this Plan and Profile drawing can be accessed by clicking the link provided at the top of this slide.

### 1.27 Technically Recommended Design (3A)

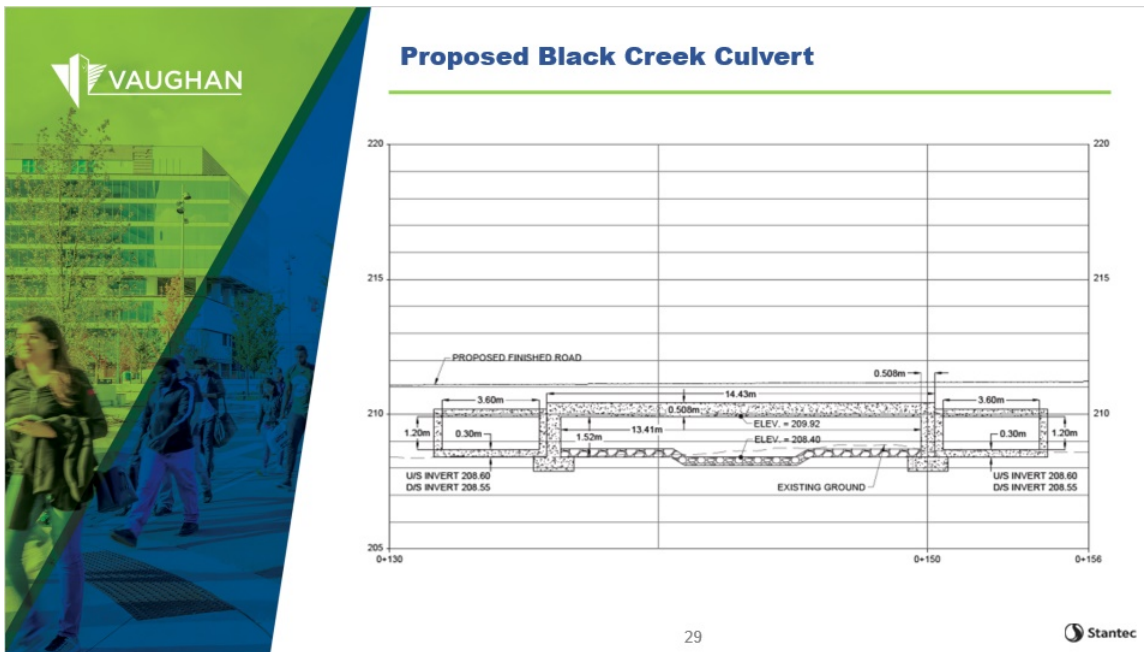


The 3D rendering on this slide was developed as part of the ongoing Vaughan Mills Centre Public Realm and Streetscape Master Plan Study and provides an illustration of what the future extension of Bass Pro Mills Drive cross-section could look like.

Photographs of other intermittent bio-retention swales are also provided on the lower right portion of this slide.

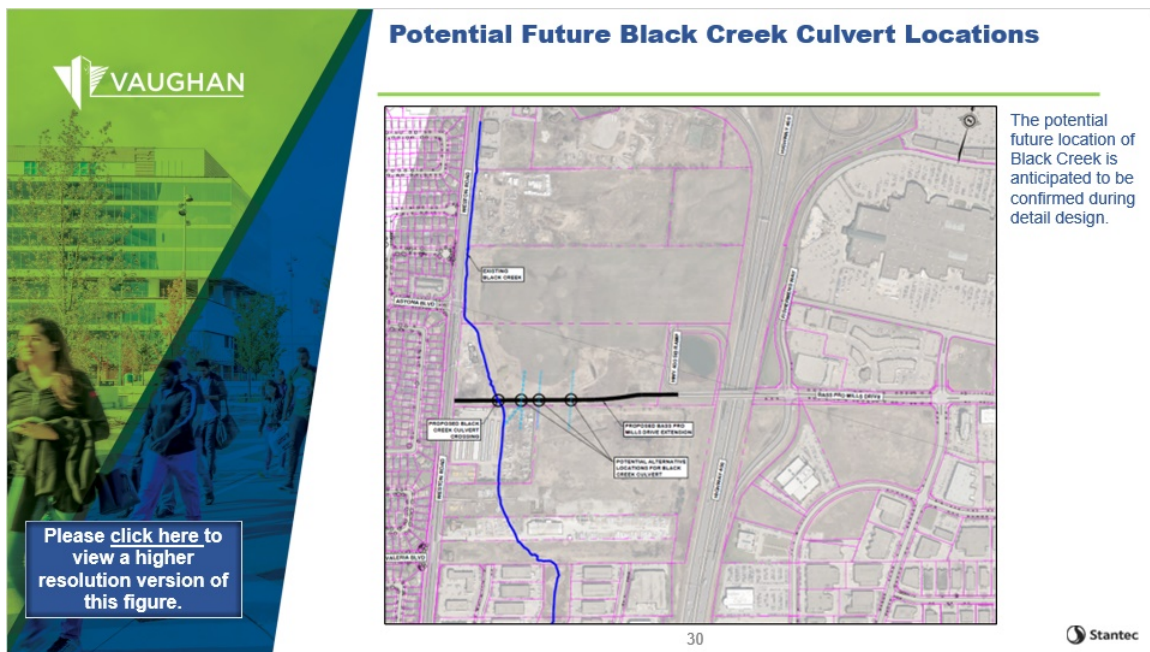


### 1.29 Proposed Black Creek Culvert



A new crossing of Black Creek is required to accommodate the new roadway extension. A new 13.41 metre by 1.52 metre trispan concrete culvert is proposed to span the creek. In addition, two, 3.6 metre by 1.2 metre box culverts are proposed to assist in potentially accommodating any increases in flows or storm events.

### 1.30 Potential Future Black Creek Culvert Locations

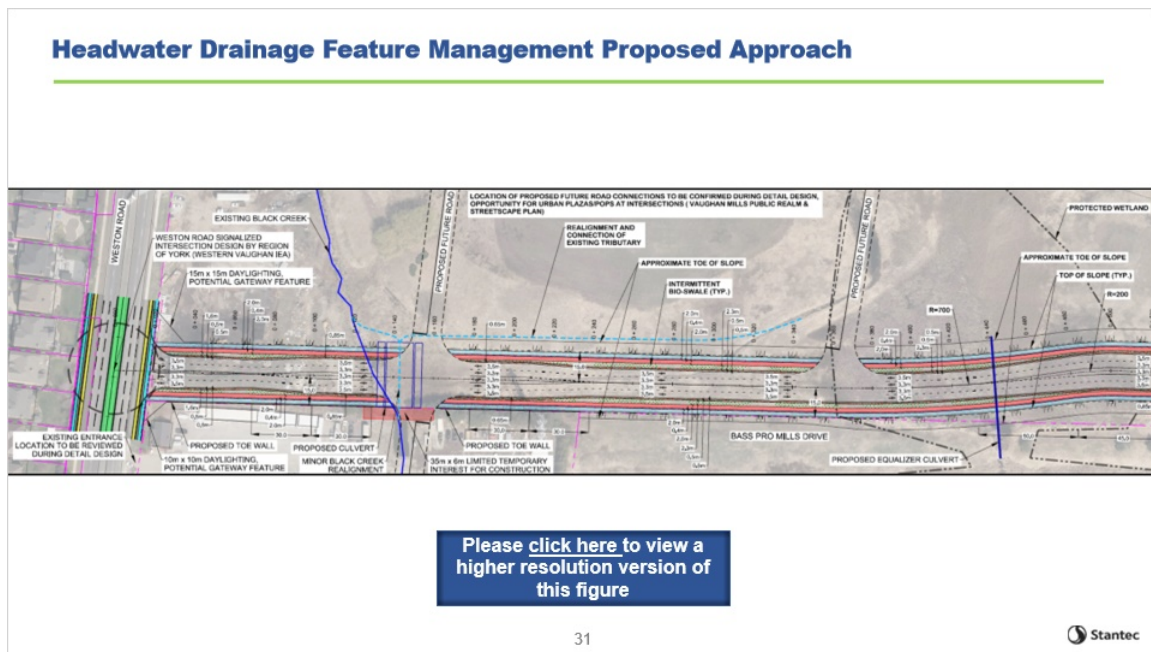


The Vaughan Mills Centre Secondary Plan was completed in 2014; however, the future configuration of local roadways and distribution of land uses within the planning area has not been confirmed. It is understood that the future realignment of Black Creek may also be required as part of future development planning in this area.

At this time, the new crossing of Black Creek is proposed at the existing creek location; however, depending on the future land use and roadway configurations in this area, potential future locations of the new crossing are also shown on this slide. It should be noted that the proposed new culvert designed as part of this undertaking can be applied at any of these potential future locations.

A higher resolution version of this figure can be accessed by clicking the link provided on the left portion of this slide.

### 1.31 Headwater Drainage Feature Management Proposed Approach



As noted on Slide 15 of this presentation, there are three headwater drainage features within the study area, each of which will be temporarily impacted by construction of the new roadway. These include: 1) the reconfiguration of Black Creek; 2) removal of a portion of the wetland vegetation community; and 3) reconfiguration of the existing channel between Black Creek and the wetland.


To help mitigate these impacts, the following is proposed:

- Black Creek will be modified to accommodate the new roadway and crossing structure. A natural channel design with riparian vegetation features will be established and will connect to the existing creek to the immediate north and south of the new roadway.
- An equalization culvert will be constructed beneath the new roadway to maintain flows associated with the existing wetland.
- The existing connection channel will be reconfigured along the north side of the new road wall right-of-way, and will maintain the connection between the wetland and Black Creek.

A higher resolution version of this figure can be accessed by clicking the link provided at the bottom of this slide.




### 1.32 Environmental Impacts & Mitigation



#### Environmental Impacts & Mitigation

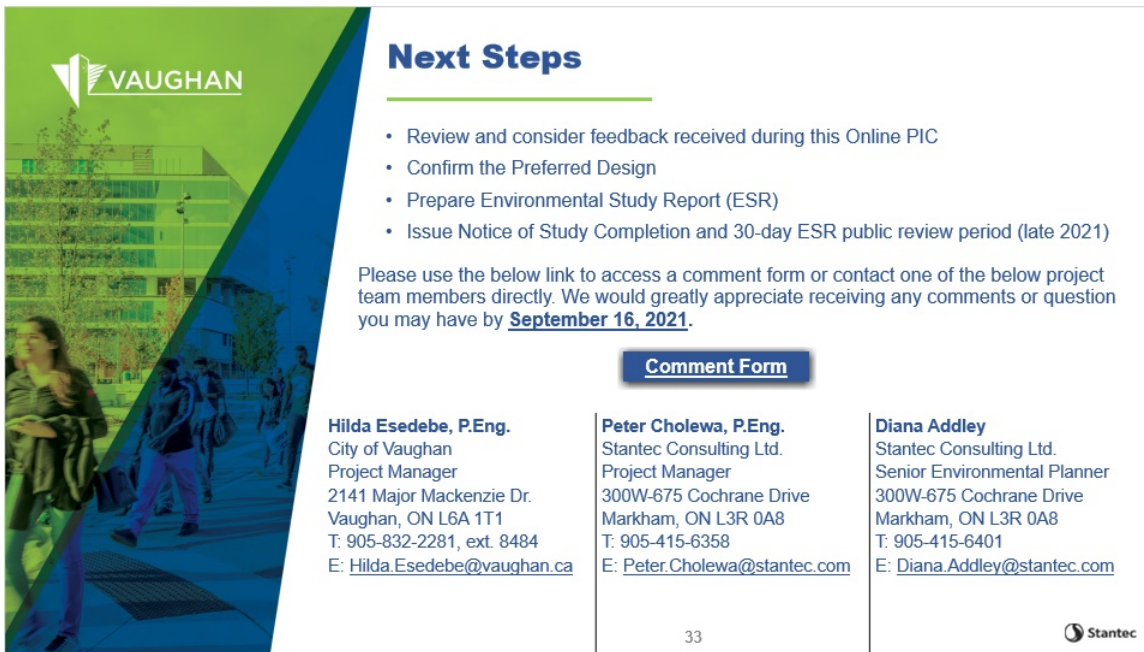
Potential Impact	Preliminary Proposed Mitigation Measures and Commitments
<b>Aquatic Environment</b>	<ul style="list-style-type: none"> <li>• Black Creek assumed to provide direct fish habitat. New crossing of Black Creek will be designed to accommodate meandering channel design with vegetated buffer to north and south.</li> </ul>
<b>Trees/Vegetation</b>	<ul style="list-style-type: none"> <li>• Vegetation and tree removal will be minimized to the extent possible. New streetscape features (grass, trees, vegetation) will be implemented as part of preferred design.</li> </ul>
<b>Wildlife/Habitat/Wetland</b>	<ul style="list-style-type: none"> <li>• Breeding bird surveys to confirm presence/absence of breeding birds and OWES Wetland Evaluation to delineate wetland boundaries and confirm absence of amphibians.</li> <li>• Wetland connection to Black Creek will be maintained via drainage ditch situated along north side of new right-of-way.</li> <li>• Wetland connection to south will be maintained via equalization culvert.</li> </ul>
<b>Archaeology</b>	<ul style="list-style-type: none"> <li>• Stage 1 Archaeological Assessment (AA) identified the potential for the recovery of archaeological resources.</li> <li>• Stage 2 AA will be undertaken during detail design. No construction activities will take place until the Ministry of Sport, Heritage, Tourism and Culture Industries have confirmed in writing that all archaeological licensing and technical review requirements have been satisfied.</li> </ul>
<b>Property</b>	<ul style="list-style-type: none"> <li>• Impacts to some private property has been identified in association with the Technically Recommended Design.</li> <li>• Property impacts will be minimized to the extent possible during detail design, in consultation with affected property owners.</li> </ul>
<b>Noise</b>	<ul style="list-style-type: none"> <li>• A Noise Assessment is being completed in accordance with Provincial guidelines to determine if measures are required to mitigate potential increases in traffic noise.</li> </ul>
<b>Air Quality</b>	<ul style="list-style-type: none"> <li>• An Air Quality Assessment is being completed in accordance with Provincial guidelines to assess the potential changes in local and regional air quality, and to determine mitigation measures as required.</li> </ul>

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The Technically Recommended Design (3A) has the potential to impact the natural and socio-economic environments. A summary of these potential impacts and preliminary proposed mitigation measures and associated commitments is provided on this slide.

More detailed information concerning the potential impacts and proposed mitigation measures associated with this project will be documented within the Environmental Study Report.

### 1.33 Next Steps



**Next Steps**


- Review and consider feedback received during this Online PIC
- Confirm the Preferred Design
- Prepare Environmental Study Report (ESR)
- Issue Notice of Study Completion and 30-day ESR public review period (late 2021)

Please use the below link to access a comment form or contact one of the below project team members directly. We would greatly appreciate receiving any comments or question you may have by **September 16, 2021**.

[Comment Form](#)

<p><b>Hilda Esedebe, P.Eng.</b>                  City of Vaughan                  Project Manager                  2141 Major Mackenzie Dr.                  Vaughan, ON L6A 1T1                  T: 905-832-2281, ext. 8484                  E: <a href="mailto:Hilda.Esedebe@vaughan.ca">Hilda.Esedebe@vaughan.ca</a></p>	<p><b>Peter Cholewa, P.Eng.</b>                  Stantec Consulting Ltd.                  Project Manager                  300W-675 Cochrane Drive                  Markham, ON L3R 0A8                  T: 905-415-6358                  E: <a href="mailto:Peter.Cholewa@stantec.com">Peter.Cholewa@stantec.com</a></p>	<p><b>Diana Addley</b>                  Stantec Consulting Ltd.                  Senior Environmental Planner                  300W-675 Cochrane Drive                  Markham, ON L3R 0A8                  T: 905-415-6401                  E: <a href="mailto:Diana.Addley@stantec.com">Diana.Addley@stantec.com</a></p>
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33



Following this online Public Information Centre, the project team will review and consider the feedback received, and will respond to any questions you may have.

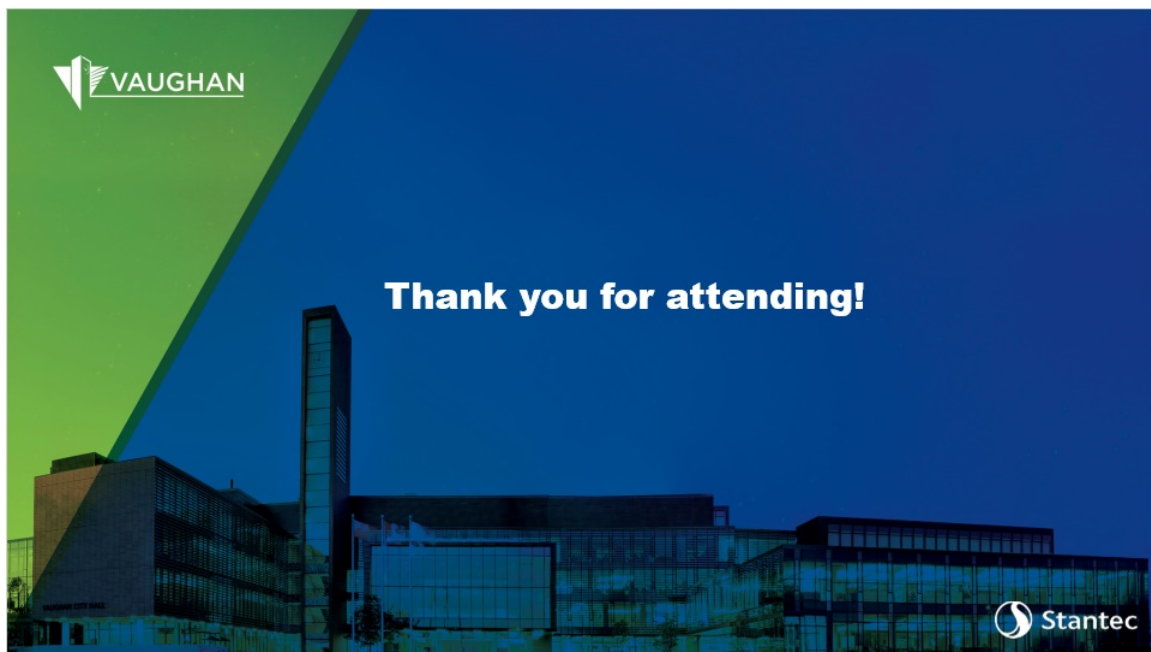
In consideration of the comments received, the project team will confirm the Preferred Design for this project, including the potential impacts and proposed mitigation measures. The Environmental Study Report (or ESR) will be prepared, and a Notice of Study Completion will be issued, at which time the ESR will be available for public review on the project website for a 30-day period.

Your input is important to us, and there are several ways to provide your comments. Please take the time to review all of the study information that is available on the study website and complete the comment form, which can be access by clicking the link provided on this slide.

If you have any comments or questions, please do not hesitate to contact the study team members listed on this slide by email, telephone or mail.

We would appreciate receiving any comments or questions that you may have by Thursday, September 16, 2021.

### **1.34 Thank you for attending!**



On behalf of the City of Vaughan, we would like to thank you for your interest in this study and for taking the time to participate in this online Public Information Centre. We would appreciate receiving any comments or questions that you may have by September 16, 2021.

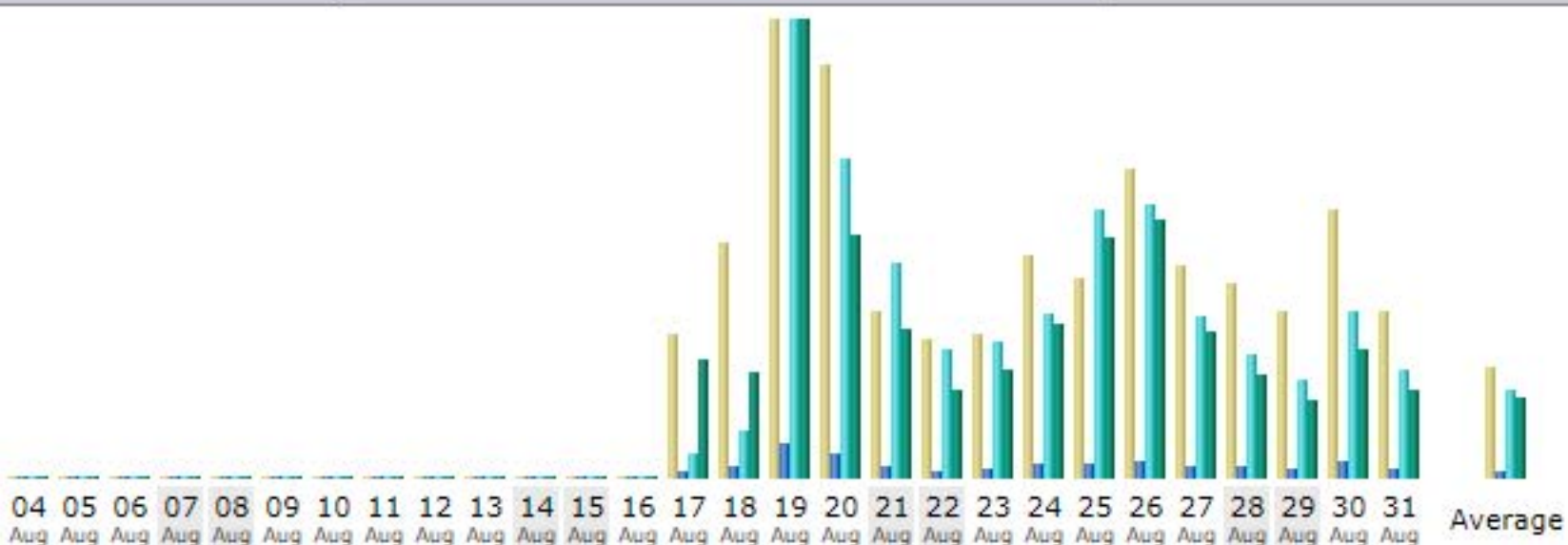
Thank you once again for taking the time to participate in this online Public Information Centre.

# Appendix C

## Online PIC 2 Visitor Statistics (Raw Data)

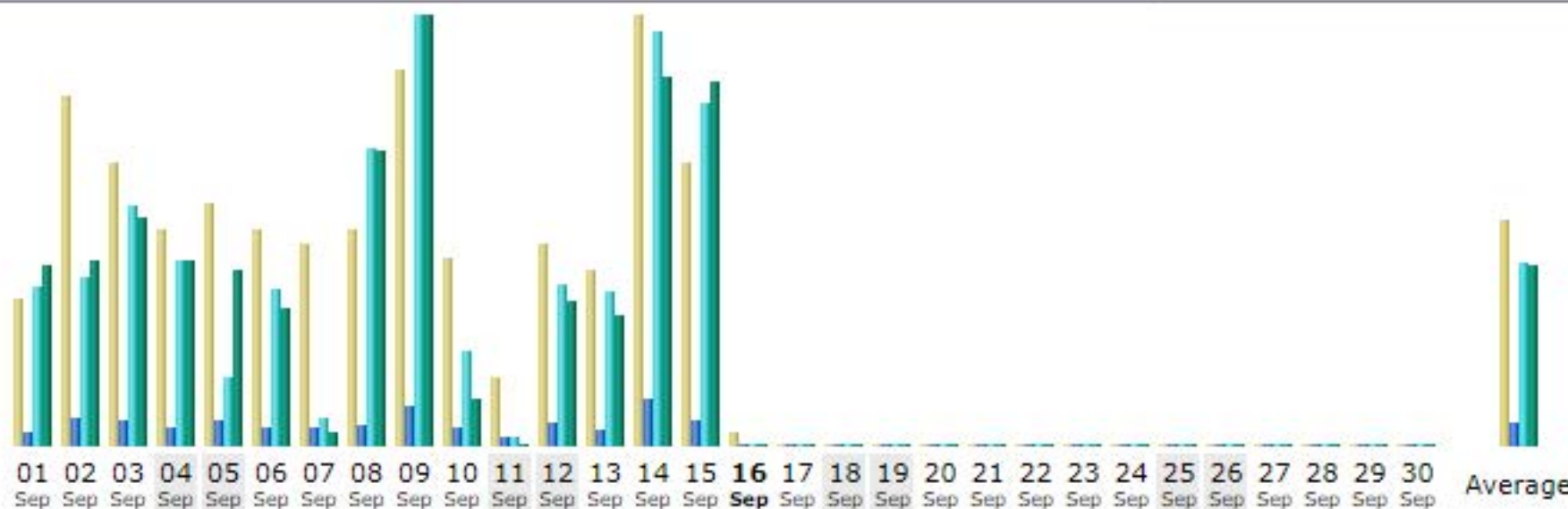


## Days of month



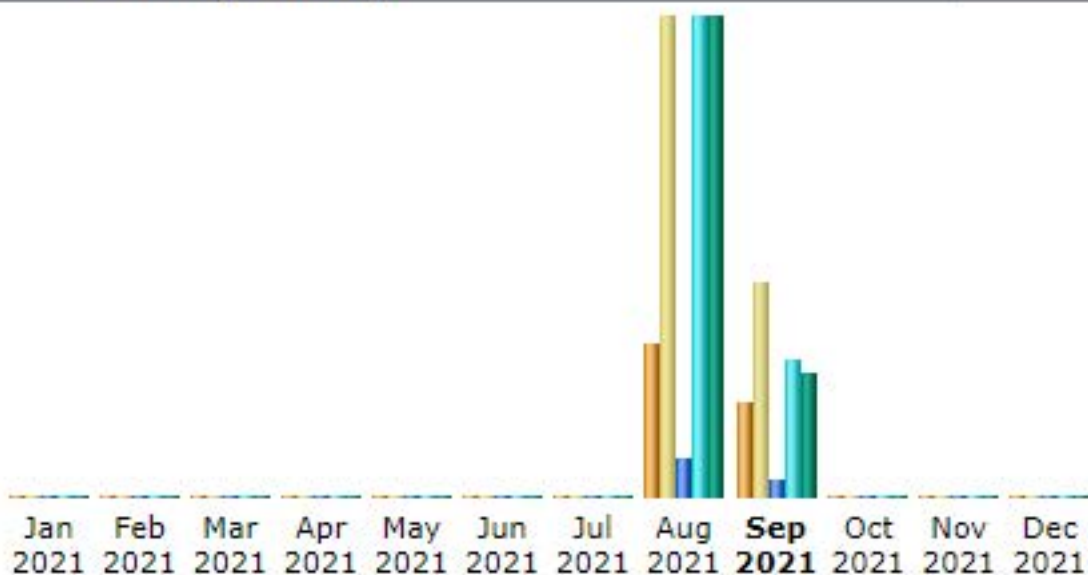
Day	Number of visits	Pages	Hits	Bandwidth
01 Aug 2021	0	0	0	0
02 Aug 2021	0	0	0	0
03 Aug 2021	0	0	0	0
04 Aug 2021	0	0	0	0
05 Aug 2021	0	0	0	0
06 Aug 2021	0	0	0	0
07 Aug 2021	0	0	0	0
08 Aug 2021	0	0	0	0
09 Aug 2021	0	0	0	0
10 Aug 2021	0	0	0	0
11 Aug 2021	0	0	0	0
12 Aug 2021	0	0	0	0
13 Aug 2021	0	0	0	0
14 Aug 2021	0	0	0	0
15 Aug 2021	0	0	0	0
16 Aug 2021	0	0	0	0
17 Aug 2021	25	25	86	181.56 MB
18 Aug 2021	41	41	172	162.32 MB
19 Aug 2021	80	123	1,642	699.36 MB
20 Aug 2021	72	87	1,143	369.44 MB
21 Aug 2021	29	44	771	227.34 MB
22 Aug 2021	24	27	457	135.78 MB
23 Aug 2021	25	34	487	164.84 MB
24 Aug 2021	39	51	587	233.81 MB
25 Aug 2021	35	53	965	367.61 MB
26 Aug 2021	54	63	982	394.87 MB
27 Aug 2021	37	41	575	221.91 MB
28 Aug 2021	34	38	439	158.89 MB
29 Aug 2021	29	36	352	119.16 MB
30 Aug 2021	47	55	596	198.05 MB
31 Aug 2021	29	34	385	135.15 MB
Average	19	24	310	121.62 MB
Total	600	752	9,639	3.68 GB

## Days of month



Day	Number of visits	Pages	Hits	Bandwidth
01 Sep 2021	11	13	151	61.05 MB
02 Sep 2021	26	27	160	62.26 MB
03 Sep 2021	21	25	230	76.73 MB
04 Sep 2021	16	17	177	62.06 MB
05 Sep 2021	18	24	66	59.23 MB
06 Sep 2021	16	18	149	45.98 MB
07 Sep 2021	15	17	26	4.16 MB
08 Sep 2021	16	20	284	99.49 MB
09 Sep 2021	28	38	410	144.89 MB
10 Sep 2021	14	17	91	15.52 MB
11 Sep 2021	5	7	7	44.24 KB
12 Sep 2021	15	22	154	48.90 MB
13 Sep 2021	13	15	148	44.24 MB
14 Sep 2021	32	44	396	124.48 MB
15 Sep 2021	21	23	326	122.61 MB
<b>16 Sep 2021</b>	1	1	1	6.32 KB
17 Sep 2021	0	0	0	0
18 Sep 2021	0	0	0	0
19 Sep 2021	0	0	0	0
20 Sep 2021	0	0	0	0
21 Sep 2021	0	0	0	0
22 Sep 2021	0	0	0	0
23 Sep 2021	0	0	0	0
24 Sep 2021	0	0	0	0
25 Sep 2021	0	0	0	0
26 Sep 2021	0	0	0	0
27 Sep 2021	0	0	0	0
28 Sep 2021	0	0	0	0
29 Sep 2021	0	0	0	0
30 Sep 2021	0	0	0	0
Average	16	20	173	60.73 MB
Total	268	328	2,776	971.64 MB

## Monthly history



Month	Unique visitors	Number of visits	Pages	Hits	Bandwidth
Jan 2021	0	0	0	0	0
Feb 2021	0	0	0	0	0
Mar 2021	0	0	0	0	0
Apr 2021	0	0	0	0	0
May 2021	0	0	0	0	0
Jun 2021	0	0	0	0	0
Jul 2021	0	0	0	0	0
Aug 2021	192	600	752	9,639	3.68 GB
<b>Sep 2021</b>	117	268	328	2,776	971.64 MB
Oct 2021	0	0	0	0	0
Nov 2021	0	0	0	0	0
Dec 2021	0	0	0	0	0
Total	309	868	1,080	12,415	4.63 GB

# Appendix D

## Online PIC 2 Survey Template









# Appendix E

## Online PIC 2 Comments & Survey Responses



# #1

COMPLETE

**Collector:** Web Link 1 (Web Link)  
**Started:** Tuesday, August 24, 2021 4:09:44 PM  
**Last Modified:** Tuesday, August 24, 2021 4:31:28 PM  
**Time Spent:** 00:21:44  
**IP Address:** 165.225.209.31

---

Page 1

## Q1

Do you have any comments regarding the evaluation of alternative cross-sections (Slides 17 – 23) and alternative alignments (Slides 24 – 25)? If yes, please describe.

There is no commentary on how the City intends to protect the Weston Downs Community from vehicular traffic that will choose to use Weston Downs as a westbound thorough fare. Weston Downs has a long discussed congestion issue that the City has been unable to address.

Congestion levels have materially increased since 2014. Why is that the relevant starting point? We need data that represents current development and traffic patterns. Using 2014 is miscasting the current severity and reality of the congestion issue.

---

## Q2

Do you have any comments regarding the Technically Recommended Design (Alternative 3A) (Slide 26)? If yes, please describe.

Again, why deposit cars onto Weston Road immediately south of Astona. What measures will be employed to prevent these vehicles from accessing Weston Downs at Astona.

I don't see any form of solution that addresses the need to get vehicles mode efficiently onto the 400 to help access other major east-west thorough fares. Ultimately, these vehicles need an east-west thorough fare. Why not make Bass Pro Mills a full 400 interchange so that commuters can ultimately access Major Mackenzie, Kirby and other roadways more efficiently.

---

## Q3

Did you like the way the information has been shared through this online public information centre? Is there anything we could improve on? If yes, please describe.

I applaud the website. I would confess that my visibility on this matter has been very limited until receipt the PIC via mail. My initial reaction is one of surprise as I have had no visibility on the matter. I suspect the pandemic and other matters may be part of the distraction but I believe it speaks to a lack of awareness in my community.

---

**Q4**

Do you have any additional comments and/or suggestions you would like to share? If yes, please describe.

A solution to manage Weston Downs traffic needs to be embedded in any solution that risks depositing more vehicles on Weston Road.

---

**Q5**

Please provide your name and email in the fields below (optional).

Name:

[REDACTED]

Mailing Address:

[REDACTED]

Telephone:

[REDACTED]

Email:

[REDACTED]

---

## #2

COMPLETE

**Collector:** Web Link 1 (Web Link)  
**Started:** Thursday, August 26, 2021 11:03:28 AM  
**Last Modified:** Thursday, August 26, 2021 11:18:24 AM  
**Time Spent:** 00:14:55  
**IP Address:** 99.247.173.3

---

Page 1

### Q1

Do you have any comments regarding the evaluation of alternative cross-sections (Slides 17 – 23) and alternative alignments (Slides 24 – 25)? If yes, please describe.

I really like the whole process and various thought out alternatives for 24-25. It was a great job that was done. The recommended alternatives for both the road ie. making bicycle lanes and pedestrian lanes very appealing and the alternative alignment slide 25 conclusion to go straight to Weston as compared to aligning to Astona were well researched and thought out.

---

### Q2

Do you have any comments regarding the Technically Recommended Design (Alternative 3A) (Slide 26)? If yes, please describe.

I agree with the Technically Recommended Design noted on slide 26 (alternative 3A).

---

### Q3

Did you like the way the information has been shared through this online public information centre? Is there anything we could improve on? If yes, please describe.

Yes, the online presentation was very good. I appreciate all of the contact info at the end of the presentation as well. It's very important to keep the lines of communication open.

---

### Q4

Respondent skipped this question

Do you have any additional comments and/or suggestions you would like to share? If yes, please describe.

---

### Q5

Please provide your name and email in the fields below (optional).

Name:

Mailing Address:

Telephone:

Email:

---

**Robinson, Jennifer**

---

**To:** Hilda Esedebe  
**Subject:** RE: [External] Vaughan/Brass Pro Mills EA

-----Original Message-----

From: Hilda Esedebe  
Sent: Wednesday, August 11, 2021 12:35 PM  
To: [REDACTED]  
Subject: RE: [External] Vaughan/Brass Pro Mills EA

Will do. Thank you for your interest in the study.

Regards,

Hilda Esedebe, P.Eng., MBA, M.Sc.  
Transportation Project Manager  
Infrastructure Planning and Corporate Asset Management 905-832-8585, ext. 8484 | hilda.esedebe@vaughan.ca

City of Vaughan | Infrastructure Development  
2141 Major Mackenzie Dr., Vaughan, ON L6A 1T1 vaughan.ca

-----Original Message-----

From: [REDACTED]  
Sent: Wednesday, August 11, 2021 8:53 AM  
To: Hilda Esedebe <Hilda.Esedebe@vaughan.ca>  
Subject: [External] Vaughan/Brass Pro Mills EA

Hi Hilda,  
Please add me to the study mailing list for the Bass Pro Mills Drive extension to Weston Road.

Thank you

[REDACTED]

Sent from my iPhone

This e-mail, including any attachment(s), may be confidential and is intended solely for the attention and information of the named addressee(s). If you are not the intended recipient or have received this message in error, please notify me immediately by return e-mail and permanently delete the original transmission from your computer, including any attachment(s). Any unauthorized distribution, disclosure or copying of this message and attachment(s) by anyone other than the recipient is strictly prohibited.

## Robinson, Jennifer

---

**From:** Hilda Esedebe <Hilda.Esedebe@vaughan.ca>  
**Sent:** Tuesday, August 17, 2021 4:47 PM  
**To:** [REDACTED]  
**Subject:** Bass Pro Mills EA - Call from Resident  
**Attachments:** VoiceMsg[ID=20000000256822 G=40 F=300 A=5601654A-98A5-4961-9A00-E5EE2E282E28 C=1 CID=[REDACTED]].mp3

Hi [REDACTED],

Thank you for reaching out to the City today in response to the Notice of the Bass Pro Mills Drive Extension Environmental Assessment (EA) Study Online Public Information Center (PIC) 2. We appreciate feedback from the public and your comments have been noted.

As promised, please use the links below to access the Vaughan Mills Center Urban Design/Streetscape Plan (draft 2021) and the Vaughan Mills Center Secondary Plan (2014) which give you a sense of plans for the area. If you have further questions on these, I could connect you to Urban Design and Policy Planning staff at the City:

- [https://www.vaughan.ca/projects/policy\\_planning\\_projects/VMC-Public-Realm-and-Streetscape-Plan/Pages/default.aspx](https://www.vaughan.ca/projects/policy_planning_projects/VMC-Public-Realm-and-Streetscape-Plan/Pages/default.aspx)
- [https://www.vaughan.ca/projects/policy\\_planning\\_projects/vgnmills\\_centre\\_secondary/General%20Documents/VMCSP%20Sept19-2014.pdf](https://www.vaughan.ca/projects/policy_planning_projects/vgnmills_centre_secondary/General%20Documents/VMCSP%20Sept19-2014.pdf)

The Project Team for the Bass Pro Mills Drive Environmental Assessment (EA) Study is certainly aware of your concerns regarding traffic infiltration into the Weston Downs area and we have met with representatives of the Weston Downs Rate Payers Association (WDRA) a number of times. We have also received feedback during our first online Public Information Center (PIC), which ran from December 3, 2020 to January 8, 2021, and the Project Team continues to review feedback from all stakeholders, along with the outcomes of technical analysis, to inform this study to the best extent possible.

The City of Vaughan is growing and infrastructure for vehicular traffic, transit and active transportation needs to be in place to support this growth. The City and Region have a number of ongoing studies aimed at growth and improving Vaughan's connectivity including this [Bass Pro Mills EA](#), [Kirby Road missing link](#), [Teston Road missing link](#), [Langstaff Road EA](#) to mention a few. York Region also has plans to widen Rutherford Road and Weston Road by 2041 (see the [Transportation Master Plan | York Region](#)). Although Weston Road and Rutherford Road fall under York Region's jurisdiction, the City works with all tiers of government to preserve the interest of citizens as best as possible, while reviewing multiple solutions to address growth and transportation needs. Regarding Regional projects you may email [roads@york.ca](mailto:roads@york.ca) or visit the study websites for more information.

As you know, the next Online PIC for the Bass Pro Mills EA Study that has been arranged to present and solicit feedback on the outcomes of PIC 1, the traffic analysis and environmental investigations, the evaluation of alternative alignments and cross-sections, the Technically Recommended Design and the next steps in the study process. As part of this online PIC, a recorded presentation and comment form will be available for your review and feedback on the project website ([Vaughan.ca/BassProMillsEA](http://Vaughan.ca/BassProMillsEA)) from **August 19, 2021 to September 16, 2021**. You are encouraged to review the information when it becomes available.

If you have further questions, please let me know.

Regards,



**Hilda Esedebe**, P.Eng., MBA, M.Sc.  
**Transportation Project Manager**  
**Infrastructure Planning and Corporate Asset Management**  
905-832-8585, ext. 8484 | [hilda.esedebe@vaughan.ca](mailto:hilda.esedebe@vaughan.ca)

**City of Vaughan | Infrastructure Development**  
2141 Major Mackenzie Dr., Vaughan, ON L6A 1T1  
[vaughan.ca](http://vaughan.ca)



## Robinson, Jennifer

---

**From:** Hilda Esedebe <Hilda.Esedebe@vaughan.ca>  
**Sent:** Monday, September 20, 2021 4:52 PM  
**To:** [REDACTED]  
**Cc:** Addley, Diana; Cholewa, Peter; Robinson, Jennifer; [REDACTED]  
**Subject:** RE: [External] Bass Pro Mills Drive Municipal Class Environmental Assessment (EA) Study - Phase 3 recommendations and comments

Hello [REDACTED],

Thank you for your email and support for the recommended plan for the Bass Pro Mills Drive extension (from Highway 400 to Weston Road). Your comments have been noted.

As previously advised, the proposed connection to Astona Blvd and the internal road network that you are referring to are outside the scope of the Bass Pro Mills Drive Municipal Class Environmental Assessment (EA) Study and falls under the Vaughan Mills Center Secondary Plan (VMCSP) and the City's Policy Planning department, whom I believe you have been in contact with.

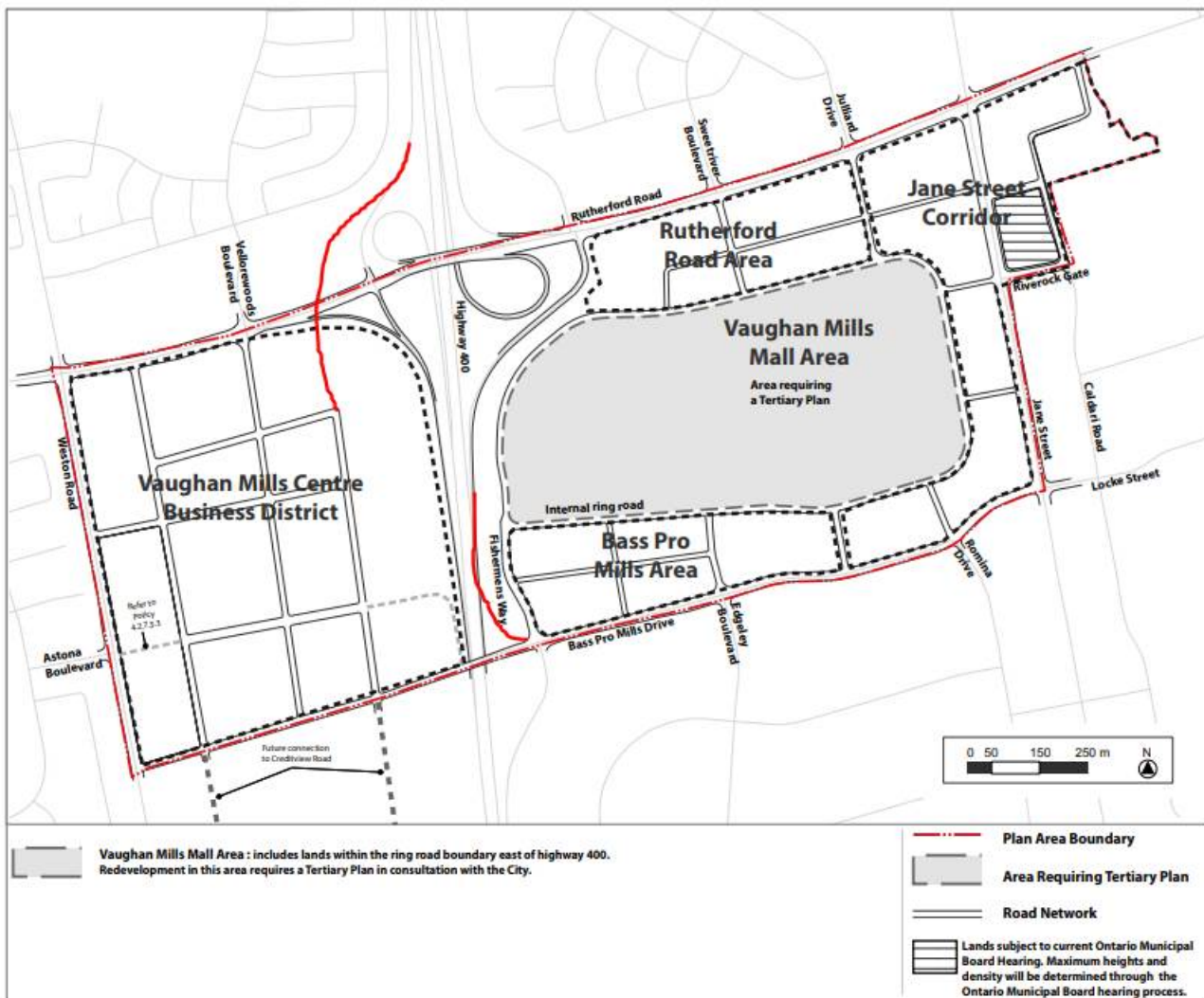
To reiterate, York Region has plans to widen Rutherford Road and Weston Road by 2041 (see the [Transportation Master Plan | York Region](#)). York Region has already begun the Detailed Design project to widen Weston Road from Highway 407 to Fieldstone Drive/Chrislea Road (Phase 1) and between the future Bass Pro Mills Extension and Hawkview Boulevard (Phase 2), with construction anticipated within the next few years. The Region is also about to conclude the [Langstaff Road Widening Environmental Assessment Study](#) which includes widening Langstaff Road from four to six lanes, the missing link, and an initial review of a full moves interchange at Langstaff Road and Highway 400. Although these projects fall under York Region's jurisdiction, the City works with all tiers of government to preserve the interest of citizens as best as possible, while reviewing multiple solutions to address growth and transportation needs. For more information regarding Regional projects, you may email [roads@york.ca](mailto:roads@york.ca) or visit the project websites. I believe York Region staff for the current Weston Road Widening project have already met with the Weston Downs Rate Payers Association (WDRA). Please note that the intersection at Bass Pro Mills Drive and Weston Road is proposed to be three-legged.



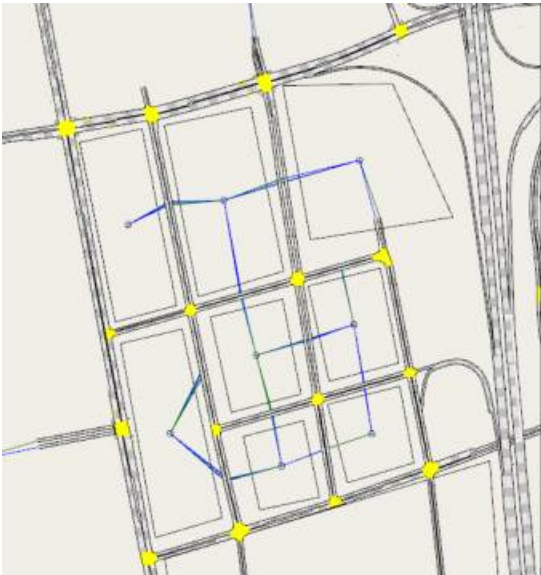
Direct Highway 400 connections to Wonderland and Vaughan Mills Center are outside the scope of the Bass Pro Mills Drive EA Study. Ramp connections to Highway 400 fall mainly under the Ministry of Transportation Ontario's (MTO's)

jurisdiction and would require compliance with MTO’s design standards including spacing, proximity to other interchanges (Major Mackenzie Drive, Rutherford Road and Langstaff Road), adequate lengths to accommodate speed changes and change in grades, property allowances, crossing bridge structures etc. This highway corridor currently accommodates many movements and creating adequate spacing for more movements while meeting MTO’s standards may be a challenge. For example, with my experience in Highway Design, and from looking into similar comments from the public for the Bass Pro Mills EA Study, the connections you have suggested under item 7 (both illustrated in red below) are likely not feasible; 1-because there isn’t much room to accommodate the elevation changes from Highway 400 southbound, then go above grade over Rutherford Road to come back down to connect to Bass Pro Mills Drive via the NW VMCSP lands. With the proposed VMCSP road network, vehicles from Highway 400 southbound can exit at Rutherford Road and turn left on Valeria Woods Blvd to get to Bass Pro Mills Drive. The proposed VMCSP road network will offer alternatives for traffic to get from Rutherford Road to the VMCSP area and Highway 400 without infiltrating adjacent neighbourhoods. 2- There isn’t much room to accommodate a direct Highway 400 northbound on ramp from the south-east side of Bass Pro Mills Drive while satisfying standards as noted above.

For more information, you may email MTO Corridor Management at [corridor-cr@ontario.ca](mailto:corridor-cr@ontario.ca) or visit the MTO website.



The project team is currently looking into a request by the WDRA for the Traffic Analysis information used for the Bass Pro Mills EA study. The road network shown in the approved [VMCSP 2014 document](#) was used for the traffic analysis. This excludes an Astona Blvd. east link.



Please be advised that Traffic Analysis Reports are typically included as part of the Environmental Study Report (ESR) for public review prior to the completion of the study. You are part of the study contact list so you will receive a notification at that time.

Regards.

**Hilda Esedebe**, P.Eng., MBA, M.Sc.  
**Transportation Project Manager**  
**Infrastructure Planning and Corporate Asset Management**  
905-832-8585, ext. 8484 | [hilda.esedebe@vaughan.ca](mailto:hilda.esedebe@vaughan.ca)

**City of Vaughan | Infrastructure Development**  
2141 Major Mackenzie Dr., Vaughan, ON L6A 1T1  
[vaughan.ca](http://vaughan.ca)



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**From:** [REDACTED] >  
**Sent:** Friday, September 17, 2021 12:30 AM  
**To:** Hilda Esedebe <[Hilda.Esedebe@vaughan.ca](mailto:Hilda.Esedebe@vaughan.ca)>; [Diana.Addley@stantec.com](mailto:Diana.Addley@stantec.com)  
**Cc:** [REDACTED]

**Subject:** [External] Bass Pro Mills Drive Municipal Class Environmental Assessment (EA) Study - Phase 3 recommendations and comments

## Bass Pro Mills Drive MCEA Study

**My recommendations and comments** for the "Bass Pro Mills Drive Municipal Class Environmental Assessment (EA) Study - Phase 3 are as follows:

1. **I do support** the extension of Bass Pro Mills directly west to Weston Road to better access the VMC complex.
2. **I do not support** the extension of Astona Blvd directly east into the VMC north west section. This road extension is not justified and will only create traffic infiltration problems into the Weston Downs community.
3. **I do not support** the multi modal connections from Bass Pro Mills Drive and adjacent proposed roads into the existing southbound ramp to the 400 Highway. This will not improve traffic on Weston Road or on Rutherford Road. See my recommendations below.
4. **I do not support** redistributing traffic from Rutherford Rd into the existing well established Weston Downs neighborhood. Installing a direct connection into the existing Bass Pro Mills 400 south bound onramp will create cut through traffic infiltration into the Weston Downs neighborhood. Please learn and apply the lessons at Pharmacy Ave. and Sheppard Ave. East I have already sent to you.
5. The existing Bass Pro Drive connection to the south bound 400 highway on ramp was installed to allow shoppers and visitors to enter the 400 Southbound highway directly from Vaughan Mills without using Rutherford Road or Weston Rd. This on ramp should be maintained for this sole purpose.
6. Any road connection proposal, directly opposite from Astona Blvd. and any new connections to the 400 South bound on ramp at Bass Pro Mills Drive is of critical importance to the residents of Weston Downs because if they go ahead and either is constructed, it will significantly increase the cut through traffic into the neighborhood of Weston Downs. This cut through traffic will significantly reduce the internal road safety for residents and for their children walking, biking and driving to and from their homes in Weston Downs. These road connection would create a major safety hazard to the residents of Weston Downs.

**A priority needs to put on immediately by Vaughan City and York Region on improving the following existing road infrastructure to properly relieve traffic around the Rutherford Road and Weston Road area without diverting it into the Weston Downs neighborhood.**

1. Increase Langstaff Road from Islington Ave. to Jane St from 4 lanes to 6 full lanes. Install full North bound on ramps to 400 highway north from for both east and west bound traffic on Langstaff Rd.
2. Install direct northbound 400 highway onramp directly from Wonderland at the southwest corner of Wonderland. Do not utilize Rutherford Rd for Wonderland visitors wanting to access 400 north bound when leaving.
3. Install direct southbound 400 highway onramp directly from Wonderland at northwest corner of Wonderland property. Do not utilize Major Mackenzie Rd for Wonderland visitors wanting to access 400 south bound when leaving.
4. Widen Rutherford Rd from 4 lanes to full 6 lanes from Islington to Jane Street.
5. Expand Weston Road and install 2 left turn lanes on Weston Rd southbound at Rutherford Rd. This will allow for rapid access to existing 400 South and North bound onramps on Rutherford

Rd. There is absolutely no need to route this traffic to a new onramp connection at Bass pro Mills to the 400 south.

6. Install 2 right turn lanes on Weston Rd northbound at Rutherford Rd. This will allow for rapid access to existing 400 South and North bound onramps on Rutherford Rd. Again there is absolutely no need to route this traffic to a new onramp connection at Bass pro Mills to the 400 south.
7. North bound 400 offramps to Vaughan Mills are well designed as is the exit from Vaughan Mills to south bound 400 via Bass Pro Mills Road. However there is no adequate direct access to 400 highway north bound for exiting from Vaughan Mills and there is no direct access to Vaughan Mills from the southbound 400. The following road infrastructure needs to be improved;
  1. The south bound off ramp onto Rutherford Rd. needs to be extended southbound into the NW VMC lands and connected to Bass Pro Mills for direct access to Vaughan Mills Centre.
  2. A direct 400 north bound on ramp from Vaughan Mills Center needs to be installed from the south east side of Bass Pro Mills for Vaughan Mills Center existing visitors wanting to access the north bound 400 highway directly

**Questions I have:**

1. Have the options I outlined above been evaluated? Why are these options not the preferred options? Why have these options not already been installed to relieve traffic in the Weston Road and Rutherford Road areas?
2. Why is widening of Weston Road, north of Bass Pro Mills Drive to Hawkview Boulevard being planned for? Is it to allow 2 lanes of left turn traffic from North of Weston road to go to the new Bass Pro Mills Road at Weston Road?
3. What is the basis for the study carried out for Vehicular trips associated with the 2014 Vaughan Mills Centre Secondary Plan (VMSCP) road network, this Bass Pro Mills Drive Municipal Class Environmental Assessment and Option B (Future Conditions with Bass Pro Mills Drive extension and VMCP (2031 and 2041 horizons)? Can I get a copy of this full report? Does this study include an Astona Blvd east link into VMC and a Bass Pro Mills Road with a multi-modal road connections to the south bound on ramp to the 400 highway?

If you have any questions regarding my input above, please contact me by email or by phone [REDACTED]

Thank you and Regards, [REDACTED]