



**Online Public Information Centre  
#2 Summary Report**

Bass Pro Mills Drive MCEA, from  
Highway 400 to Weston Road

160540006

October 12, 2021

Prepared for:

City of Vaughan

Prepared by:

Stantec Consulting Ltd.



## ONLINE PUBLIC INFORMATION CENTRE #2 SUMMARY REPORT

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Prepared by \_\_\_\_\_  
*J. Robinson*  
(signature)

**Jenn Robinson**

Approved by \_\_\_\_\_  
*D. Addley*  
(signature)

**Diana Addley**



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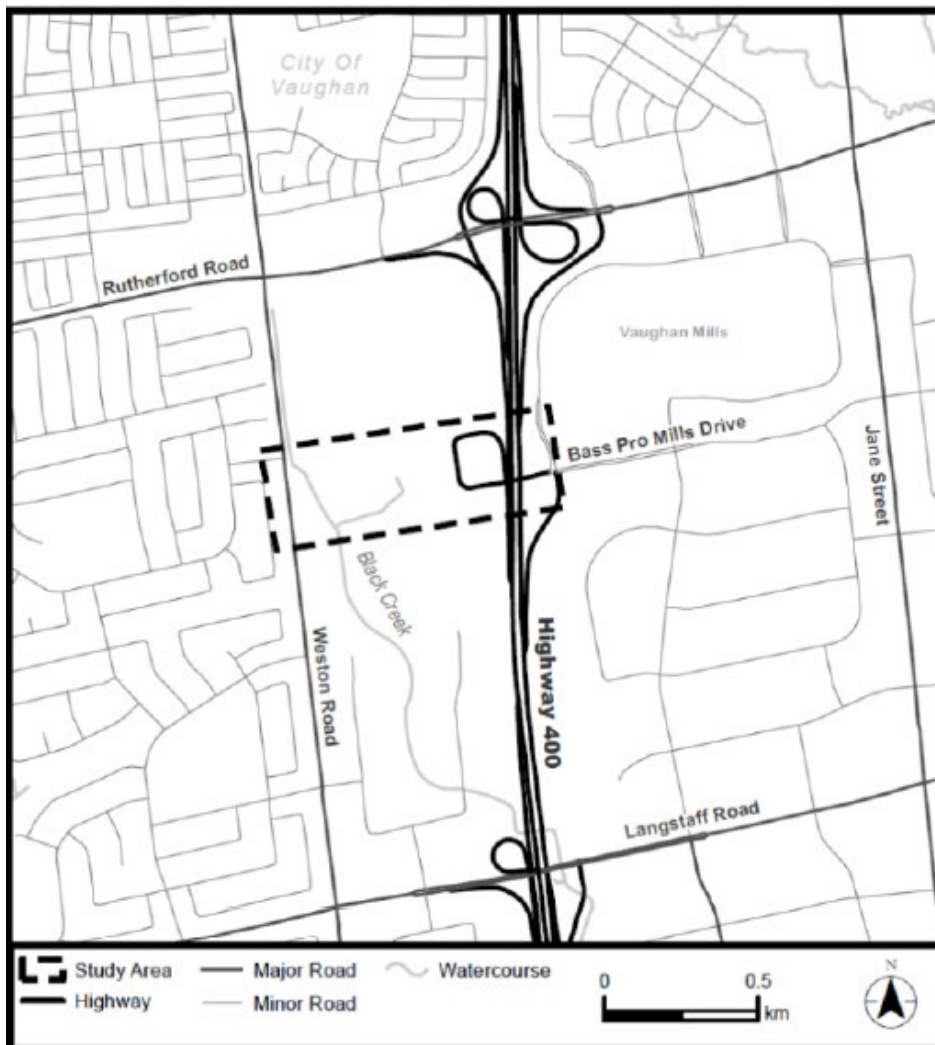


## 1.0 INTRODUCTION

Online Public Information Centre (PIC) 2 was held from August 19, 2021 to September 16, 2021 to present and solicit feedback on the study background, outcomes of Online PIC 1 (held from December 3, 2020 to January 8, 2021), the preliminary findings of the traffic analysis and environmental investigations, evaluation of alternative alignments and cross-sections, Technically Recommended Design and next steps in the study process. Due to COVID-19 and associated physical distancing requirements, PIC 2 was hosted online via the study website ([Vaughan.ca/BassProMillsEA](http://Vaughan.ca/BassProMillsEA)).

The study area for this is generally situated between Highway 400 and Weston Road, within the City of Vaughan, as shown on the Key Plan in **Figure 1** below:

**Figure 1: Key Plan**



## 2.0 PURPOSE

Online PIC 2 forms part of the overall consultation plan for the Bass Pro Mills Drive Municipal Class Environmental Assessment (Class EA) and was designed to inform and engage City residents and other stakeholders on study background, outcomes from the first PIC, the preliminary findings of the traffic analysis and environmental investigations, evaluation of alternative alignments and cross-sections, Technically Recommended Design and next steps in the study process.

## 3.0 LOCATION, DATE AND TIME

The Online PIC was hosted on the study website ([Vaughan.ca/BassProMillsEA](http://Vaughan.ca/BassProMillsEA)) from August 19, 2021 through September 16, 2021. A recorded presentation was available for online participants to listen to and/or read study information. Opportunities to offer feedback and ask questions were provided through an online survey provided at the end of the presentation, or by contacting the study team directly via telephone or email.

## 4.0 NOTIFICATION

The Notice of Online PIC 2 provided information about the PIC, including the date that the materials and online survey would become available for review online, where to access the materials and online survey, and a requested end date to provide feedback to the study team.

The Notice of Online PIC 2 was distributed to the public, agencies, utilities, stakeholders and First Nation communities through the methods outlined below in **Table 1**.

**Table 1: Notification Distribution**

Method of Distribution	Date of Distribution
Posted in the Thornhill Liberal local newspaper	August 12, 2021 September 2, 2021
Posted in the Vaughan Citizen local newspaper	August 12, 2021 September 2, 2021
Hardcopies mailed via Canada Post Ad Mail to 3,393 residents within a 200m radius of the study area.	August 18, 2021
Hardcopies mailed directly via Canada Post to residents backing directly onto Weston Road (from Astona Boulevard to 500m south of Astona Boulevard), external agencies, as well as members of the public who expressed an interest in the study, where addresses were available.	August 12, 2021



## ONLINE PUBLIC INFORMATION CENTRE #2 SUMMARY REPORT

Hardcopies mailed directly via Canada Post to First Nation communities	August 12, 2021
Emailed electronic copy to the contact list	August 12, 2021
Emailed electronic copy of notice, along with a Public Service Announcement, to the Technical Advisory Committee (TAC) and Stakeholder Group	August 12, 2021

A copy of the Notice of Online PIC 2 is available for review within **Appendix A**.

In addition to the above, the City of Vaughan published several social media ‘pushes’ to their Facebook, Twitter, Instagram and LinkedIn accounts on the dates below, encouraging the public to have their say on the proposed extension. While the August 19, 2021 social media push provided a link to the City’s Public Service Announcement (PSA), all other pushes provided direct links to the study website.

- August 19, 2021
- August 26, 2021
- September 2, 2021
- September 9, 2021
- September 16, 2021

The City’s PSA was published on August 19, 2021 through the City of Vaughan news website (<https://www.vaughan.ca/news>) and social media pushes on this day, encouraging the public to “Help shape the extension of Bass Pro Mills Drive” and to “Provide feedback and help inform the City’s proposed extension plan” by participating in Online PIC 2 through the provided links to the study website. The announcement also provided background information on the study, such as the recommendations made by the Vaughan Mills Centre Secondary Plan (VMCSP, 2014) and the City’s Transportation and Mobility Objectives that were outlined in the 2018 - 2022 Term of Council Service Excellence Strategic Plan. Additionally, banners highlighting the dates and website location of Online PIC 2 were also published using the City’s electronic banner system within and outside of City Hall throughout the duration of the comment period.

A copy of the wording and dates of the social media pushes, PSA and banners are available for review within **Appendix A**.

## 5.0 REFERENCE MATERIALS AND HANDOUTS

The following City of Vaughan Bass Pro Mills Extension Class EA displays were presented as part of Online PIC 2:

- Title Page
- PIC 2 Objectives
- Study Overview



## ONLINE PUBLIC INFORMATION CENTRE #2 SUMMARY REPORT

- Municipal Class Environmental Process
- Online Public Information Centre 1
- What Have We Heard?
- Simulation of Future Transportation Options
- Transportation Network Improvements
- Vaughan Mills Centre Secondary Plan
- Transportation Network Assessment (1)
- Transportation Network Assessment (2)
- Transportation Network Assessment (3)
- Roundabout Screening
- Natural Environment – Background
- Natural Environment – Site Surveys
- City of Vaughan Design Criteria
- Evaluation Process
- Alternative Cross-Sections (1)
- Alternative Cross-Sections (2)
- Alternative Cross-Sections (3)
- Alternative Cross-Sections (4)
- Evaluation of Alternative Cross-Sections
- Recommended Cross-Section
- Alternative Alignments
- Evaluation of Alternative Alignments
- Technically Recommended Design (3A) (1)
- Technically Recommended Design (3A) (2)
- Recommended Design – Highway 400 Bridge
- Proposed Black Creek Culvert
- Potential Future Black Creek Culvert Locations
- Headwater Drainage Feature Management Proposed Approach
- Environmental Impacts & Mitigation Measures
- Next Steps
- Thank you for attending!

A copy of the Online PIC 2 displays and accompanying script are included in **Appendix B**.



## 6.0 FORMAT

In light of COVID-19 and associated physical distancing requirements, the PIC was held online via the study website and included a pre-recorded presentation and an online survey through which to provide feedback. A summary of the online survey questions, and associated feedback received is provided in **Section 8.0**.

The Articulate Storyline presentation platform was used to encourage interaction throughout the presentation, allowing users to pause the presentation, or fast forward to sections of the presentation that interested them most and/or rewind to review information in more detail. The text of each narrated presentation slide was offered as part of the online PIC to provide both a visual and audio experience, and to help to ensure that accessibility needs were accommodated.

All attendees were encouraged to provide their feedback by September 16, 2021. As part of the Online PIC review and comment period, members of the study team were available to answer questions and/or respond to concerns submitted through the comment form provided on the study website, as well as via mail, phone, and email.

The information provided during Online PIC 2, including the displays and narration are provided within **Appendix B**, and a copy of the online survey can be reviewed within **Appendix D**.

## 7.0 PARTICIPATION

Statistics were gathered during the PIC period (i.e., August 19, 2021 to September 16, 2021) to determine the number of viewers of the online presentation. The website visit statistics were broken down into the following two categories:

- 1) **Unique Visitors** – the total number of people that visited the site. The same person visiting the site multiple times during the PIC time period is only counted once.
- 2) **Number of Visits** – the total number of visits by browsing session. If a visitor viewed another page on the site within 30 minutes of their last pageview, it is counted as the same visit. If a





## ONLINE PUBLIC INFORMATION CENTRE #2 SUMMARY REPORT

visitor returns to the study website 30 minutes after their last pageview, it is counted as a separate visit.

The data gathered for each category is illustrated below within **Table 2** and **Table 3**. It should be noted that while a daily value can be determined for the number of visits, the unique visitor statistic is only available on a monthly basis.

**Table 2: Number of Unique Visitors to the Study Website**

Unique Visitors	
August 2021	192 visitors
September 2021	117 visitors
<b>Total:</b>	<b>309 visitors</b>

**Table 3: Number of Visits to the Study Website**

Number of Visits	
August 19, 2021 to August 31, 2021	534 visits
September 1, 2021 to September 16, 2021	268 visits
<b>Total:</b>	<b>802 visits</b>

A copy of the raw statistics from the cPanel platform can be reviewed within **Appendix C**.

## 8.0 FEEDBACK

All Online PIC 2 participants were encouraged to provide feedback through a link to the online survey offered at the end of the PIC presentation. In addition, the study team contact information was provided as part of the Online PIC 2 notice, the study website, and the online presentation.

As part of the online survey, participants were asked to answer a series of questions and share their thoughts in relation to the evaluations of alternative cross-sections and alignments, the Technically Recommended Design, the format of the Online PIC and any additional comments they wished to provide. Participants were also encouraged to submit additional comments by emailing the study team directly. In total, 2 survey responses, 1 telephone call, 2 emails from the general public were received following the notification of Online PIC 2 and during the comment period. There were no comments received through the City's social media pushes.

Based on the online survey responses, telephone conversation, and email responses, the following key comment themes were noted:



## ONLINE PUBLIC INFORMATION CENTRE #2 SUMMARY REPORT

- Concern regarding traffic infiltration into adjacent residential communities.
- Concern regarding increased traffic on Weston Road.
- Concern for pedestrian/cyclist safety, especially near the Highway 400 ramp.
- Agreement that the extension will provide much needed alleviation of east-west traffic congestion on Rutherford Road.
- Positive feedback regarding the provision of active transportation facilities within the Technically Recommended Design.
- General satisfaction with the way information has been presented on the Online PIC 2 platform.

A copy of the online survey, as well as all comments and survey responses received is included within **Appendix E**. It should be noted that all names and addresses from respondents were included on the study mailing list.

### 9.0 NEXT STEPS

All comments received as part of Online PIC 2 have been reviewed and considered by members of the study team and will continue to be considered as the Class EA study progresses.

Next steps will include reviewing and considering feedback received as part of Online PIC 2, confirming the Preferred Design and preparing the Environmental Study Report (ESR). Upon finalization of the ESR, a Notice of Study Completion will be issued to initiate a 30-day public review period.

Consultation will continue to be ongoing, and all interested parties who have been requested to be included on the mailing list will be notified of key study milestones, including the Notice of Study Completion.



# Appendix A

## Notification Materials



# NOTICE OF ONLINE PUBLIC INFORMATION CENTRE 2

## MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT STUDY

### Bass Pro Mills Drive, from Highway 400 to Weston Road

#### THE STUDY

The City of Vaughan (City) is undertaking a Municipal Class Environmental Assessment (Class EA) study to assess the need to extend Bass Pro Mills Drive, from Highway 400 to Weston Road, as recommended in the Vaughan Mills Centre Secondary Plan (2014).

These recommendations were made to:

- provide a new east-west multi-modal connection between Highway 400 and Weston Road;
- help alleviate traffic congestion along Rutherford Road;
- support future growth and development within the plan area;
- create new multi-modal transportation connections; and,
- develop a safe and comfortable environment for active transportation users.

#### THE PROCESS

The study is being completed in accordance with the planning and design process for Schedule 'C' projects, as outlined in the Municipal Engineers Association (MEA) Municipal Class EA guidelines (October 2000, amended 2007, 2011 and 2015), which is approved under the Ontario Environmental Assessment Act. Upon study completion, an Environmental Study Report (ESR) will be made available for public review and comment. The ESR will document the study, consultation process and decision-making rationale.

#### ONLINE PUBLIC INFORMATION CENTRE 2

The City of Vaughan values the voice of its citizens and is dedicated to promoting a dialogue with the community that is open, transparent, accessible, and inclusive. Consultation with citizens is a key component of the study and as part of this Class EA process.

Online Public Information Centre 1 (PIC 1) for this study was held between December 3, 2020 and January 8, 2021, to present and obtain feedback on the study background, existing study area conditions, the evaluation of alternative solutions and next steps in the study process. A second PIC has been arranged to present and obtain feedback on the outcomes of PIC 1, the traffic analysis and environmental investigations, the evaluations of alternative alignments and cross-sections, the Technically Recommended Design, and the next steps in the study process. In light of COVID-19 and associated physical distancing requirements, PIC 2 will be hosted online. **Online PIC 2 will be available for your review on the project website at [Vaughan.ca/BassProMillsEA](http://Vaughan.ca/BassProMillsEA) on August 19, 2021.** A narrated presentation will be available as part of the online PIC, as well as an online survey, until **September 16, 2021.**

#### CONTACT US

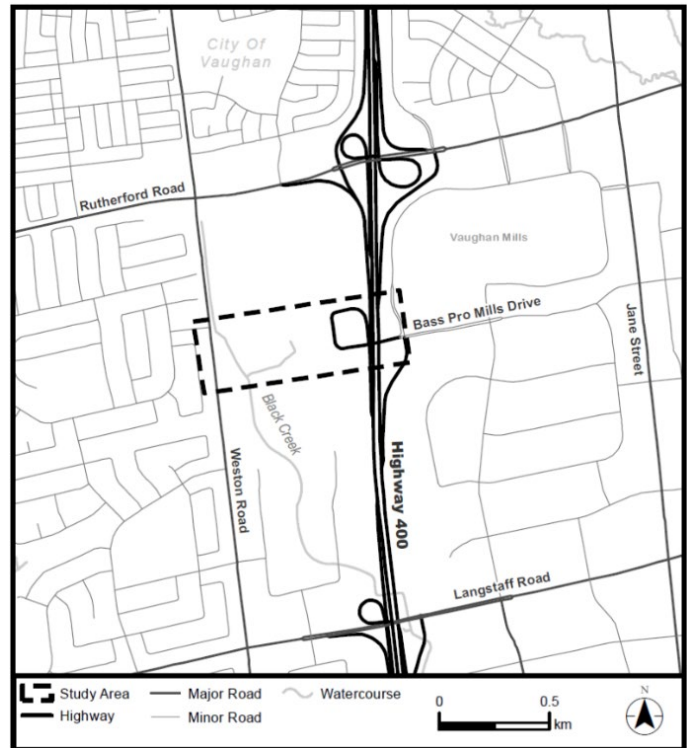
To join the study mailing list or to share comments, please contact:

**Hilda Esedebe, P.Eng.**  
**City of Vaughan**  
**Project Manager**  
2141 Major Mackenzie Dr.  
Vaughan, ON L6A 1T1  
T: 905-832-2281, ext. 8484

E: [Hilda.Esedebe@vaughan.ca](mailto:Hilda.Esedebe@vaughan.ca)

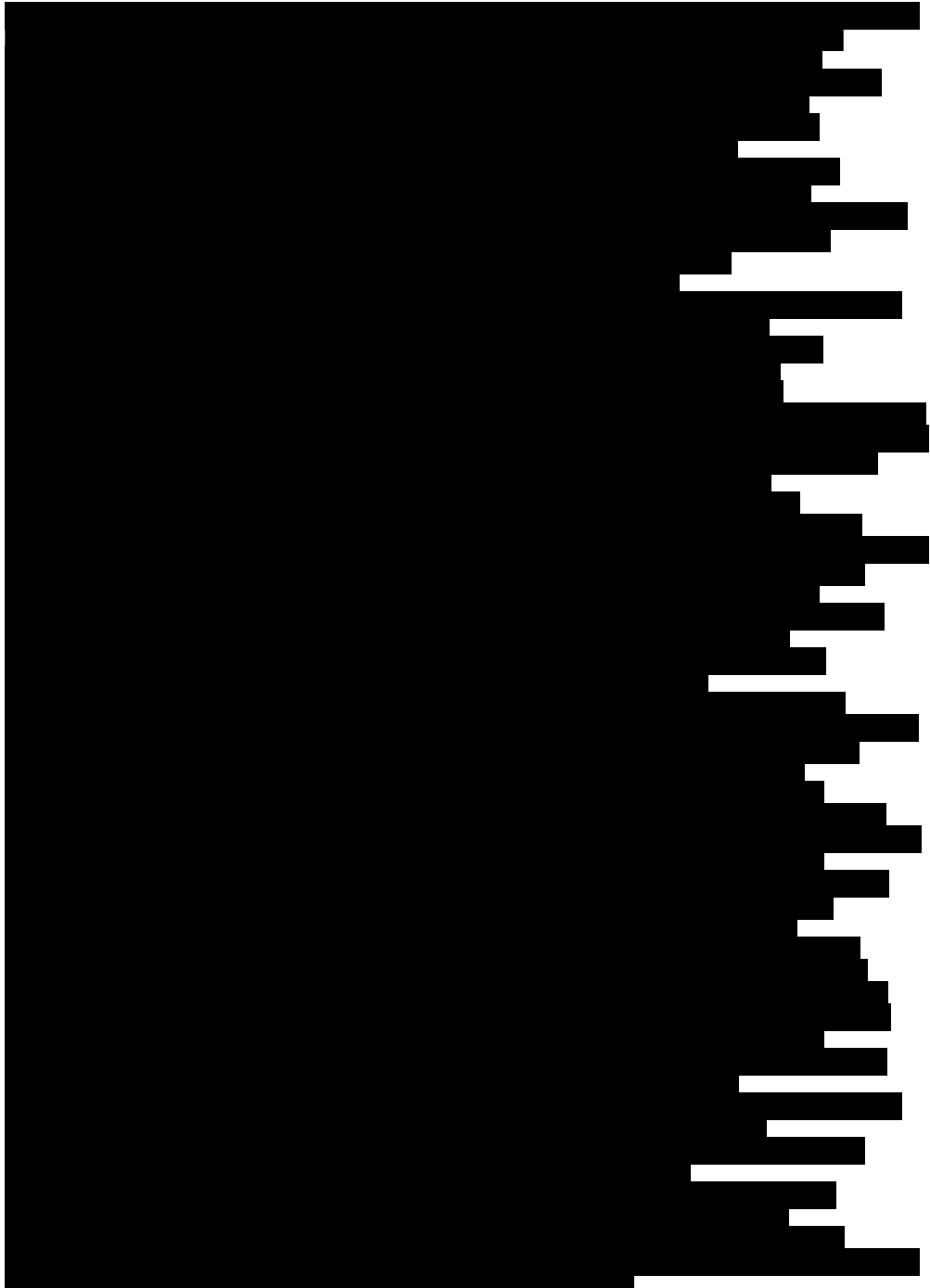
**Diana Addley**  
**Stantec Consulting Ltd.**  
**Senior Environmental Planner**  
300W-675 Cochrane Drive  
Markham, ON L3R 0A8  
T: 905-415-6401

E: [Diana.Addley@stantec.com](mailto:Diana.Addley@stantec.com)



Personal information on this form is collected under the authority of the Municipal Act, 2001 and will be used for the purpose of administering the Bass Pro Mills Environmental Assessment Study (between Highway 400 and Weston Road). Questions about this collection can be directed to the Manager, Transportation Planning, City of Vaughan, 2141 Major Mackenzie Drive, Vaughan, ON L6A 1T1, 905-832-8585.

**From:** [Robinson, Jennifer](#)  
**To:** [Robinson, Jennifer](#)  
**Cc:** [Hilda Esedebe](#); [Cholewa, Peter](#); [Addley, Diana](#)  
**Bcc:**



**Subject:** Notice of Online PIC 2 - MCEA Class EA, Bass Pro Mills Drive (Hwy 400 to Weston Rd)  
**Date:** Thursday, August 12, 2021 3:26:00 PM  
**Attachments:** [160540006\\_basspromills\\_notice\\_PIC2\\_20210727.pdf](#)

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Hello,

Please see the attached Notice of Online Public Information Centre (PIC) 2 for the **Bass Pro Mills Drive Municipal Class Environmental Assessment (EA) Study**. As indicated within the attached notice, the City of Vaughan is undertaking this EA Study for the proposed extension of Bass Pro Mills Drive, between Highway 400 and Weston Road. The purpose of this notice is to inform of the Online Public Information

Centre that has been arranged to present and solicit feedback on the outcomes of PIC 1, the traffic analysis and environmental investigations, the evaluations of alternative alignments and cross-sections, the Technically Recommended Design and the next steps in the study process. As part of this online PIC, a recorded presentation and comment form will be available for your review and feedback on the project website ([Vaughan.ca/BassProMillsEA](http://Vaughan.ca/BassProMillsEA)) from **August 19, 2021 to September 16, 2021**.

Should you have any questions or concerns, please do not hesitate to contact us.

Regards,

**Jenn Robinson**

Environmental Planner, Transportation GTA

OSEC, Whitby Office

[Jennifer.Robinson@stantec.com](mailto:Jennifer.Robinson@stantec.com)

Stantec



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**CITY OF VAUGHAN  
NOTICE OF PUBLIC MEETING  
REGARDING PROPOSED AREA SPECIFIC DEVELOPMENT CHARGE  
BY-LAWS - EDGELEY POND AND PARK AND BLACK CREEK CHANNEL  
WORKS**

**COMMITTEE OF THE WHOLE MEETING  
Tuesday, September 14, 2021 at 1:00 P.M.  
Electronic Meeting**

TAKE NOTICE that at the Committee of the Whole meeting of September 14, 2021, a public meeting will be held by The Corporation of the City of Vaughan pursuant to Section 12 of the Development Charges Act, 1997, R.S.O., 1997 for the purpose of receiving input from the public on the following matters:

**Area Specific Development Charge Background Study and Proposed  
Development Charge By-Law - Edgeley Pond and Park and Black Creek  
Channel Works**

In order that sufficient information is available to the public, a copy of the background study and proposed by-law will be made available for viewing as of August 30, 2021 on the City of Vaughan website at [www.vaughan.ca/developmentcharges](http://www.vaughan.ca/developmentcharges).

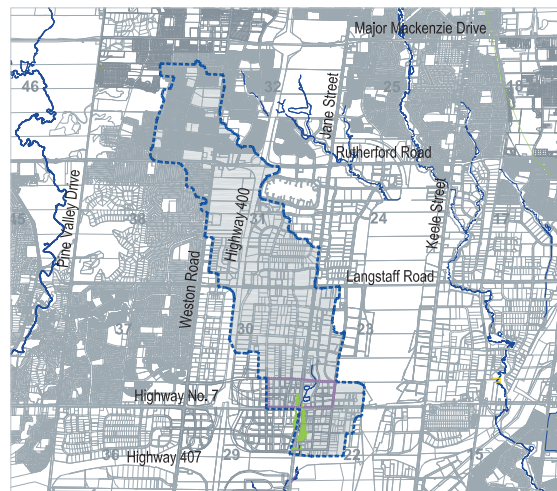
This is an Electronic Meeting. The Council Chamber will not be open to the public. Any member of the public may attend the public meeting to make an oral submission via electronic participation. If you wish to speak to this item, please pre-register by contacting Access Vaughan at 905-832-2281 or [clerks@vaughan.ca](mailto:clerks@vaughan.ca) by noon on the last business day before the meeting on Monday, September 13, 2021.

Alternatively, written submissions may be e-mailed to [clerks@vaughan.ca](mailto:clerks@vaughan.ca), and must be received in the Office of the City Clerk no later than 12:00 noon on Monday, September 13, 2021 in order to receive advance circulation. For further information call 905-832-8585 ext. 8482.

The Agenda, including any applicable Staff Report(s) on the above matters will also be posted at [www.vaughan.ca](http://www.vaughan.ca) on Tuesday, September 7, 2021.

This Notice is provided in accordance with the Development Charges Act, 1997, R.S.O., 1997 and Ontario Regulation 82/98.

**Todd Coles  
City Clerk**



AREA SPECIFIC  
DEVELOPMENT CHARGE  
Edgeley Pond and Black Creek  
Channel Works

KEY MAP

BENEFITING AREAS - MAP 1  
 BENEFITING AREAS - MAP 2  
 BENEFITING AREAS - MAP 3

NOT TO SCALE



June 8, 2016



**NOTICE OF ONLINE PUBLIC INFORMATION CENTRE 2  
MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT STUDY  
Bass Pro Mills Drive, from Highway 400 to Weston Road**

**THE STUDY**

The City of Vaughan (City) is undertaking a Municipal Class Environmental Assessment (Class EA) study to assess the need to extend Bass Pro Mills Drive, from Highway 400 to Weston Road, as recommended in the Vaughan Mills Centre Secondary Plan (2014). These recommendations were made to:

- provide a new east-west multi-modal connection between Highway 400 and Weston Road;
- help alleviate traffic congestion along Rutherford Road;
- support future growth and development within the plan area;
- create new multi-modal transportation connections; and,
- develop a safe and comfortable environment for active transportation users



**THE PROCESS**

The study is being completed in accordance with the planning and design process for Schedule 'C' projects, as outlined in the Municipal Engineers Association (MEA) Municipal Class EA guidelines (October 2000, amended 2007, 2011 and 2015), which is approved under the Ontario Environmental Assessment Act. Upon study completion, an Environmental Study Report (ESR) will be made available for public review and comment. The ESR will document the study, consultation process and decision-making rationale.

**ONLINE PUBLIC INFORMATION CENTRE 2**

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**Diana Addley**  
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This Notice was first issued August 12, 2021.



## NOTICE OF ONLINE PUBLIC INFORMATION CENTRE 2 MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT STUDY Bass Pro Mills Drive, from Highway 400 to Weston Road

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This Notice was first issued August 12, 2021.

## NEWS

# 'GET INVOLVED'

## PARK COMING TO PETER RUPERT AVENUE AND RUTHERFORD ROAD AREA

DINA AL-SHIBEEB  
[dibrahim@yrmg.com](mailto:dibrahim@yrmg.com)

Vaughan citizens have until Sunday, Aug. 15 to bring their insights for "final design" of the new 7.7-hectare park coming to Peter Rupert Avenue and Rutherford Road, the city said July 29 on its website, urging people to "get involved."

The park will be segmented in different zones, one for children to play, another for event staging and concerts and one area for connections to trails and greenspace.

It will be built on approx-



Metroland file photo

You have until Aug. 15 to have your say on the new 7.7-hectare park coming to the city.

imately 28.5 hectares (70 acres) of greenspace that includes parkland, trails, stormwater ponds and the Cook Woodlot, "allowing for beautiful nature views," the City said July 29.

"This central location will also have direct connections to the Rutherford GO station and several nearby communities."

The city also described the proposed park as "dynamic, welcoming and inspiring gathering place

with a wide range of amenities for the entire community to enjoy."

Citizens can visit [vaughan.ca/PeterRupert-Park](http://vaughan.ca/PeterRupert-Park) to participate in the online self-guided public open house to:

- Learn about and provide feedback on the proposed park concept plans.
- Find out more about the design process and work completed to date.
- Hear about the next steps for the project.

# 'NOW IS THE TIME TO GET VACCINATED': SCHOOL BOARD

DINA AL-SHIBEEB  
[dibrahim@yrmg.com](mailto:dibrahim@yrmg.com)

York Region District School Board (YRDSB)'s official Twitter account is active with messages urging students to get vaccinated and making them aware of services around them to get the shot.

On July 22, the YRDSB also retweeted its message, saying how "COVID-19 infection can have serious consequences for all age groups, including young people."

"Now is the time to get vaccinated," it added.

"By working together to make sure everyone who is eligible to get a vaccine can get one, we can reduce the number of cases of COVID-19 in our community

and our school."

So far, the YRDSB has drawn criticism for introducing the hybrid model of education for next year. Its counterpart, the Catholic district school board, has axed the hybrid model for its elementary schools, but not for secondary. The YRDSB, however, stated that if enough students opt for in-person learning, there won't be any hybrid education.

Students aged 12 to 17 who haven't received their first dose "should get one as soon as possible or get your second dose if your last dose was at least 21 days ago."

For those who require assistance booking their appointment or with transportation to get to a clinic,

they can call Access York at 1-877-464-9675.

If anyone has questions or concerns about COVID-19 vaccinations, we encourage you to speak with your health care provider or call the York Region Health Connection line at 1-800-361-5653 (TTY: 1-866-512-6228) and speak with a public health nurse. They can provide you with reliable, up-to-date information to help you make an informed decision.

A total of 1,552,984 vaccines have been administered to York Region residents living anywhere in Ontario.

As of July 21, 845,804 York Region residents have received at least one dose and 707,180 have received two doses. That amounts to 81.5 per cent of adults 18 years and older have received one dose, while 69.5 per cent of adults are fully vaccinated with two doses.





## NOTICE OF ONLINE PUBLIC INFORMATION CENTRE 2 MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT STUDY Bass Pro Mills Drive, from Highway 400 to Weston Road

### THE STUDY

The City of Vaughan (City) is undertaking a Municipal Class Assessment Environmental Assessment (Class EA) study to assess the need to extend Bass Pro Mills Drive, from Highway 400 to Weston Road, as recommended in the Vaughan Mills Centre Secondary Plan (2014). These recommendations were made to:

- provide a new east-west multi-modal connection between Highway 400 and Weston Road;
- help alleviate traffic congestion along Rutherford Road;
- support future growth and development within the plan area;
- create new multi-modal transportation connections; and,
- develop a safe and comfortable environment for active transportation users



### THE PROCESS

The study is being completed in accordance with the planning and design process for Schedule 'C' projects, as outlined in the Municipal Engineers Association (MEA) Municipal Class EA guidelines (October 2000, amended 2007, 2011 and 2015), which is approved under the Ontario Environmental Assessment Act. Upon study completion, an Environmental Study Report (ESR) will be made available for public review and comment. The ESR will document the study, consultation process and decision-making rationale.

### ONLINE PUBLIC INFORMATION CENTRE 2

The City of Vaughan values the voice of its citizens and is dedicated to promoting a dialogue with the community that is open, transparent, accessible, and inclusive. Consultation with citizens is a key component of the study and as part of this Class EA process.

Online Public Information Centre 1 (PIC 1) for this study was held between December 3, 2020 and January 8, 2021, to present and obtain feedback on the study background, existing study area conditions, the evaluation of alternative solutions and next steps in the study process. A second PIC has been arranged to present and obtain feedback on the outcomes of PIC 1, the traffic analysis and environmental investigations, the evaluations of alternative alignments and cross-sections, the Technically Recommended Design, and the next steps in the study process. In light of COVID-19 and associated physical distancing requirements, PIC 2 will be hosted online. Online **PIC 2 will be available for your review on the project website at [Vaughan.ca/BassProMillsEA](http://Vaughan.ca/BassProMillsEA) on August 19, 2021.** A narrated presentation will be available as part of the online PIC, as well as an online survey, until **September 16, 2021.**

### CONTACT US

To join the study mailing list or to share comments, please contact:

**Hilda Esedebe, P.Eng.**  
City of Vaughan  
Project Manager  
2141 Major Mackenzie Dr.  
Vaughan, ON L6A 1T1  
T: 905-832-2281, ext. 8484  
E: [Hilda.Esedebe@vaughan.ca](mailto:Hilda.Esedebe@vaughan.ca)

**Diana Addley**  
Stantec Consulting Ltd.  
Senior Environmental Planner  
300W-675 Cochrane Drive  
Markham, ON L3R 0A8  
T: 905-415-6401  
E: [Diana.Addley@stantec.com](mailto:Diana.Addley@stantec.com)

Personal information on this form is collected under the authority of the Municipal Act, 2001 and will be used for the purpose of administering the Bass Pro Mills Environmental Assessment Study (between Highway 400 and Weston Road). Questions about this collection can be directed to the Manager, Transportation Planning, City of Vaughan, 2141 Major Mackenzie Drive, Vaughan, ON L6A 1T1, 905-832-8585.

This Notice was first issued August 12, 2021.

# It's time to vote!

We received hundreds of truly inspiring nominations from newcomers coast to coast who have made a positive impact since their arrival in Canada. Now it's your turn to help choose this year's Top 25 Canadian Immigrant award winners!

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## NOTICE OF ONLINE PUBLIC INFORMATION CENTRE 2 MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT STUDY Bass Pro Mills Drive, from Highway 400 to Weston Road

### THE STUDY

The City of Vaughan (City) is undertaking a Municipal Class Environmental Assessment (Class EA) study to assess the need to extend Bass Pro Mills Drive, from Highway 400 to Weston Road, as recommended in the Vaughan Mills Centre Secondary Plan (2014). These recommendations were made to:

- provide a new east-west multi-modal connection between Highway 400 and Weston Road;
- help alleviate traffic congestion along Rutherford Road;
- support future growth and development within the plan area;
- create new multi-modal transportation connections; and,
- develop a safe and comfortable environment for active transportation users



### THE PROCESS

The study is being completed in accordance with the planning and design process for Schedule 'C' projects, as outlined in the Municipal Engineers Association (MEA) Municipal Class EA guidelines (October 2000, amended 2007, 2011 and 2015), which is approved under the Ontario Environmental Assessment Act. Upon study completion, an Environmental Study Report (ESR) will be made available for public review and comment. The ESR will document the study, consultation process and decision-making rationale.

### ONLINE PUBLIC INFORMATION CENTRE 2

The City of Vaughan values the voice of its citizens and is dedicated to promoting a dialogue with the community that is open, transparent, accessible, and inclusive. Consultation with citizens is a key component of the study and as part of this Class EA process.

Online Public Information Centre 1 (PIC 1) for this study was held between December 3, 2020 and January 8, 2021, to present and obtain feedback on the study background, existing study area conditions, the evaluation of alternative solutions and next steps in the study process. A second PIC has been arranged to present and obtain feedback on the outcomes of PIC 1, the traffic analysis and environmental investigations, the evaluations of alternative alignments and cross-sections, the Technically Recommended Design, and the next steps in the study process. In light of COVID-19 and associated physical distancing requirements, PIC 2 will be hosted online. Online **PIC 2 will be available for your review on the project website at [Vaughan.ca/BassProMillsEA](http://Vaughan.ca/BassProMillsEA) on August 19, 2021.** A narrated presentation will be available as part of the online PIC, as well as an online survey, until **September 16, 2021.**

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2141 Major Mackenzie Dr.  
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**Diana Addley**  
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This Notice was first issued August 12, 2021.



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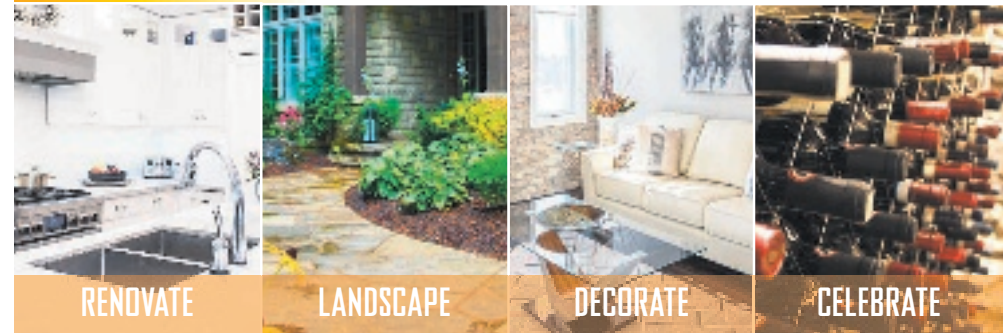
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# Public Service Announcement

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## Help shape the extension of Bass Pro Mills Drive

*Provide feedback and help inform the City's proposed extension plan*

**VAUGHAN, ON (Aug. 19, 2021)** – What should be considered for the proposed extension of Bass Pro Mills Drive? The City of Vaughan is hosting another virtual Public Information Centre as part of the Bass Pro Mills Extension Municipal Class Environmental Assessment (MCEA) Study. You can get involved and help shape the future of this road. The session, which includes a narrated presentation and comment form, is available until Thursday, Sept. 16 at [vaughan.ca/BassProMillsEA](https://vaughan.ca/BassProMillsEA).

As recommended in the [Vaughan Mills Centre Secondary Plan \(2014\)](#) (PDF), the study will assess the need to extend Bass Pro Mills Drive from Highway 400 to Weston Road. These recommendations were made to:

- provide a new east-west transportation network between Highway 400 and Weston Road, including a new route for York Region Transit.
- help distribute east-west traffic and alleviate congestion along Rutherford Road to the north.
- support future growth and development within the plan area.
- develop a safe and effective environment for active transportation users.

As part of the process, the Bass Pro Mills Extension MCEA Study will define problems and opportunities, identify solutions and determine a design for recommended improvements that align with the area's natural, social, economic and cultural environment. Upon study completion, an Environmental Study Report will be created.

Public engagement is vital to the development and success of this study. Through previous input opportunities, including a survey and an initial Public Information Session, the City gathered feedback on alternative solutions and shared key findings. Now, citizens are invited to get involved in the next stage of the study by participating in a second virtual Public Information Centre until Thursday, Sept. 16. In doing so, participants will:

- learn more about the project, including key findings collected and planned next steps.
- share thoughts on current challenges, potential solutions, active transportation opportunities and more for the study area.
- review alternative alignments and the recommended design for the road.
- understand potential impacts, such as traffic and environmental impacts, and proposed mitigation measures.

Get started at [vaughan.ca/BassProMillsEA](https://vaughan.ca/BassProMillsEA).

To be a fully connected and integrated community, the City is committed to undertaking initiatives like the proposed Bass Pro Mills Extension to improve the municipal road network, promote the development of

# Public Service Announcement

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transit and increase cycling and pedestrian-friendly infrastructure. This project also supports Vaughan's Transportation and Mobility objective – a priority outlined in the [2018-2022 Term of Council Service Excellence Strategic Plan](#).

-30-

**ABOUT VAUGHAN:** The City of Vaughan is one of Canada's fastest growing cities with a population of more than 335,000. Incorporated in 1991, Vaughan includes the communities of Concord, Kleinburg, Maple, Thornhill and Woodbridge. This culturally diverse municipality is located in the heart of York Region and the Greater Toronto Area.

**MEDIA CONTACT:** Teresa Fazari, Corporate and Strategic Communications  
T. 905-832-2281, ext. 8185 E. [teresa.fazari@vaughan.ca](mailto:teresa.fazari@vaughan.ca)

[vaughan.ca](http://vaughan.ca)



# Social Media

## Bass Pro Mills EA Study – Public Information Centre

**DATE:** Aug. 19 to Sept. 16, 2021

**EXTERNAL LEAD:** Teresa Fazari

**PCE LEAD REVIEW:** Catherine Vettese

### ORGANIC SOCIAL CAMPAIGN

Date	Platform	Message	Image
Aug. 19	Twitter Facebook Instagram	<p>What should be considered for the proposed extension of Bass Pro Mills Drive? Until Thursday, Sept. 16, participate in a virtual Public Information Centre to have your say on the City's extension plans. Learn more: <a href="#">PSA LINK</a></p> <p>Character count: 223 Friendly URL: <a href="http://vaughan.ca/BassProMillsEA">vaughan.ca/BassProMillsEA</a></p>	
Aug. 19	LinkedIn	<p>What should be considered for the proposed extension of Bass Pro Mills Drive? The City is hosting a virtual Public Information Centre, which includes a narrated presentation and comment form, as part of the Bass Pro Mills Extension Municipal Class Environmental Assessment Study. Until Thursday, Sept. 16, you can get involved and help shape the future of this road. Learn more: <a href="#">PSA LINK</a></p> <p>Character count: 410</p>	
Aug. 26	Twitter	<p>The City is carrying out a study for the extension of Bass Pro Mills Drive from Highway 400 to Weston Road. Alternative road designs, active transportation options and more are being considered. Have your say at <a href="http://vaughan.ca/BassProMillsEA">vaughan.ca/BassProMillsEA</a></p> <p>Character count: 237</p>	
Aug. 26	Facebook	<p>The City is carrying out a study for the extension of Bass Pro Mills Drive from Highway 400 to Weston Road. Alternative road designs, active transportation options, a new intersection and more are being considered. Until Sept. 16, participate in a virtual Public Information Centre to have your say. Get started at <a href="http://vaughan.ca/BassProMillsEA">vaughan.ca/BassProMillsEA</a></p> <p>Character count: 340</p>	
Aug. 26	Instagram	<p>The City's study to extend Bass Pro Mills Drive from Highway 400 to Weston Road is underway. Alternative road designs, active transportation</p>	

# Social Media

		<p>options, a new intersection and more are being considered. Until Sept. 16, participate in a virtual Public Information Centre to have your say. Get involved at <a href="http://vaughan.ca/BassProMillsEA">vaughan.ca/BassProMillsEA</a></p> <p>Character count: 328</p>	
Sept. 2	Twitter	<p>Have your say on the City's extension plan for Bass Pro Mills Drive, between Highway 400 and Weston Road. Visit <a href="http://vaughan.ca/BassProMillsEA">vaughan.ca/BassProMillsEA</a> until Sept. 16 to learn more about the study and help shape the future of this road.</p> <p>Character count: 223</p>	
Sept. 2	Facebook	<p>The Bass Pro Mills Drive extension study includes alternative solutions and recommendations for an extension between Highway 400 and Weston Road. This includes road designs, active transportation options, a new intersection and more. How should this area look? Visit <a href="http://vaughan.ca/BassProMillsEA">vaughan.ca/BassProMillsEA</a> to have your say.</p> <p>Character count: 309</p>	
Sept. 2	Instagram	<p>Have your say on the City's extension plan for Bass Pro Mills Drive. The study includes alternative solutions and recommendations for an extension between Highway 400 and Weston Road, including road designs, active transportation options, a new intersection and more. Get started at <a href="http://vaughan.ca/BassProMillsEA">vaughan.ca/BassProMillsEA</a></p> <p>Character count: 308</p>	
Sept. 9	Twitter	<p>There's still time to get involved! The City has initiated the Bass Pro Mills Drive extension study, which considers alternative solutions for the extension of the road from Highway 400 to Weston Road. Provide input until Sept. 16 at <a href="http://vaughan.ca/BassProMillsEA">vaughan.ca/BassProMillsEA</a></p> <p>Character count: 223</p>	
Sept. 9	Facebook Instagram	<p>The City has initiated a Bass Pro Mills Drive extension study to consider alternative solutions for the extension of the road between Highway 400 and Weston Road. There's still time to get involved! Provide input on proposed alignments, cross-sections, the recommended design and more until Sept. 16 at <a href="http://vaughan.ca/BassProMillsEA">vaughan.ca/BassProMillsEA</a></p> <p>Character count: 292</p>	

# Social Media

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Sept. 16	Twitter	<p>#Reminder: today is the last day to participate in the virtual Public Information Centre to review and comment on the City's Bass Pro Mills Drive extension study. Get started at <a href="http://vaughan.ca/BassProMillsEA">vaughan.ca/BassProMillsEA</a></p> <p>Character count: 209</p>	
Sept. 16	Facebook Instagram	<p>#Reminder: today is the last day to participate in the virtual Public Information Centre to review and comment on the City's Bass Pro Mills Drive extension study. Plans are in place to extend the road from Highway 400 to Weston Road. Get started at <a href="http://vaughan.ca/BassProMillsEA">vaughan.ca/BassProMillsEA</a></p> <p>Character count: 274</p>	

## NOTES:

- **Tags:** use .@TAG if the tag is at the start of the post. If the '.' is not inserted, it'll post as a private DM (not public). Tags will also need to be inserted manually per platform.
- **Hashtags:** ensure hashtags are official and widely used. Event-specific hashtags need to be pre-approved
- **URL:** use friendly URLs only, i.e. [vaughan.ca/news](http://vaughan.ca/news) vs [www.vaughan.ca/news/pages/default.aspx](http://www.vaughan.ca/news/pages/default.aspx)
- **Character count:** stick to 280 characters for Twitter and up to 400 characters for Facebook, Instagram and LinkedIn
- **Paid sponsored posts:** images for paid ads must have little to no text, or else it will get denied. Less than 1/3 of the image can have text.



**Help shape the extension  
of Bass Pro Mills Drive**

**Get involved!**

**Virtual Public Information Centre  
[vaughan.ca/BassProMillsEA](http://vaughan.ca/BassProMillsEA)**





# Help shape the extension of Bass Pro Mills Drive

Provide feedback and help inform  
the City's proposed extension plan

# **Appendix B**

## **Online PIC 2 Presentation (Displays and Script)**



## **Bass Pro Mills Extension - Online Public Information Centre 2**

### ***1.1 Online Public Information Centre 2***



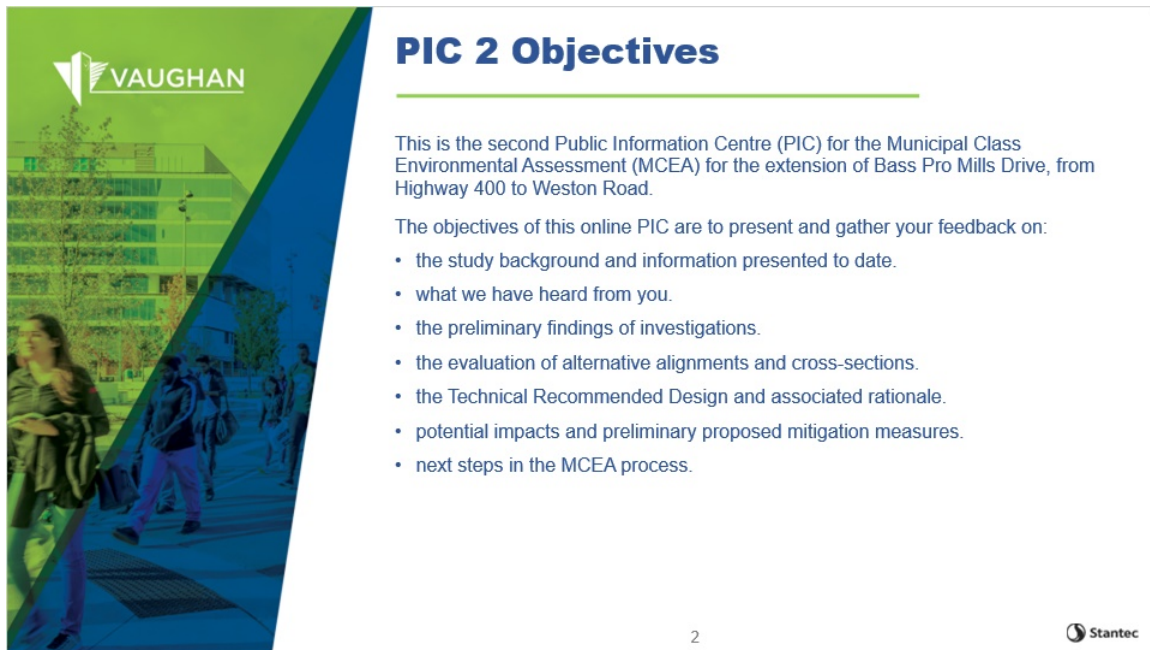
Hello, and thank you for joining us for this Online Public Information Centre for the Bass Pro Mills Extension Schedule C Municipal Class Environmental Assessment study.

This video presentation will provide you with an overview of this Municipal Class Environmental Assessment (or MCEA) study. A PDF copy of this presentation is also available for your review by clicking the Resources tab located at the top right of this presentation.

This is the second and final Public Information Centre for this study. In light of COVID-19 and associated physical distancing requirements, this PIC is being hosted online.

Please stay tuned until the end of the presentation to discover ways in which you can provide valuable feedback to the project team.

## 1.2 PIC 2 Objectives



**VAUGHAN**

### PIC 2 Objectives

This is the second Public Information Centre (PIC) for the Municipal Class Environmental Assessment (MCEA) for the extension of Bass Pro Mills Drive, from Highway 400 to Weston Road.

The objectives of this online PIC are to present and gather your feedback on:

- the study background and information presented to date.
- what we have heard from you.
- the preliminary findings of investigations.
- the evaluation of alternative alignments and cross-sections.
- the Technical Recommended Design and associated rationale.
- potential impacts and preliminary proposed mitigation measures.
- next steps in the MCEA process.

2

Stantec

As noted, this is the second and final Public Information Centre (or PIC) for this Municipal Class Environmental Assessment study for the proposed extension of Bass Pro Mills Drive, from Highway 400 to Weston Road.

The objectives of this online PIC are to present and gather your feedback on:

- The study background and information presented at PIC 1
- What we have heard from you
- The preliminary findings of investigations
- The evaluation of alternative alignments and cross-sections
- The Technical Recommended Design and associated rationale
- Potential impacts and preliminary proposed mitigation measures
- Next steps in the MCEA process

### 1.3 Study Overview



The slide features a green and blue background on the left with the City of Vaughan logo and a photograph of people walking. The main content area is titled "Study Overview" and includes an aerial map of the study area with a yellow rectangular highlight. Below the map is a small inset image of a brochure titled "VAUGHAN MILLS CENTRE SECONDARY PLAN" by DIALOG. To the right of the map, there is a text block explaining the proposed extension of Bass Pro Mills Drive.

#### Study Overview

The proposed extension of Bass Pro Mills Drive is envisaged to support future development in the study area, including the employment and intensification plans developed as part of the Vaughan Mills Centre Secondary Plan (VMCSP).

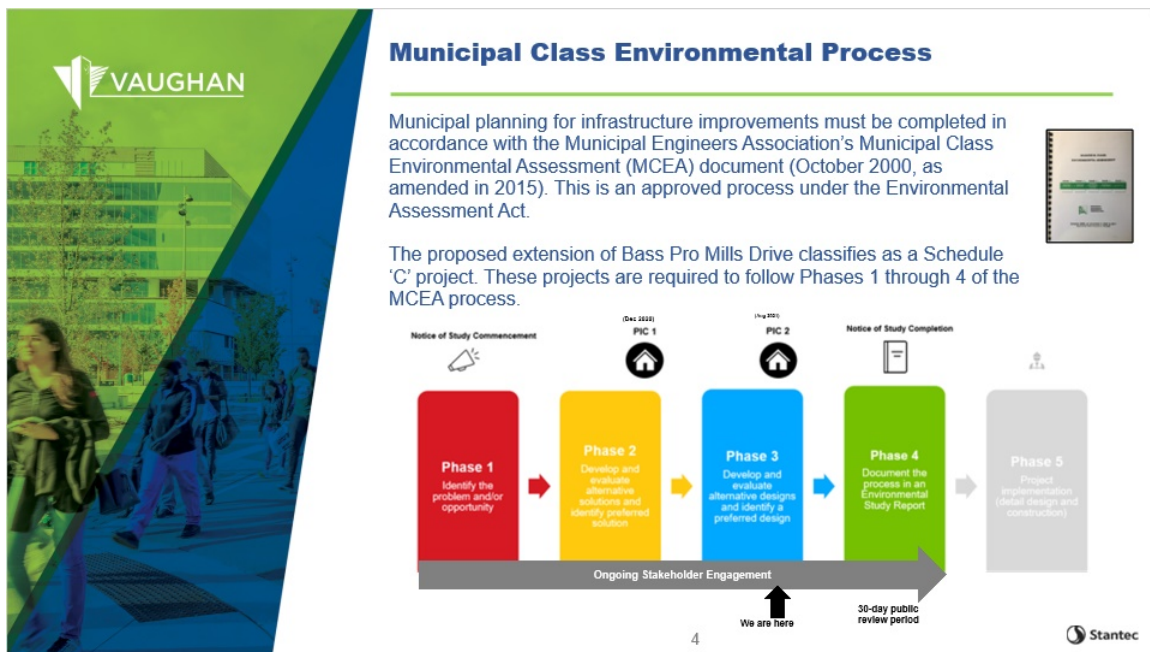
3 

The City of Vaughan is undertaking this Municipal Class Environmental Assessment (or MCEA) study for the proposed extension of Bass Pro Mills Drive, from Highway 400 westerly to Weston Road. This extension would provide a new major collector roadway that unites neighbourhoods from Weston Road to Jane Street, redistributes east-west traffic and alleviates congestion on Rutherford Road.

An enhanced boulevard could accommodate new York Region Transit amenities, a pedestrian friendly multi-use trail, as well as on-street cycling facilities.

The proposed extension of Bass Pro Mills Drive is envisaged to support future development in the study area, which is outlined within the figure on this slide, including the employment and intensification plans developed as part of the Vaughan Mills Centre Secondary Plan (VMCSP).

## 1.4 Municipal Class Environmental Process



Municipal planning for infrastructure improvements must be completed in accordance with the Municipal Engineers Association's Municipal Class Environmental Assessment (MCEA) document, which is an approved process under the Environmental Assessment Act.

The proposed extension of Bass Pro Mills Drive classifies as a Schedule 'C' project. These projects are required to follow Phases 1 through 4 of the MCEA process.

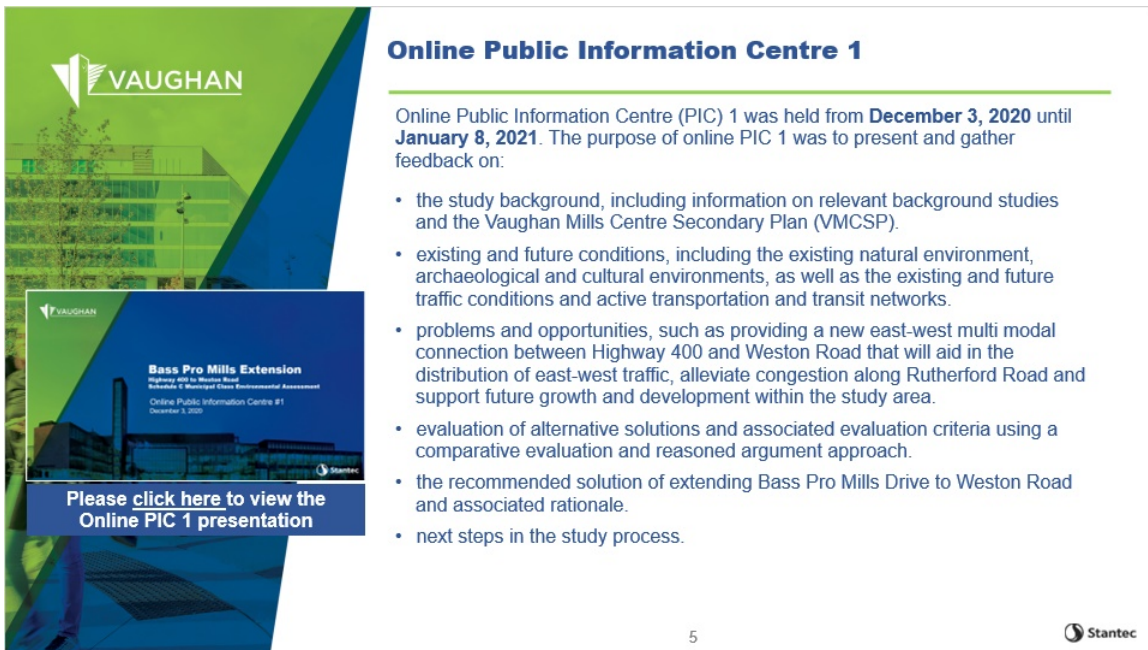
The four phases of the Municipal Class Environmental Process are:

- Phase 1 – Identify the problem and/or opportunity
- Phase 2 – Develop and evaluate alternative solutions and identify a preferred solution
- Phase 3 – Develop and evaluate alternative designs and identify a preferred design
- Phase 4 – Document the process in an Environmental Study Report, which is available for a 30-day public review period before the project is able to move on to the next and final phase of the MCEA process, Project Implementation.

As indicated within the timeline figure, consultation with the public and community stakeholders is carried out throughout the study process and you are encouraged to participate and share your ideas with members of the study team at any time.

At this time, we are currently within Phase 3 of the study process, and we will confirm the preferred design once we have reviewed and considered the feedback received during this Online PIC.

## 1.5 Online Public Information Centre 1




**Online Public Information Centre 1**

Online Public Information Centre (PIC) 1 was held from **December 3, 2020** until **January 8, 2021**. The purpose of online PIC 1 was to present and gather feedback on:

- the study background, including information on relevant background studies and the Vaughan Mills Centre Secondary Plan (VMCSP).
- existing and future conditions, including the existing natural environment, archaeological and cultural environments, as well as the existing and future traffic conditions and active transportation and transit networks.
- problems and opportunities, such as providing a new east-west multi modal connection between Highway 400 and Weston Road that will aid in the distribution of east-west traffic, alleviate congestion along Rutherford Road and support future growth and development within the study area.
- evaluation of alternative solutions and associated evaluation criteria using a comparative evaluation and reasoned argument approach.
- the recommended solution of extending Bass Pro Mills Drive to Weston Road and associated rationale.
- next steps in the study process.

Please click here to view the Online PIC 1 presentation

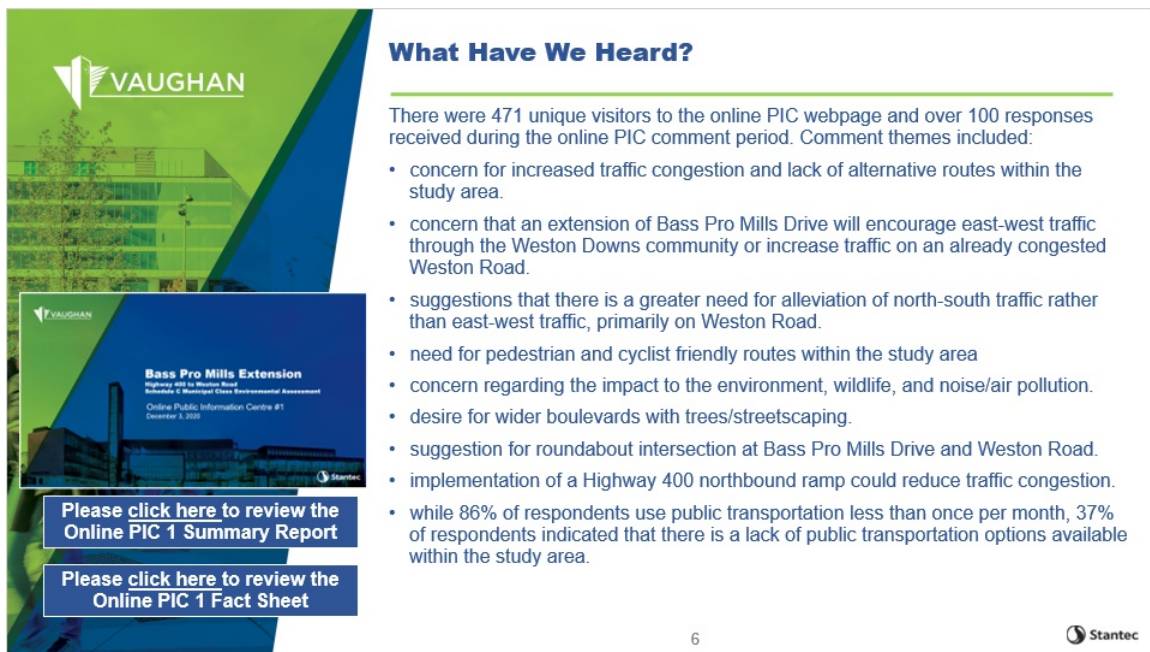
5 

As noted, this is the second and final PIC for this study. Online PIC 1 was held from December 3, 2020 until January 8, 2021 via a recorded presentation on the project website. The purpose of Online PIC 1 was to present and gather feedback on:

- The study background, including the Vaughan Mills Centre Secondary Plan (VMCSP) and other relevant policy directives
- Existing natural and technical conditions within the study area, as well as existing and future transportation conditions
- The Problem and Opportunity statement for the project
- The evaluation of Alternative Solutions and the recommended solution, which is to extend Bass Pro Mills Drive from its existing terminus at Highway 400, westerly to Weston Road
- Next steps in the study process

Please use the link on the left side of the slide to review the materials presented as part of Online Public Information Centre 1.

## 1.6 What Have We Heard?




**What Have We Heard?**

There were 471 unique visitors to the online PIC webpage and over 100 responses received during the online PIC comment period. Comment themes included:

- concern for increased traffic congestion and lack of alternative routes within the study area.
- concern that an extension of Bass Pro Mills Drive will encourage east-west traffic through the Weston Downs community or increase traffic on an already congested Weston Road.
- suggestions that there is a greater need for alleviation of north-south traffic rather than east-west traffic, primarily on Weston Road.
- need for pedestrian and cyclist friendly routes within the study area
- concern regarding the impact to the environment, wildlife, and noise/air pollution.
- desire for wider boulevards with trees/streetscaping.
- suggestion for roundabout intersection at Bass Pro Mills Drive and Weston Road.
- implementation of a Highway 400 northbound ramp could reduce traffic congestion.
- while 86% of respondents use public transportation less than once per month, 37% of respondents indicated that there is a lack of public transportation options available within the study area.

Please click here to review the Online PIC 1 Summary Report

Please click here to review the Online PIC 1 Fact Sheet

6 


There were 471 unique visitors to the Online PIC webpage and over 100 responses received during the Online PIC comment period. The main comment themes we heard included:

- Concern for increased traffic congestion and lack of alternative routes within the study area.
- Concern that an extension of Bass Pro Mills Drive will encourage east-west traffic through the Weston Downs community or increase traffic on an already congested Weston Road.
- Suggestions that there is a greater need to alleviate north-south traffic, rather than east-west traffic, primarily on Weston Road.
- The desire for pedestrian and cyclist friendly routes within the study area, such as through separated bike lanes, sidewalks, multi-use paths and wider boulevards with street trees and other landscape features.
- Concerns related to the potential to impact natural environmental features or increases in noise and air pollution.
- A suggestion for a roundabout intersection at Bass Pro Mills Drive and Weston Road to help alleviate traffic congestion; and
- A lack of public transportation options available within the study area.

Please use the link on the left side of this slide to review the Online Public Information Centre 1 Summary Report and Fact Sheet.




### 1.7 Simulation of Future Transportation Options



#### Simulation of Future Transportation Options

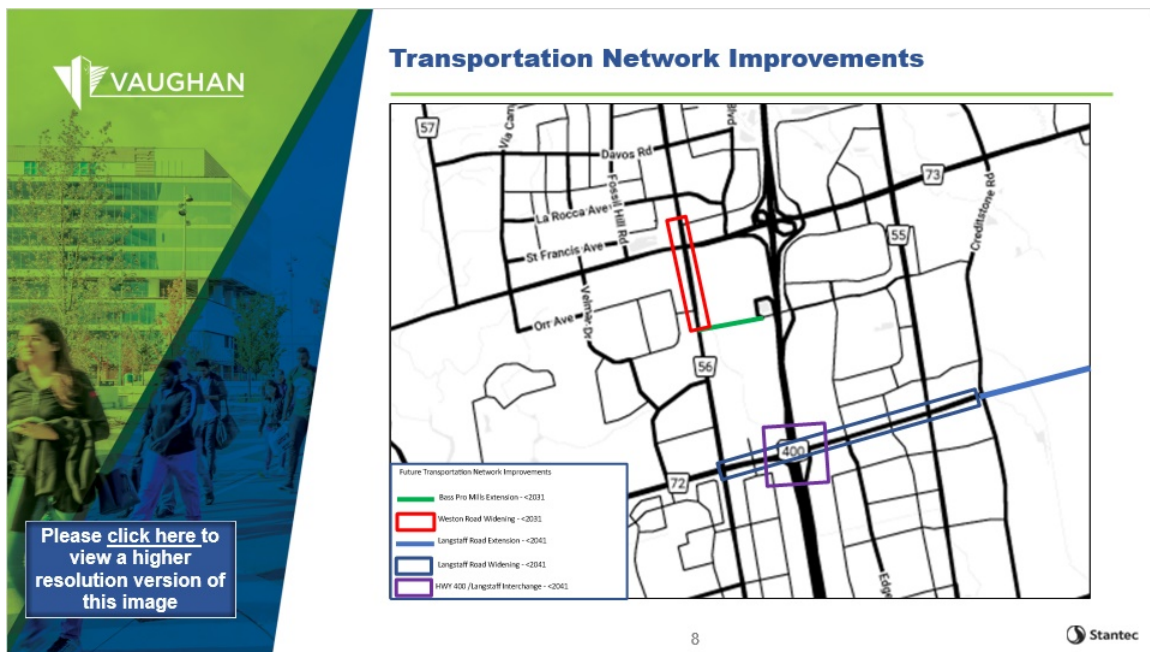
<p><b>Option 0</b></p> <ul style="list-style-type: none"> <li>• Future Base Conditions (2031 and 2041 horizons)</li> </ul>	<ul style="list-style-type: none"> <li>• 2041 horizon show impacts from Langstaff Road extension to Highway 7 and its widening between Weston Road and east of Jane Street</li> </ul>
<p><b>Option A</b></p> <ul style="list-style-type: none"> <li>• Future Conditions with Bass Pro Mills Drive extension (2031 and 2041 horizons)</li> </ul>	<ul style="list-style-type: none"> <li>• Includes Bass Pro Mills Drive Extension and Weston Road Widening north of Bass Pro Mills Extension to Hawkview Boulevard</li> </ul>
<p><b>Option B</b></p> <ul style="list-style-type: none"> <li>• Future Conditions with Bass Pro Mills Drive extension and VMCS (2031 and 2041 horizons)</li> </ul>	<ul style="list-style-type: none"> <li>• Includes 2014 Vaughan Mills Centre Secondary Plan road network and trips</li> </ul>

7 

To assess future transportation operations within the study area, three future options were developed and reviewed, including:

- Option 0 - Future Base Conditions, which assessed future traffic operations in the 2031 and 2041 horizon years. As part of this option, existing conditions were maintained; however, the approved infrastructure plan that was considered in this analysis was the Langstaff Road extension to Highway 7, and its associated widening between Weston Road east of Jane Street.
- Option A – Future Conditions with the extension of Bass Pro Mills Drive in the 2031 and 2041 horizon years. As part of this option, travel conditions with the proposed extension of Bass Pro Mills Drive, as well as the future widening of Weston Road, north of Bass Pro Mills Drive to Hawkview Boulevard, were assessed.
- Option B – Future Conditions with the extension of Bass Pro Mills Drive and full build out of the 2014 Vaughan Mills Centre Secondary Plan in the 2031 and 2041 horizon years. This option is similar to Option A, although the future Vaughan Mills Centre Secondary Plan internal road network and estimated trips generated by that proposed development were considered in the analysis.

## 1.8 Transportation Network Improvements





Several road modifications are planned in the vicinity of the study area that will enhance the transportation network's connectivity by providing additional east-west connections and additional access to and from Highway 400. The location of these future transportation network improvements are shown on this slide.

By 2031, Weston Road, between Bass Pro Mills Drive and Hawkview Boulevard, is expected to be widened to include an additional travel lane in each direction. The approximate location of this area is highlighted in red on this slide. In addition, the proposed extension of Bass Pro Mills Drive is anticipated to be constructed by 2031. The location of this proposed project is highlighted in green on this slide.

By 2041, the following roadway modifications are anticipated to be constructed:

- The extension of Langstaff Road to Highway 7 (highlighted in light blue);
- The widening of Langstaff Road, from west of Weston Road easterly to Creditstone Road (outlined in blue); and
- Additional ramps at the interchange of Langstaff and Highway 400, that will provide access from Langstaff Road to Highway 400 northbound and from Highway 400 southbound to Langstaff Road (outlined in purple).

### 1.9 Vaughan Mills Centre Secondary Plan





#### Vaughan Mills Centre Secondary Plan

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Vehicular trips associated with the 2014 Vaughan Mills Centre Secondary Plan (VMCSP) road network were modelled as part of **Option B** (Future Conditions with Bass Pro Mills Drive extension and VMCSP (2031 and 2041 horizons) in study area.

VMCSP Future Trips	AM Peak		PM Peak	
	Inbound	Outbound	Inbound	Outbound
<b>East of Highway 400</b>	852	828	1,183	1,277
<b>West of Highway 400</b>	2,839	468	996	2,970
<b>Total</b>	3,691	1,296	2,179	4,247

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As noted on Slide 7, the number of trips generated by the road network proposed as part of the 2014 Vaughan Mills Centre Secondary Plan were included in the analysis of Option B, which assessed the future traffic conditions with the extension of Bass Pro Mills Drive and implementation of the 2014 VMCSP for the 2031 and 2041 horizon years.

The future development of the Vaughan Mills Centre Secondary Plan east of Highway 400 would use the existing road network.

The internal road network conceptually outlined as part of the 2014 Vaughan Mills Centre Secondary Plan, and located west of Highway 400, was added to the road network for the analysis of Option B.

Based on the findings of the analysis, the main access points to this potential development would be at Rutherford Road and Bass Pro Mills Drive. In addition, the total vehicular trips estimated for the future Vaughan Mills Centre Secondary Plan was approximately 5,000 in the AM peak hour and 6,400 in the PM peak hour.

### 1.10 Transportation Network Assessment

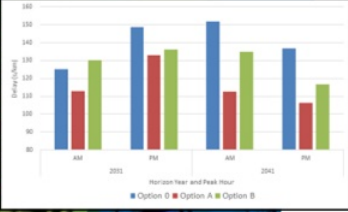


#### Transportation Network Assessment

**Transportation Analysis – Study Area Network Delay Performance**

- Transportation simulation results showed that Option A (Bass Pro Mills Drive Extension and Weston Road Widening) will result in overall reduction in delay and operational improvements in the area.
- The proposed improvements in Option A will result in approximately 10% improvement in 2031 and above 22% improvement in 2041.
- Option A provides better connectivity and greater capacity in the road network.

Horizon Year	Peak Hour	Delay Values			Delay Reduction %	
		Option 0	Option A	Option B	Option A	Option B
2031	AM	125	113	130	-10%	4%
	PM	149	133	136	-11%	-9%
2041	AM	152	113	135	-26%	-11%
	PM	137	106	117	-22%	-15%




The network-wide impacts of each option were determined as part of the traffic analysis, and the delay values shown on this slide provide the quantitative measurements of aggregate changes within the transportation network associated with each option.

The results of the analysis showed that Option A, which assumed implementation of the extension of Bass Pro Mills Drive and widening of Weston Road, will result in an overall reduction in travel delays in the area. The proposed improvements associated with Option A will also result in an over 10% improvement in operations in 2031, and over 22% in 2041.

The operational improvements forecasted for 2041 is due to the improved connectivity and capacity provided within the transportation road network.

### 1.11 Transportation Network Assessment



## Transportation Network Assessment

**Transportation Analysis – Study Area Network Delay Performance**

Intersection	Future Base 2031				Option A 2031				Option B 2031			
	AM Peak Hour		PM Peak Hour		AM Peak Hour		PM Peak Hour		AM Peak Hour		PM Peak Hour	
	Delay(s)	LOS	Delay(s)	LOS	Delay(s)	LOS	Delay(s)	LOS	Delay(s)	LOS	Delay(s)	LOS
Weston Road at Rutherford Road	118	F	133	F	113	F	140	F	136	F	123	F
Weston Road at Astona Blvd	20	C	81	F	27	C	79	E	23	C	61	E
Weston Road at Bass Pro Mills Drive	NA	NA	NA	NA	50	D	83	F	45	D	57	E
Weston Road at Greenpark Crestmount	82	F	130	F	86	F	28	C	114	F	30	C
Weston Road at Langstaff Road	112	F	165	F	89	F	114	F	137	F	151	F

Level of Service (LOS)	Delay (seconds)	Assessment
A	≤10s	Acceptable
B	>10s and ≤20s	Acceptable
C	>20s and ≤35s	Acceptable
D	>35s and ≤55s	Acceptable
E	>55s and ≤80s	Increased Delays
F	>80s	High Delay

A comparison of Level of Service (LOS) results along Weston Road shows improvements in operations while further traffic will be served by Option A (Bass Pro Mills Drive Extension and Weston Road Widening)


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A comparison of the operational Level of Service in 2031 at signalized intersections located along Weston Road is provided in this slide. The Level of Service (or LOS) is a way to measure the free flow of traffic based on the performance of signalized intersection operations, and is used to determine how well a transportation facility is operating from a traveler’s perspective and shows an average of how long it will take a vehicle from the time it reaches a signal to the time it leaves the intersection.

Level of Service is expressed in terms of traffic delays and is represented by the letters A through F. Level of Service of ‘A’ represents free-flow traffic conditions with the lowest delays, while Level of Service of ‘F’ represents a breakdown in traffic flow with stop-and-go conditions, where drivers will face the greatest extended delays. In general, a Level of Service of A, B, C or D, shown in green within the table shown on this slide, represents an acceptable Level of Service. Levels of Service E or F, shown in red on this slide, indicate when motorists will start to feel increased delays or significantly higher delays, respectively. The LOS groups A to D, colored in green, are acceptable. LOS E is when the drivers start to experience congestion and increased delays. LOS F highlighted in red indicates high delays.

A comparison between baseline and future options shows traffic congestion will improve in Option A while further traffic will be served.

### 1.12 Transportation Network Assessment



## Transportation Network Assessment

Transportation Analysis – Study Area Network Delay Performance

Intersection	Future Base 2041				Option A 2041				Option B 2041			
	AM Peak Hour		PM Peak Hour		AM Peak Hour		PM Peak Hour		AM Peak Hour		PM Peak Hour	
	Delay(s)	LOS	Delay(s)	LOS	Delay(s)	LOS	Delay(s)	LOS	Delay(s)	LOS	Delay(s)	LOS
Weston Road at Rutherford Road	163	F	125	F	103	F	100	F	148	F	105	F
Weston Road at Astona Blvd	51	D	30	C	59	E	12	B	30	C	11	B
Weston Road at Bass Pro Mills Drive	NA	NA	NA	NA	57	E	23	C	38	D	36	D
Weston Road at Greenpark/Crestmount	113	F	28	C	111	F	15	B	76	E	15	B
Weston Road at Langstaff Road	270	F	242	F	113	F	114	F	112	F	130	F


A comparison of Level of Service (LOS) results along Weston Road in 2041 shows improvements in operations while further traffic will be served by Option A (Bass Pro Mills Drive Extension and Weston Road Widening)

Level of Service (LOS)	Delay (seconds)	Assessment
A	≤10s	Acceptable
B	>10s and ≤20s	Acceptable
C	>20s and ≤35s	Acceptable
D	>35s and ≤55s	Acceptable
E	>55s and ≤80s	Increased Delays
F	>80s	High Delay

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The Level of Service expected in 2041 is shown on this slide. The preliminary results show that Option A generally provides a better Level of Service along Weston Road in relation to the three options.


### 1.13 Roundabout Screening



## Roundabout Screening

A roundabout intersection at Bass Pro Mills Drive and Weston Road was considered; however, is not recommended because:

- additional spatial and right of way requirements.
- operational challenges related to the unequal approach traffic volumes that can increase delays and queue lengths.
- pedestrian crossing and cyclist challenges.



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In response to the feedback received as part of Online Public Information Centre 1, the study team reviewed the feasibility of a roundabout intersection at Bass Pro Mills Drive and Weston Road; however, a roundabout is not recommended at this location because:

- It would require additional spatial and right of way requirements.
- Operational challenges are expected in relation to the unequal traffic volumes at the roundabout approaches, which can increase delays and queue lengths; and
- It would introduce challenges for pedestrians and cyclists when crossing the intersection.

### 1.14 Natural Environment - Background



**Legend**

- Watercourse (Permanent)
- ▭ Subject Property
- ▭ Study Area
- ▭ ELC
- ELC Code
- CVC\_1 - Business Sector
- CVC\_2 - Light Industry
- CVI\_1 - Transportation
- CVR\_3 - Single Family Residential
- MASM1-12 - Common Reed Mineral Shallow Marsh Type
- ME - Meadow
- SA - Shallow Water

#### Natural Environment - Background



**2020 Background Review**

**Terrestrial Environment**

- Lies within TRCA Regulated Area
- Shallow marsh community present
- Potential habitat for bats and bird Species at Risk (SAR)
- Candidate Significant Wildlife Habitat for some species

**Aquatic Environment**

- Black Creek permanent watercourse
- Warm thermal regime
- Assumed to provide direct fish habitat
- No aquatic SAR documented in/near this reach of Black Creek

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As part of this study, a desktop environmental review was completed in 2020 to confirm the existing conditions within the study area. Based on the findings of this review, the following was noted:

- With respect to the terrestrial environment, the study area lies within Toronto Region Conservation Authority Regulated Area. A shallow marsh community is present within the south-central portion of the study area. Potential habitat for some species at risk was noted, as well as potential candidate significant wildlife habitat for some species.
- With respect to the aquatic environment, Black Creek is present within the west portion of the study area, and is assumed to provide direct fish habitat. However, no aquatic Species at Risk were documented in/near this reach of Black Creek.

## 1.15 Natural Environment - Site Surveys



### Natural Environment – Site Surveys

#### Site Observations

- Site visits in spring and summer 2021
- No breeding birds, amphibians and/or species at risk identified

#### Headwater Drainage Features

Three features observed in the study area:

- 1) Black Creek
- 2) Wetland Vegetation Community
- 3) Connecting Channel




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To further investigate the findings of the review undertaken in 2020, the study team conducted field surveys from publicly accessible areas in spring and summer of 2021, the findings of which did not identify any breeding birds, amphibians and/or species at risk within the study area. However, three headwater drainage features were identified within the study areas including:

- The Black Creek tributary;
- A Wetland Vegetation Community; and
- A channel connect the wetland to Black Creek




### 1.16 City of Vaughan Design Criteria



**City of Vaughan Design Criteria**

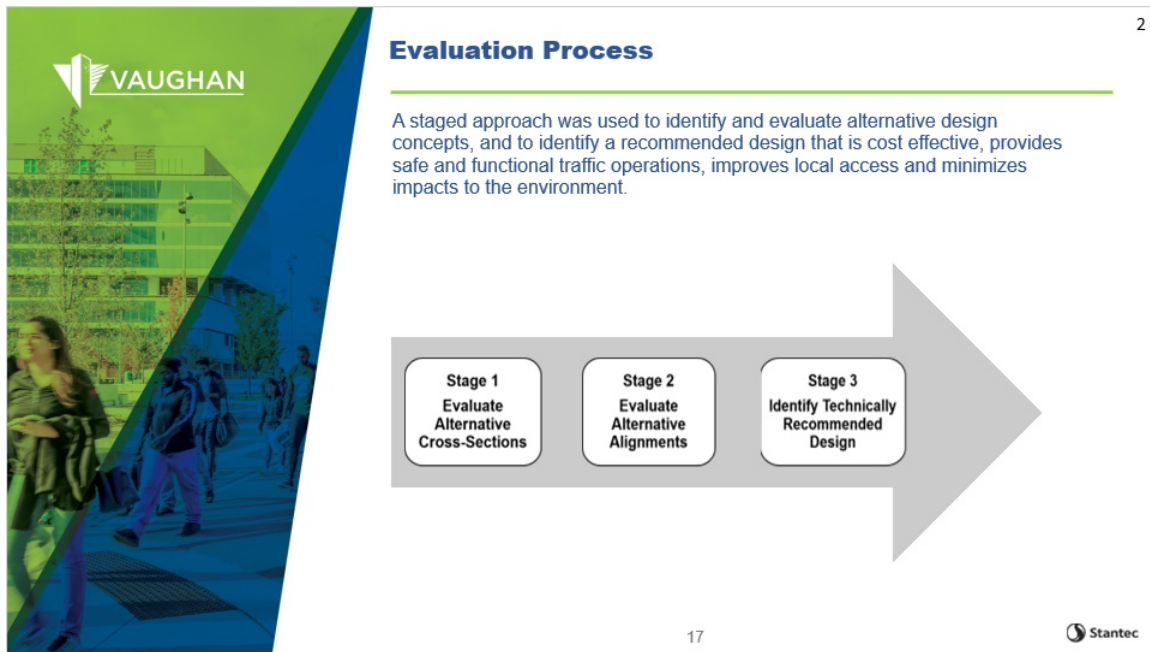
Criteria	City of Vaughan Major Collector Roadway
Design Speed	70 km/hour
Posted Speed	50 km/hour
Through Lane Width	3.3 m
Curb Lane Width	3.5 m
Buffer between Cyclists and Clearways	Minimum 0.5 m
Sidewalk Width	Minimum 1.5 metre and 2.0 metre adjacent to
Cycle Tracks	1.8 metre one-way, 3.0 metre two-way

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Design criteria for future Major Collector Roadways have been established by the City of Vaughan. These include:

- A design speed of 70 km/hr and posted speed limit of 50 km/hr
- 3.3 metre wide through lanes and 3.5 metre wide curb lanes
- A 0.5 metre wide buffer between cyclists and clearways
- A minimum 1.5 metre wide sidewalk, which is increased to a minimum 2.0 metres when the sidewalk is adjacent to the curb
- A minimum 1.8 metre wide one-way cycle track and minimum 3.0 metre wide two-way cycle track

## 1.17 Evaluation Process



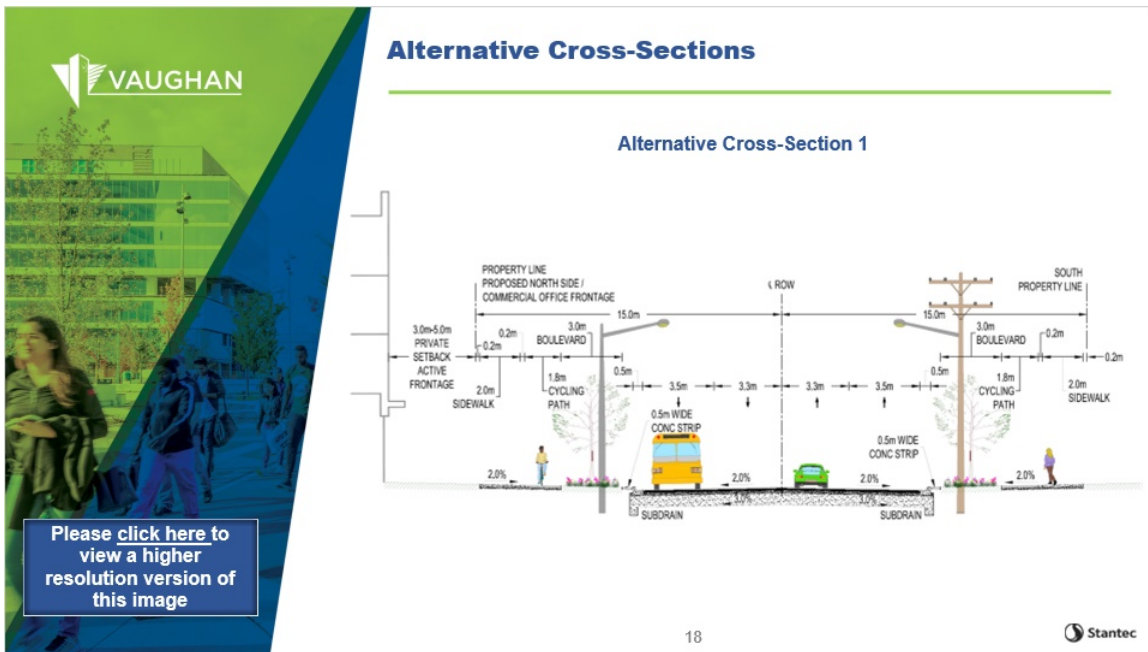
As noted earlier within this presentation, we are in Phase 3 of the Municipal Class Environmental Assessment process, where alternative designs are developed and assessed using a staged approach to identify and evaluate alternative design concepts, and to identify a technically recommended design that is cost effective, provides safe and functional traffic operations, improves local access, and minimizes impacts to the environment. This approach involved the following three stages:

- Stage 1 – Evaluate Alternative Cross-Sections
- Stage 2 – Evaluate Alternative Alignments
- Stage 3 – Identify a Technically Recommended Design

The alternatives are evaluated using a reasoned argument approach, which describes both the advantages of each alternative in response to a defined set of evaluation criteria.

Opportunities to incorporate mitigation measures to offset potential adverse impacts are also considered within this process.

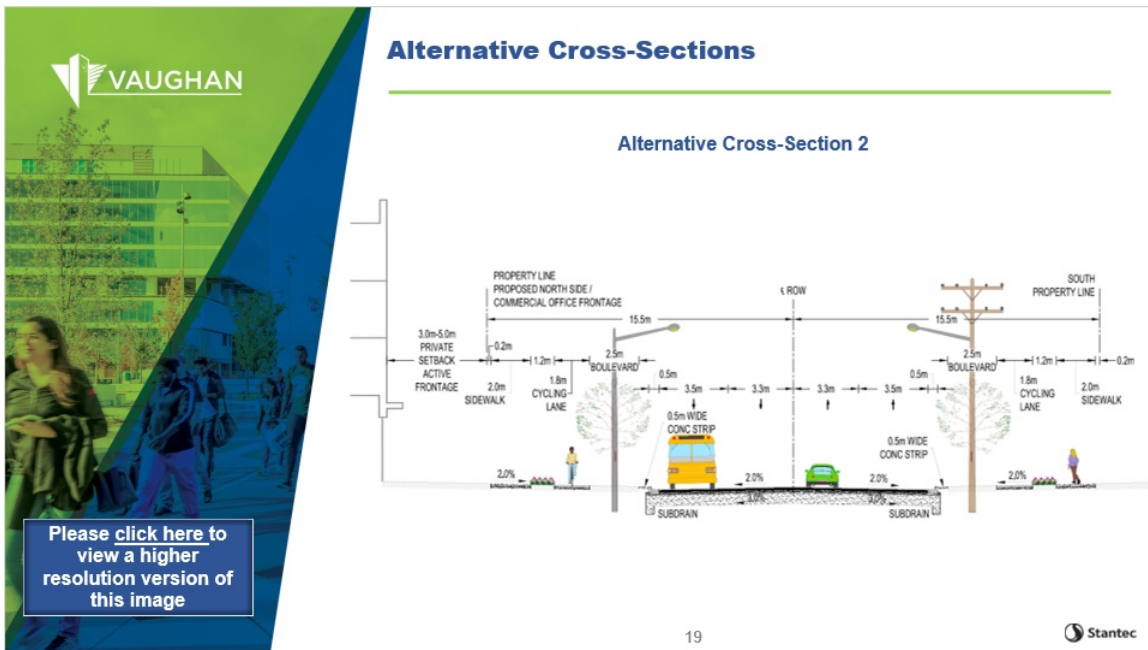
### 1.18 Alternative Cross-Sections



Four Alternative Cross-Sections were developed as part of Stage 1 of the evaluation process.

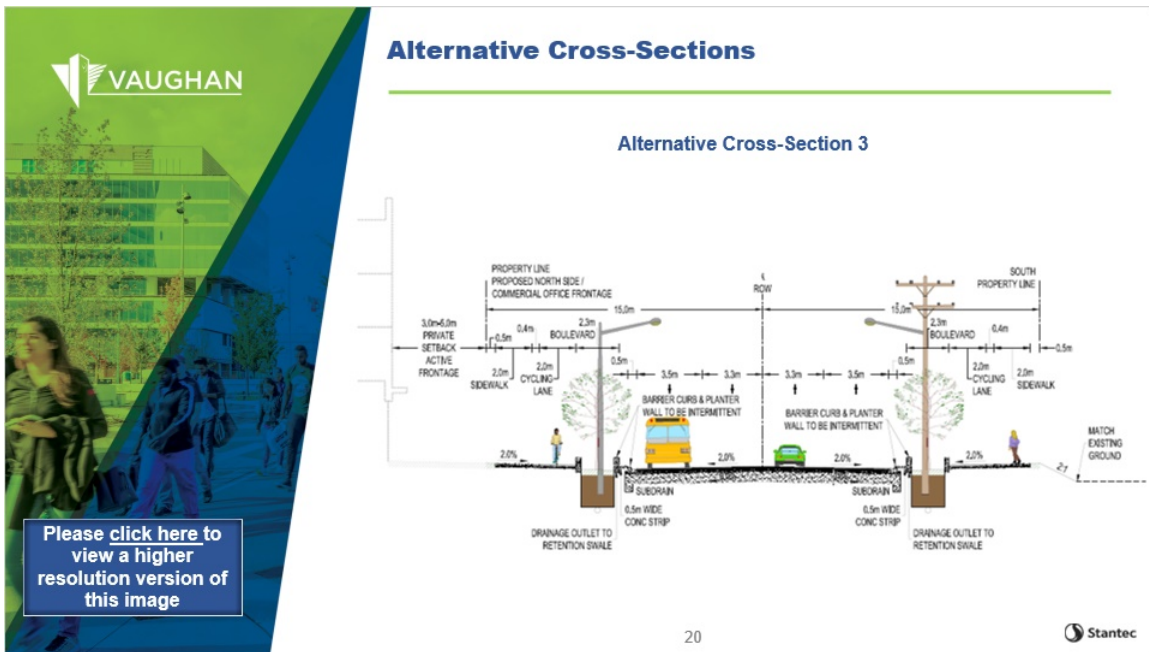
Alternative Cross-Section 1 consists of a 30 metre right-of-way, with a 2.0 metre wide sidewalk and 1.8 metre wide cycling path on either side of the roadway. While there is no formal separation between pedestrians and cyclists, cyclists are separated from vehicular traffic by a 3.0 metre boulevard, which would facilitate streetlighting, utilities, and streetscaping opportunities.

### 1.19 Alternative Cross-Sections



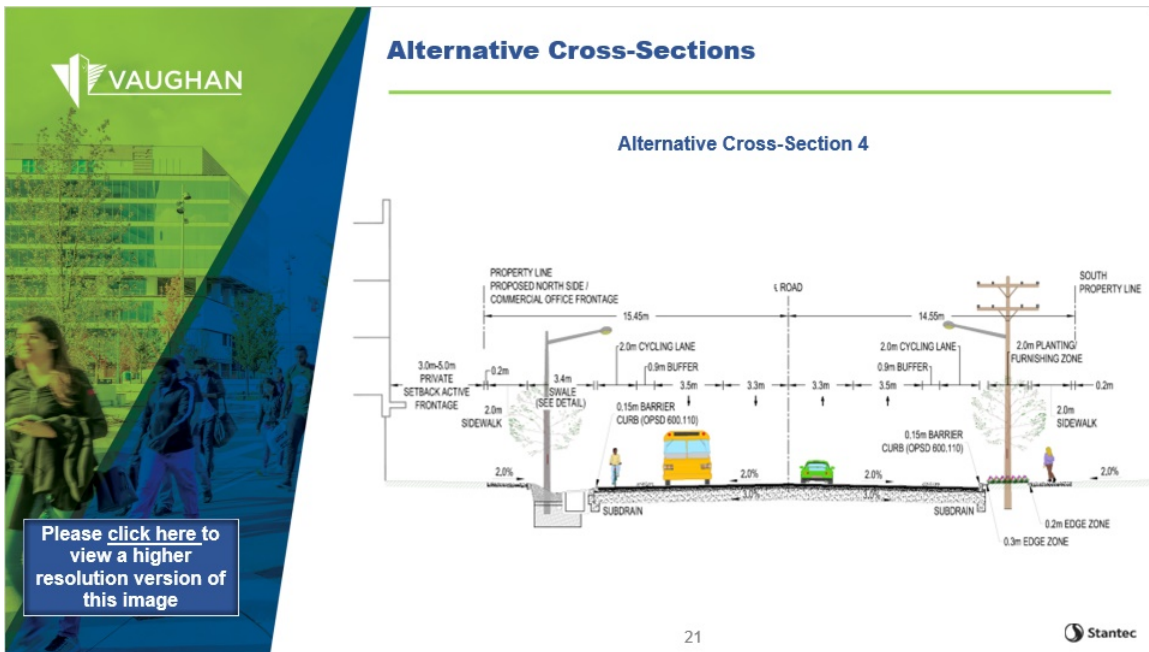
Alternative Cross-Section 2 consists of a 31 metre wide right-of way, with a 2.0 metre wide sidewalk and 1.8 metre wide cycling path on either side of the roadway. A 2.5 metre wide boulevard is provided as part of this option, and offers a 1.2 metre landscape buffer between the sidewalk and cycling path. Cyclists and pedestrians are also separated from vehicles by the 2.5 metre boulevard, which would also facilitate streetlighting, utilities, and streetscaping.

### 1.20 Alternative Cross-Sections



Alternative Cross-Section 3 consists of a 30 metre right-of-way with a 2.0 metre wide sidewalk and 2.0 metre wide cycling path on either side of the roadway. A 0.4 metre wide paved buffer is offered between the sidewalk and cycling path within a 2.3 metre boulevard. Cyclists are separated from vehicles by the 2.3 metre boulevard, which would also facilitate a bio-retention swale, streetlighting, utilities, and streetscaping.



### 1.21 Alternative Cross-Sections



Please [click here](#) to view a higher resolution version of this image

Alternative Cross-Section 4 consists of a 30 metre right-of-way with a 2.0 metre wide sidewalk on either side of the roadway. Cycling facilities are provided on either side of the roadway within 2.0 metre wide cycling lanes that are separated from vehicular traffic by a 0.9 metre wide concrete buffer. A 3.4 metre wide swale would be incorporated within the north side of the right-of-way, and 2.0 metre wide planting/furnishing zones would be provided on the south side.

### 1.22 Evaluation of Alternative Cross-Sections


Please [click here](#) to view the detailed Evaluation of Alternative Cross-Sections

#### Evaluation of Alternative Cross-Sections

Criteria Category	1	2	3	4
Technical	<span style="color: green;">●</span>	<span style="color: orange;">●</span>	<span style="color: yellow;">●</span>	<span style="color: red;">●</span>
Transportation	<span style="color: orange;">●</span>	<span style="color: green;">●</span>	<span style="color: green;">●</span>	<span style="color: red;">●</span>
Socio-Economic	<span style="color: red;">●</span>	<span style="color: yellow;">●</span>	<span style="color: green;">●</span>	<span style="color: orange;">●</span>
Natural Environment	<span style="color: orange;">●</span>	<span style="color: red;">●</span>	<span style="color: green;">●</span>	<span style="color: yellow;">●</span>
<b>Overall Conclusion</b>	Not Recommended	Not Recommended	<b>Technically Recommended Design</b>	Not Recommended

Least Preferred ● → ● → ● → ● Preferred


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The table shown on this slide presents a summary of the evaluation of the four alternative cross-sections. Based on this comparative evaluation, the four alternatives were ranked from least to most preferred in categories considering technical, transportation, socio-economic and natural environment measures. Based on the findings of this evaluation, Alternative Cross-Section 3 was recommended.

A copy of the detailed evaluation of alternative cross-sections can be accessed by clicking the link provided at the bottom of this slide.

### 1.23 Recommended Cross-Section

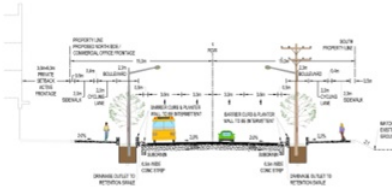


#### Recommended Cross-Section


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Alternative Cross-Section 3 was selected because it:

- is consistent with City design standards and vision for the community and public realm, as well as the recommendations of the VMCSPP.
- has a moderate capital cost and operations and maintenance costs.
- offers safe and comfortable environment for both cyclists and pedestrians.
- provides opportunities to create a pedestrian friendly space through streetlighting, wayfinding, accessible street furniture, etc.
- has high potential to accommodate municipal infrastructure, utilities and streetlighting.
- has high potential to accommodate green infrastructure through the implementation of a retention swale on both sides of the ROW.



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In summary, Cross-Section 3 is recommended because it:

- Is consistent with City of Vaughan design standards and vision for the community and public realm, as well as the recommendations of the Vaughan Mills Centre Secondary Plan.
- Has a moderate capital cost and operations and maintenance cost.
- Offers a safe and comfortable environment for both cyclists and pedestrians.
- Provides opportunities to create a pedestrian friendly space through streetlighting, wayfinding, accessible street furniture, etc.
- Has high potential to accommodate municipal infrastructure, utilities and streetlighting.
- Has high potential to accommodate green infrastructure through the implementation of a retention swale on both sides of the right-of-way.



## 1.24 Alternative Alignments

**Alternative Alignments**

**Alignment Alternative 'A'**

**Alignment Alternative 'B'**

Please [click here](#) to view higher resolution version of the Alternative Alignments

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Stantec


Once the recommended cross-section design and width were confirmed, the study team proceeded to Stage 2 of the evaluation process; evaluate alternative alignments.

Two alternative alignments were subjected to a detailed evaluation as part of this study.

Alignment Alternative A generally provides a straight and direct connection from the existing terminus of Bass Pro Mills Drive westerly to Weston Road, and Alignment Alternative B traverses offers a connection through the study area from the existing terminus of Bass Pro Mills Drive northwesterly to Weston Road, opposite Astona Boulevard.

A higher resolution copy of the Alternative Alignments can be viewed by clicking the link provided at the bottom of this slide.

### 1.25 Evaluation of Alternative Alignments




**Evaluation of Alternative Alignments**

Criteria Category	Alternative A (Straight Connection to Weston Rd)	Alternative B (Astona Blvd Connection)
<b>Technical</b>	Preferred	Least Preferred
<b>Cultural Heritage</b>	Preferred	Least Preferred
<b>Socio-Economic</b>	Preferred	Least Preferred
<b>Natural Environment</b>	Preferred	Least Preferred
<b>Overall Conclusion</b>	<b>Recommended</b>	Not Recommended

**Alignment Alternative A** is recommended because:

- Avoids direct connection to adjacent residential areas, less traffic infiltration anticipated
- Aligns with City's vision set forth in VMCSPP
- Lower potential environmental impacts (archaeological, natural heritage)
- Lower anticipated capital, operations and maintenance costs
- Less impacts to private property

Please click here to view the detailed Evaluation of Alternative Alignments

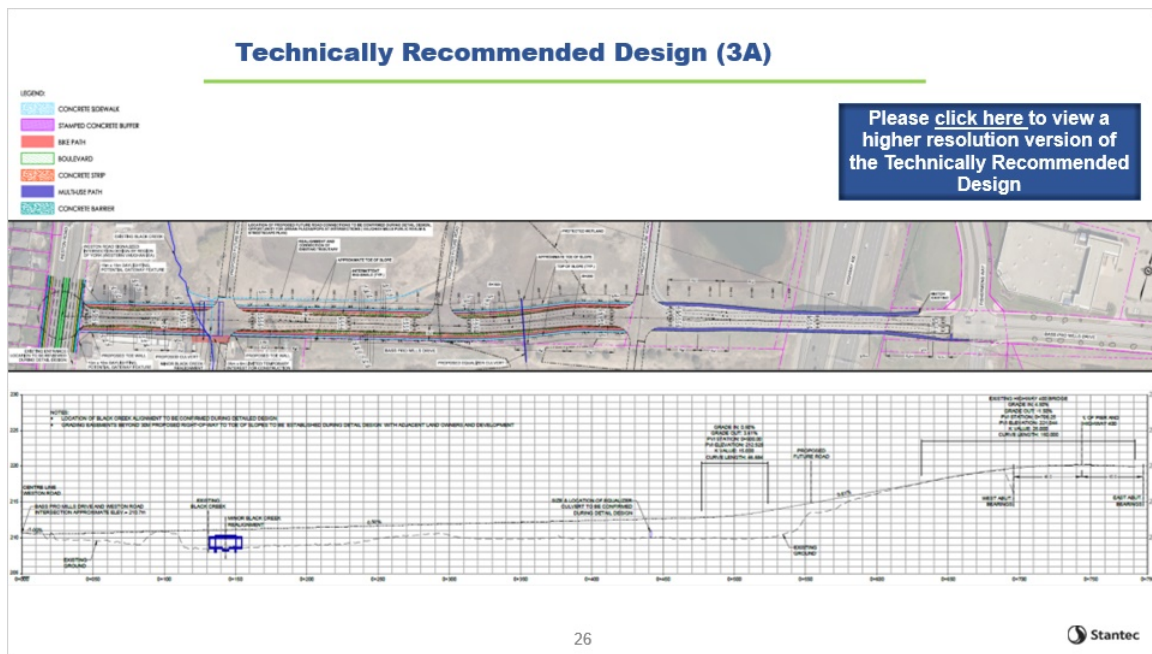
25 

The table shown on this slide presents a summary of the comparative evaluation of the two alternative alignments. Based on the findings of this evaluation, Alignment A is recommended because it:

- Avoids a direct connection to adjacent residential areas;
- Aligns with the City of Vaughan' vision set forth within the Vaughan Mills Centre Secondary Plan;
- Has a lower potential to impact the environment;
- Has a lower anticipated capital, operations and maintenance costs; and
- Impacts less private property.

A copy of the detailed evaluation of alternative alignments can be viewed by clicking the link provided at the bottom of this slide.

### 1.26 Technically Recommended Design (3A)



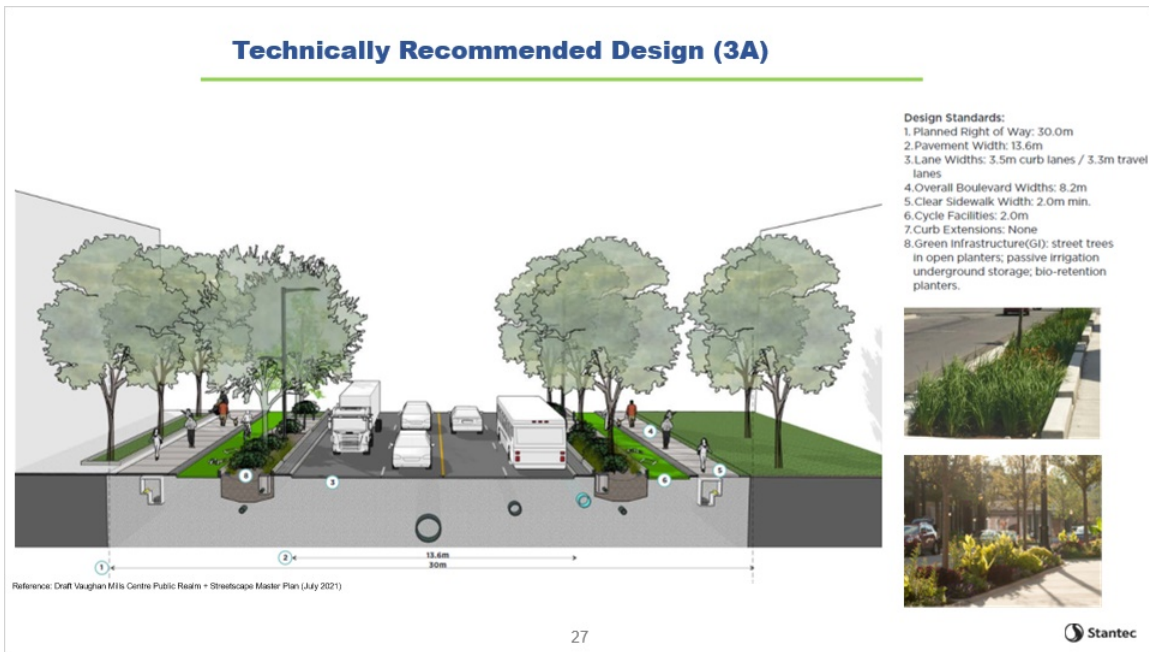
Stage 3 of the evaluation process includes the identification of the Technically Recommended Design. As shown on this slide, the Technically Recommended Design includes Alternative Cross-Section 3 combined with Alternative Alignment A.

The Plan and Profile for Technically Recommended Design 3A includes:

- A 30 metre right-of-way, including 4 lanes of vehicular travel, with 3 lanes in each direction
- A 2.0 metre wide sidewalk and 2.0 metre wide cycling path on either side of the roadway.
- A 0.4 metre wide paved buffer between the sidewalk and cycling path
- A 2.3 metre boulevard with an intermittent bio-retention swale, streetlighting, utilities, and streetscaping
- A potential gateway feature located at the proposed new roadway intersection with Weston Road; and
- A new culvert crossing Black Creek.

A higher resolution version of this Plan and Profile drawing can be accessed by clicking the link provided at the top of this slide.

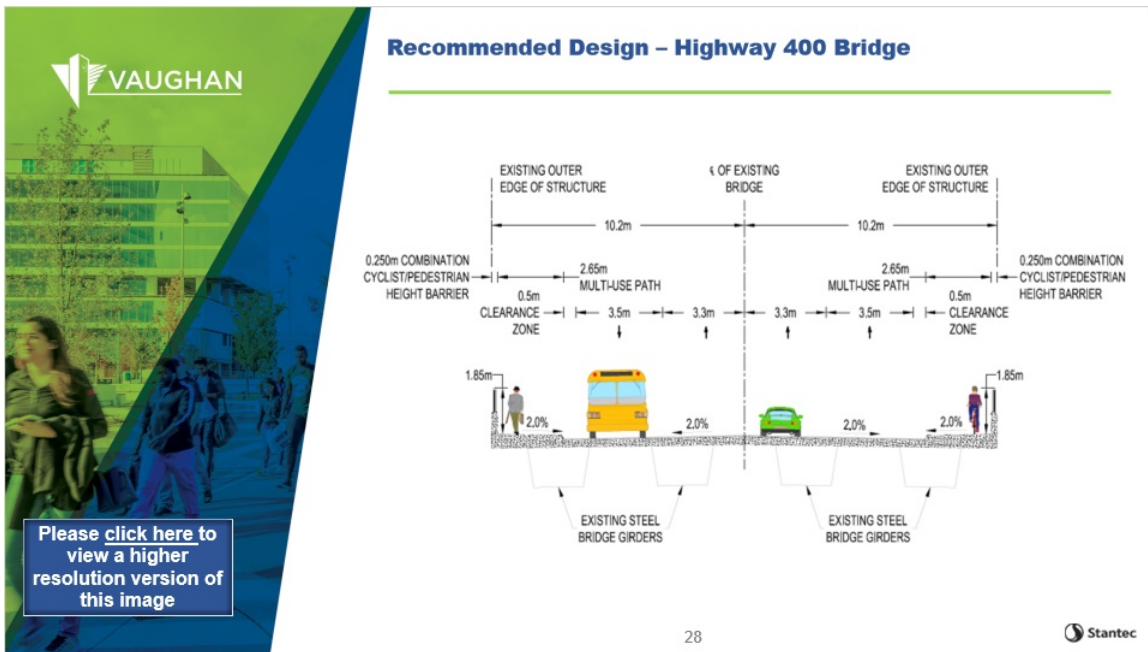
### 1.27 Technically Recommended Design (3A)



The 3D rendering on this slide was developed as part of the ongoing Vaughan Mills Centre Public Realm and Streetscape Master Plan Study and provides an illustration of what the future extension of Bass Pro Mills Drive cross-section could look like.

Photographs of other intermittent bio-retention swales are also provided on the lower right portion of this slide.

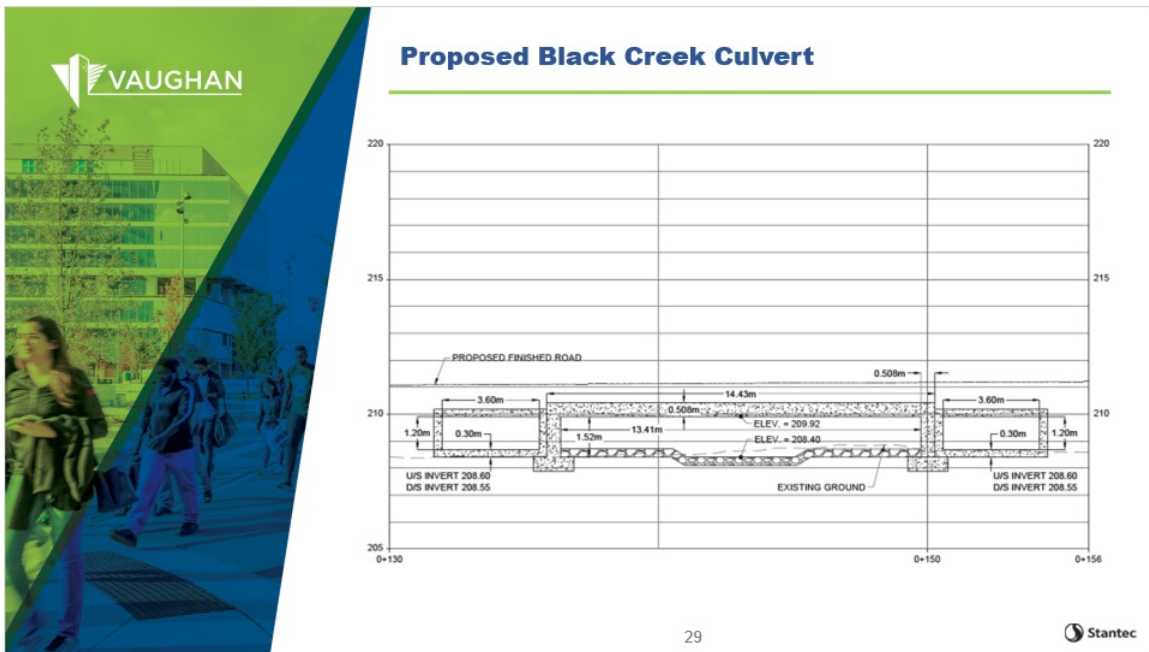
### 1.28 Recommended Design - Highway 400 Bridge



As part of the Technically Recommended Design, the existing Highway 400 bridge would be reconfigured to include multi-use paths on either side of the roadway to allow for active transportation users to travel east-west across the structure, and to connect to the existing facilities on the east side of the structure, and the new active transportation facilities provided within the new roadway.

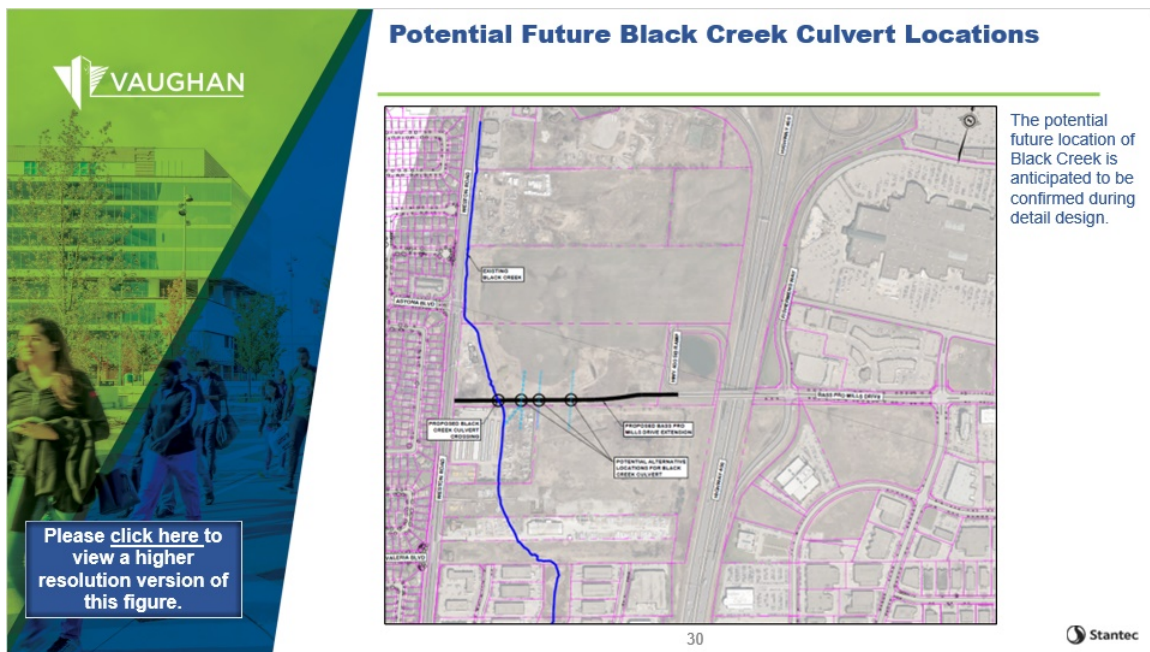
A higher resolution version of this image can be accessed by clicking the link provided at the bottom of this slide.

### 1.29 Proposed Black Creek Culvert



A new crossing of Black Creek is required to accommodate the new roadway extension. A new 13.41 metre by 1.52 metre trispan concrete culvert is proposed to span the creek. In addition, two, 3.6 metre by 1.2 metre box culverts are proposed to assist in potentially accommodating any increases in flows or storm events.

### 1.30 Potential Future Black Creek Culvert Locations

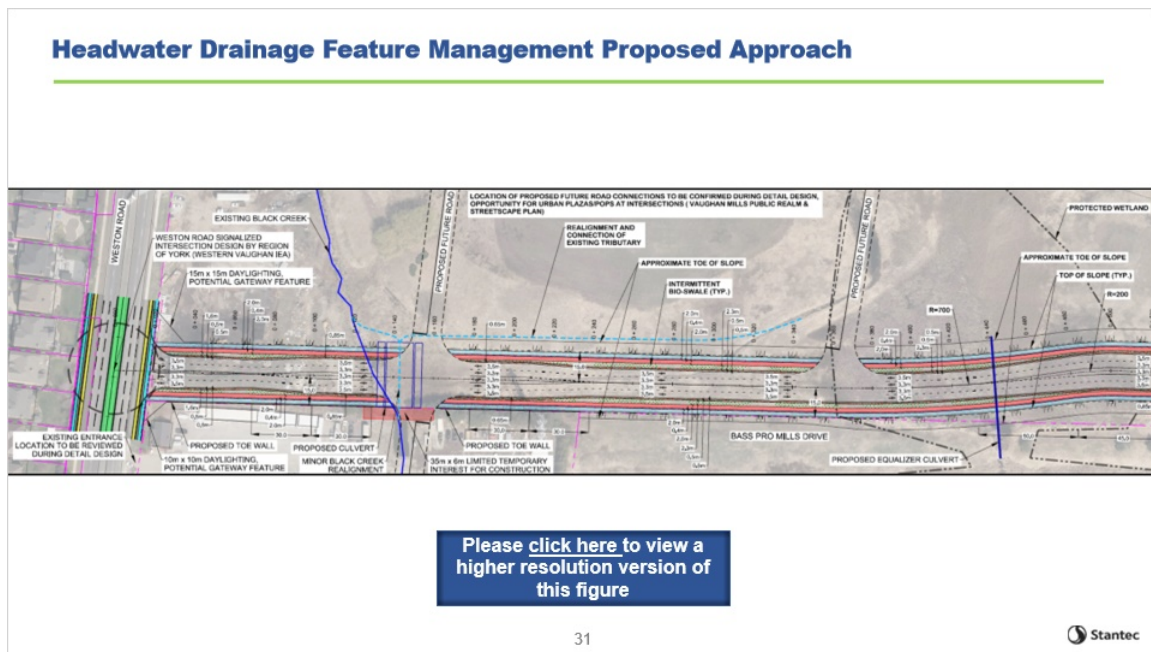


The Vaughan Mills Centre Secondary Plan was completed in 2014; however, the future configuration of local roadways and distribution of land uses within the planning area has not been confirmed. It is understood that the future realignment of Black Creek may also be required as part of future development planning in this area.

At this time, the new crossing of Black Creek is proposed at the existing creek location; however, depending on the future land use and roadway configurations in this area, potential future locations of the new crossing are also shown on this slide. It should be noted that the proposed new culvert designed as part of this undertaking can be applied at any of these potential future locations.

A higher resolution version of this figure can be accessed by clicking the link provided on the left portion of this slide.

### 1.31 Headwater Drainage Feature Management Proposed Approach



As noted on Slide 15 of this presentation, there are three headwater drainage features within the study area, each of which will be temporarily impacted by construction of the new roadway. These include: 1) the reconfiguration of Black Creek; 2) removal of a portion of the wetland vegetation community; and 3) reconfiguration of the existing channel between Black Creek and the wetland.


To help mitigate these impacts, the following is proposed:

- Black Creek will be modified to accommodate the new roadway and crossing structure. A natural channel design with riparian vegetation features will be established and will connect to the existing creek to the immediate north and south of the new roadway.
- An equalization culvert will be constructed beneath the new roadway to maintain flows associated with the existing wetland.
- The existing connection channel will be reconfigured along the north side of the new road wall right-of-way, and will maintain the connection between the wetland and Black Creek.

A higher resolution version of this figure can be accessed by clicking the link provided at the bottom of this slide.




### 1.32 Environmental Impacts & Mitigation



#### Environmental Impacts & Mitigation

Potential Impact	Preliminary Proposed Mitigation Measures and Commitments
<b>Aquatic Environment</b>	<ul style="list-style-type: none"> <li>Black Creek assumed to provide direct fish habitat. New crossing of Black Creek will be designed to accommodate meandering channel design with vegetated buffer to north and south.</li> </ul>
<b>Trees/Vegetation</b>	<ul style="list-style-type: none"> <li>Vegetation and tree removal will be minimized to the extent possible. New streetscape features (grass, trees, vegetation) will be implemented as part of preferred design.</li> </ul>
<b>Wildlife/Habitat/Wetland</b>	<ul style="list-style-type: none"> <li>Breeding bird surveys to confirm presence/absence of breeding birds and OWES Wetland Evaluation to delineate wetland boundaries and confirm absence of amphibians.</li> <li>Wetland connection to Black Creek will be maintained via drainage ditch situated along north side of new right-of-way.</li> <li>Wetland connection to south will be maintained via equalization culvert.</li> </ul>
<b>Archaeology</b>	<ul style="list-style-type: none"> <li>Stage 1 Archaeological Assessment (AA) identified the potential for the recovery of archaeological resources.</li> <li>Stage 2 AA will be undertaken during detail design. No construction activities will take place until the Ministry of Sport, Heritage, Tourism and Culture Industries have confirmed in writing that all archaeological licensing and technical review requirements have been satisfied.</li> </ul>
<b>Property</b>	<ul style="list-style-type: none"> <li>Impacts to some private property has been identified in association with the Technically Recommended Design.</li> <li>Property impacts will be minimized to the extent possible during detail design, in consultation with affected property owners.</li> </ul>
<b>Noise</b>	<ul style="list-style-type: none"> <li>A Noise Assessment is being completed in accordance with Provincial guidelines to determine if measures are required to mitigate potential increases in traffic noise.</li> </ul>
<b>Air Quality</b>	<ul style="list-style-type: none"> <li>An Air Quality Assessment is being completed in accordance with Provincial guidelines to assess the potential changes in local and regional air quality, and to determine mitigation measures as required.</li> </ul>

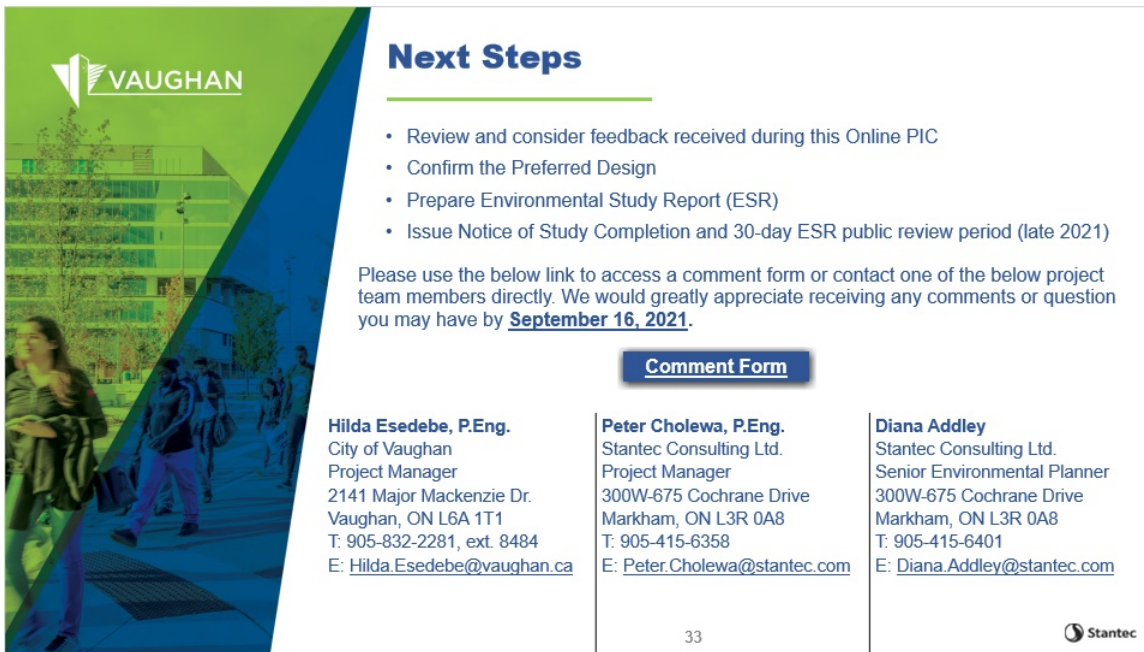
32



The Technically Recommended Design (3A) has the potential to impact the natural and socio-economic environments. A summary of these potential impacts and preliminary proposed mitigation measures and associated commitments is provided on this slide.

More detailed information concerning the potential impacts and proposed mitigation measures associated with this project will be documented within the Environmental Study Report.

### 1.33 Next Steps



**Next Steps**


- Review and consider feedback received during this Online PIC
- Confirm the Preferred Design
- Prepare Environmental Study Report (ESR)
- Issue Notice of Study Completion and 30-day ESR public review period (late 2021)

Please use the below link to access a comment form or contact one of the below project team members directly. We would greatly appreciate receiving any comments or question you may have by **September 16, 2021**.

[Comment Form](#)

<p><b>Hilda Esedebe, P.Eng.</b>                  City of Vaughan                  Project Manager                  2141 Major Mackenzie Dr.                  Vaughan, ON L6A 1T1                  T: 905-832-2281, ext. 8484                  E: <a href="mailto:Hilda.Esedebe@vaughan.ca">Hilda.Esedebe@vaughan.ca</a></p>	<p><b>Peter Cholewa, P.Eng.</b>                  Stantec Consulting Ltd.                  Project Manager                  300W-675 Cochrane Drive                  Markham, ON L3R 0A8                  T: 905-415-6358                  E: <a href="mailto:Peter.Cholewa@stantec.com">Peter.Cholewa@stantec.com</a></p>	<p><b>Diana Addley</b>                  Stantec Consulting Ltd.                  Senior Environmental Planner                  300W-675 Cochrane Drive                  Markham, ON L3R 0A8                  T: 905-415-6401                  E: <a href="mailto:Diana.Addley@stantec.com">Diana.Addley@stantec.com</a></p>
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33



Following this online Public Information Centre, the project team will review and consider the feedback received, and will respond to any questions you may have.

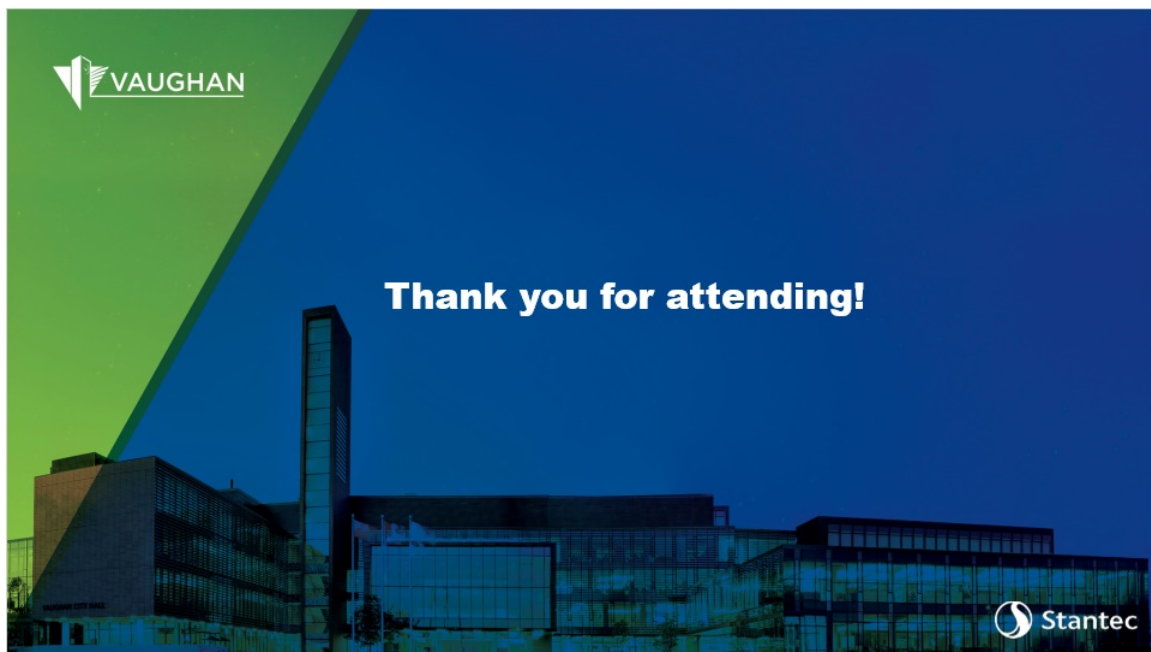
In consideration of the comments received, the project team will confirm the Preferred Design for this project, including the potential impacts and proposed mitigation measures. The Environmental Study Report (or ESR) will be prepared, and a Notice of Study Completion will be issued, at which time the ESR will be available for public review on the project website for a 30-day period.

Your input is important to us, and there are several ways to provide your comments. Please take the time to review all of the study information that is available on the study website and complete the comment form, which can be access by clicking the link provided on this slide.

If you have any comments or questions, please do not hesitate to contact the study team members listed on this slide by email, telephone or mail.

We would appreciate receiving any comments or questions that you may have by Thursday, September 16, 2021.

### ***1.34 Thank you for attending!***



On behalf of the City of Vaughan, we would like to thank you for your interest in this study and for taking the time to participate in this online Public Information Centre. We would appreciate receiving any comments or questions that you may have by September 16, 2021.

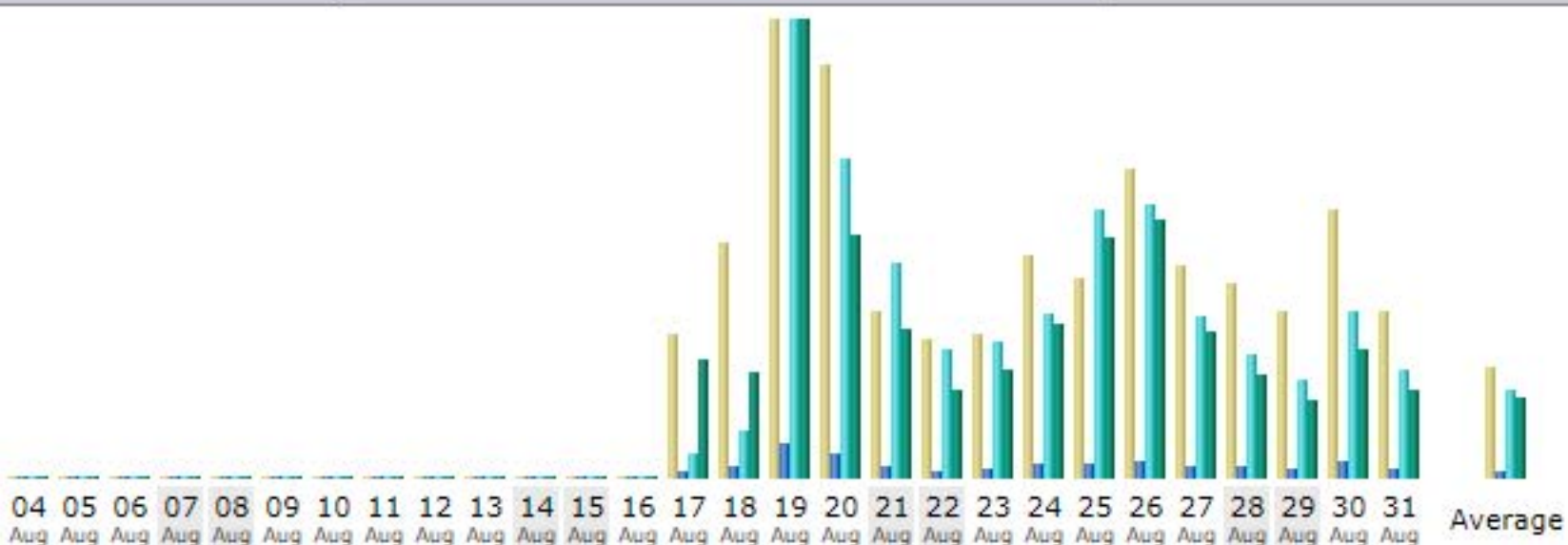
Thank you once again for taking the time to participate in this online Public Information Centre.

# Appendix C

## Online PIC 2 Visitor Statistics (Raw Data)

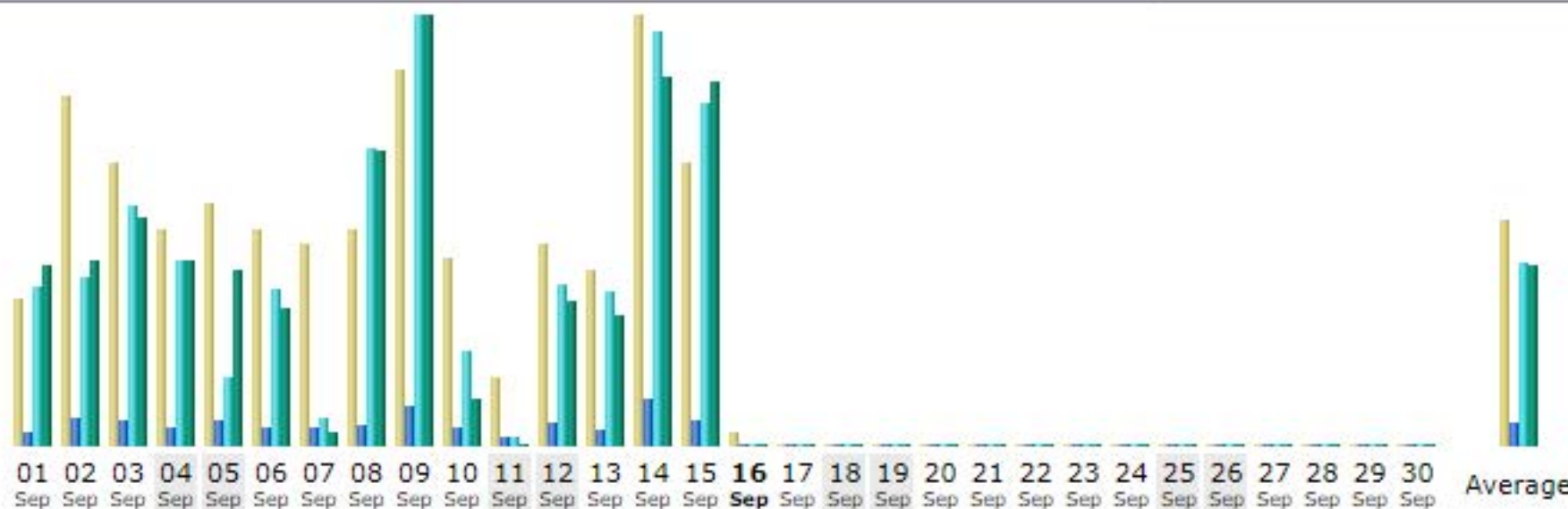


## Days of month



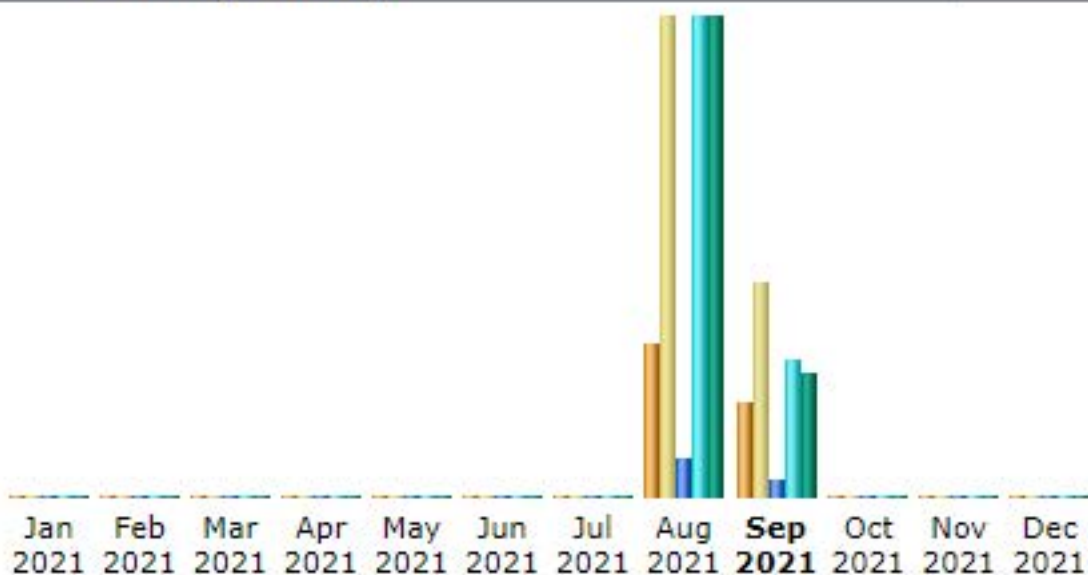
Day	Number of visits	Pages	Hits	Bandwidth
01 Aug 2021	0	0	0	0
02 Aug 2021	0	0	0	0
03 Aug 2021	0	0	0	0
04 Aug 2021	0	0	0	0
05 Aug 2021	0	0	0	0
06 Aug 2021	0	0	0	0
07 Aug 2021	0	0	0	0
08 Aug 2021	0	0	0	0
09 Aug 2021	0	0	0	0
10 Aug 2021	0	0	0	0
11 Aug 2021	0	0	0	0
12 Aug 2021	0	0	0	0
13 Aug 2021	0	0	0	0
14 Aug 2021	0	0	0	0
15 Aug 2021	0	0	0	0
16 Aug 2021	0	0	0	0
17 Aug 2021	25	25	86	181.56 MB
18 Aug 2021	41	41	172	162.32 MB
19 Aug 2021	80	123	1,642	699.36 MB
20 Aug 2021	72	87	1,143	369.44 MB
21 Aug 2021	29	44	771	227.34 MB
22 Aug 2021	24	27	457	135.78 MB
23 Aug 2021	25	34	487	164.84 MB
24 Aug 2021	39	51	587	233.81 MB
25 Aug 2021	35	53	965	367.61 MB
26 Aug 2021	54	63	982	394.87 MB
27 Aug 2021	37	41	575	221.91 MB
28 Aug 2021	34	38	439	158.89 MB
29 Aug 2021	29	36	352	119.16 MB
30 Aug 2021	47	55	596	198.05 MB
31 Aug 2021	29	34	385	135.15 MB
Average	19	24	310	121.62 MB
Total	600	752	9,639	3.68 GB

## Days of month



Day	Number of visits	Pages	Hits	Bandwidth
01 Sep 2021	11	13	151	61.05 MB
02 Sep 2021	26	27	160	62.26 MB
03 Sep 2021	21	25	230	76.73 MB
04 Sep 2021	16	17	177	62.06 MB
05 Sep 2021	18	24	66	59.23 MB
06 Sep 2021	16	18	149	45.98 MB
07 Sep 2021	15	17	26	4.16 MB
08 Sep 2021	16	20	284	99.49 MB
09 Sep 2021	28	38	410	144.89 MB
10 Sep 2021	14	17	91	15.52 MB
11 Sep 2021	5	7	7	44.24 KB
12 Sep 2021	15	22	154	48.90 MB
13 Sep 2021	13	15	148	44.24 MB
14 Sep 2021	32	44	396	124.48 MB
15 Sep 2021	21	23	326	122.61 MB
<b>16 Sep 2021</b>	1	1	1	6.32 KB
17 Sep 2021	0	0	0	0
18 Sep 2021	0	0	0	0
19 Sep 2021	0	0	0	0
20 Sep 2021	0	0	0	0
21 Sep 2021	0	0	0	0
22 Sep 2021	0	0	0	0
23 Sep 2021	0	0	0	0
24 Sep 2021	0	0	0	0
25 Sep 2021	0	0	0	0
26 Sep 2021	0	0	0	0
27 Sep 2021	0	0	0	0
28 Sep 2021	0	0	0	0
29 Sep 2021	0	0	0	0
30 Sep 2021	0	0	0	0
Average	16	20	173	60.73 MB
Total	268	328	2,776	971.64 MB

## Monthly history



Month	Unique visitors	Number of visits	Pages	Hits	Bandwidth
Jan 2021	0	0	0	0	0
Feb 2021	0	0	0	0	0
Mar 2021	0	0	0	0	0
Apr 2021	0	0	0	0	0
May 2021	0	0	0	0	0
Jun 2021	0	0	0	0	0
Jul 2021	0	0	0	0	0
Aug 2021	192	600	752	9,639	3.68 GB
<b>Sep 2021</b>	117	268	328	2,776	971.64 MB
Oct 2021	0	0	0	0	0
Nov 2021	0	0	0	0	0
Dec 2021	0	0	0	0	0
Total	309	868	1,080	12,415	4.63 GB

# Appendix D

## Online PIC 2 Survey Template









# Appendix E

## Online PIC 2 Comments & Survey Responses



# #1

COMPLETE

**Collector:** Web Link 1 (Web Link)  
**Started:** Tuesday, August 24, 2021 4:09:44 PM  
**Last Modified:** Tuesday, August 24, 2021 4:31:28 PM  
**Time Spent:** 00:21:44  
**IP Address:** 165.225.209.31

---

Page 1

## Q1

Do you have any comments regarding the evaluation of alternative cross-sections (Slides 17 – 23) and alternative alignments (Slides 24 – 25)? If yes, please describe.

There is no commentary on how the City intends to protect the Weston Downs Community from vehicular traffic that will choose to use Weston Downs as a westbound thorough fare. Weston Downs has a long discussed congestion issue that the City has been unable to address.

Congestion levels have materially increased since 2014. Why is that the relevant starting point? We need data that represents current development and traffic patterns. Using 2014 is miscasting the current severity and reality of the congestion issue.

---

## Q2

Do you have any comments regarding the Technically Recommended Design (Alternative 3A) (Slide 26)? If yes, please describe.

Again, why deposit cars onto Weston Road immediately south of Astona. What measures will be employed to prevent these vehicles from accessing Weston Downs at Astona.

I don't see any form of solution that addresses the need to get vehicles mode efficiently onto the 400 to help access other major east-west thorough fares. Ultimately, these vehicles need an east-west thorough fare. Why not make Bass Pro Mills a full 400 interchange so that commuters can ultimately access Major Mackenzie, Kirby and other roadways more efficiently.

---

## Q3

Did you like the way the information has been shared through this online public information centre? Is there anything we could improve on? If yes, please describe.

I applaud the website. I would confess that my visibility on this matter has been very limited until receipt the PIC via mail. My initial reaction is one of surprise as I have had no visibility on the matter. I suspect the pandemic and other matters may be part of the distraction but I believe it speaks to a lack of awareness in my community.

---

**Q4**

Do you have any additional comments and/or suggestions you would like to share? If yes, please describe.

A solution to manage Weston Downs traffic needs to be embedded in any solution that risks depositing more vehicles on Weston Road.

---

**Q5**

Please provide your name and email in the fields below (optional).

Name:

[REDACTED]

Mailing Address:

[REDACTED]

Telephone:

[REDACTED]

Email:

[REDACTED]

---

## #2

COMPLETE

**Collector:** Web Link 1 (Web Link)  
**Started:** Thursday, August 26, 2021 11:03:28 AM  
**Last Modified:** Thursday, August 26, 2021 11:18:24 AM  
**Time Spent:** 00:14:55  
**IP Address:** 99.247.173.3

---

Page 1

### Q1

Do you have any comments regarding the evaluation of alternative cross-sections (Slides 17 – 23) and alternative alignments (Slides 24 – 25)? If yes, please describe.

I really like the whole process and various thought out alternatives for 24-25. It was a great job that was done. The recommended alternatives for both the road ie. making bicycle lanes and pedestrian lanes very appealing and the alternative alignment slide 25 conclusion to go straight to Weston as compared to aligning to Astona were well researched and thought out.

---

### Q2

Do you have any comments regarding the Technically Recommended Design (Alternative 3A) (Slide 26)? If yes, please describe.

I agree with the Technically Recommended Design noted on slide 26 (alternative 3A).

---

### Q3

Did you like the way the information has been shared through this online public information centre? Is there anything we could improve on? If yes, please describe.

Yes, the online presentation was very good. I appreciate all of the contact info at the end of the presentation as well. It's very important to keep the lines of communication open.

---

### Q4

Respondent skipped this question

Do you have any additional comments and/or suggestions you would like to share? If yes, please describe.

---

### Q5

Please provide your name and email in the fields below (optional).

Name:

Mailing Address:

Telephone:

Email:

---

**Robinson, Jennifer**

---

**To:** Hilda Esedebe  
**Subject:** RE: [External] Vaughan/Brass Pro Mills EA

-----Original Message-----

From: Hilda Esedebe  
Sent: Wednesday, August 11, 2021 12:35 PM  
To: [REDACTED]  
Subject: RE: [External] Vaughan/Brass Pro Mills EA

Will do. Thank you for your interest in the study.

Regards,

Hilda Esedebe, P.Eng., MBA, M.Sc.  
Transportation Project Manager  
Infrastructure Planning and Corporate Asset Management 905-832-8585, ext. 8484 | hilda.esedebe@vaughan.ca

City of Vaughan | Infrastructure Development  
2141 Major Mackenzie Dr., Vaughan, ON L6A 1T1 vaughan.ca

-----Original Message-----

From: [REDACTED]  
Sent: Wednesday, August 11, 2021 8:53 AM  
To: Hilda Esedebe <Hilda.Esedebe@vaughan.ca>  
Subject: [External] Vaughan/Brass Pro Mills EA

Hi Hilda,  
Please add me to the study mailing list for the Bass Pro Mills Drive extension to Weston Road.

Thank you

[REDACTED]

Sent from my iPhone

This e-mail, including any attachment(s), may be confidential and is intended solely for the attention and information of the named addressee(s). If you are not the intended recipient or have received this message in error, please notify me immediately by return e-mail and permanently delete the original transmission from your computer, including any attachment(s). Any unauthorized distribution, disclosure or copying of this message and attachment(s) by anyone other than the recipient is strictly prohibited.

## Robinson, Jennifer

---

**From:** Hilda Esedebe <Hilda.Esedebe@vaughan.ca>  
**Sent:** Tuesday, August 17, 2021 4:47 PM  
**To:** [REDACTED]  
**Subject:** Bass Pro Mills EA - Call from Resident  
**Attachments:** VoiceMsg[ID=20000000256822 G=40 F=300 A=5601654A-98A5-4961-9A00-E5EE2E282E28 C=1 CID=[REDACTED]].mp3

Hi [REDACTED],

Thank you for reaching out to the City today in response to the Notice of the Bass Pro Mills Drive Extension Environmental Assessment (EA) Study Online Public Information Center (PIC) 2. We appreciate feedback from the public and your comments have been noted.

As promised, please use the links below to access the Vaughan Mills Center Urban Design/Streetscape Plan (draft 2021) and the Vaughan Mills Center Secondary Plan (2014) which give you a sense of plans for the area. If you have further questions on these, I could connect you to Urban Design and Policy Planning staff at the City:

- [https://www.vaughan.ca/projects/policy\\_planning\\_projects/VMC-Public-Realm-and-Streetscape-Plan/Pages/default.aspx](https://www.vaughan.ca/projects/policy_planning_projects/VMC-Public-Realm-and-Streetscape-Plan/Pages/default.aspx)
- [https://www.vaughan.ca/projects/policy\\_planning\\_projects/vgnmills\\_centre\\_secondary/General%20Documents/VMCSP%20Sept19-2014.pdf](https://www.vaughan.ca/projects/policy_planning_projects/vgnmills_centre_secondary/General%20Documents/VMCSP%20Sept19-2014.pdf)

The Project Team for the Bass Pro Mills Drive Environmental Assessment (EA) Study is certainly aware of your concerns regarding traffic infiltration into the Weston Downs area and we have met with representatives of the Weston Downs Rate Payers Association (WDRA) a number of times. We have also received feedback during our first online Public Information Center (PIC), which ran from December 3, 2020 to January 8, 2021, and the Project Team continues to review feedback from all stakeholders, along with the outcomes of technical analysis, to inform this study to the best extent possible.

The City of Vaughan is growing and infrastructure for vehicular traffic, transit and active transportation needs to be in place to support this growth. The City and Region have a number of ongoing studies aimed at growth and improving Vaughan's connectivity including this [Bass Pro Mills EA](#), [Kirby Road missing link](#), [Teston Road missing link](#), [Langstaff Road EA](#) to mention a few. York Region also has plans to widen Rutherford Road and Weston Road by 2041 (see the [Transportation Master Plan | York Region](#)). Although Weston Road and Rutherford Road fall under York Region's jurisdiction, the City works with all tiers of government to preserve the interest of citizens as best as possible, while reviewing multiple solutions to address growth and transportation needs. Regarding Regional projects you may email [roads@york.ca](mailto:roads@york.ca) or visit the study websites for more information.

As you know, the next Online PIC for the Bass Pro Mills EA Study that has been arranged to present and solicit feedback on the outcomes of PIC 1, the traffic analysis and environmental investigations, the evaluation of alternative alignments and cross-sections, the Technically Recommended Design and the next steps in the study process. As part of this online PIC, a recorded presentation and comment form will be available for your review and feedback on the project website ([Vaughan.ca/BassProMillsEA](http://Vaughan.ca/BassProMillsEA)) from **August 19, 2021 to September 16, 2021**. You are encouraged to review the information when it becomes available.

If you have further questions, please let me know.

Regards,



**Hilda Esedebe**, P.Eng., MBA, M.Sc.  
**Transportation Project Manager**  
**Infrastructure Planning and Corporate Asset Management**  
905-832-8585, ext. 8484 | [hilda.esedebe@vaughan.ca](mailto:hilda.esedebe@vaughan.ca)

**City of Vaughan | Infrastructure Development**  
2141 Major Mackenzie Dr., Vaughan, ON L6A 1T1  
[vaughan.ca](http://vaughan.ca)



## Robinson, Jennifer

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**From:** Hilda Esedebe <Hilda.Esedebe@vaughan.ca>  
**Sent:** Monday, September 20, 2021 4:52 PM  
**To:** [REDACTED]  
**Cc:** Addley, Diana; Cholewa, Peter; Robinson, Jennifer; [REDACTED]  
**Subject:** RE: [External] Bass Pro Mills Drive Municipal Class Environmental Assessment (EA) Study - Phase 3 recommendations and comments

Hello [REDACTED],

Thank you for your email and support for the recommended plan for the Bass Pro Mills Drive extension (from Highway 400 to Weston Road). Your comments have been noted.

As previously advised, the proposed connection to Astona Blvd and the internal road network that you are referring to are outside the scope of the Bass Pro Mills Drive Municipal Class Environmental Assessment (EA) Study and falls under the Vaughan Mills Center Secondary Plan (VMCSP) and the City's Policy Planning department, whom I believe you have been in contact with.

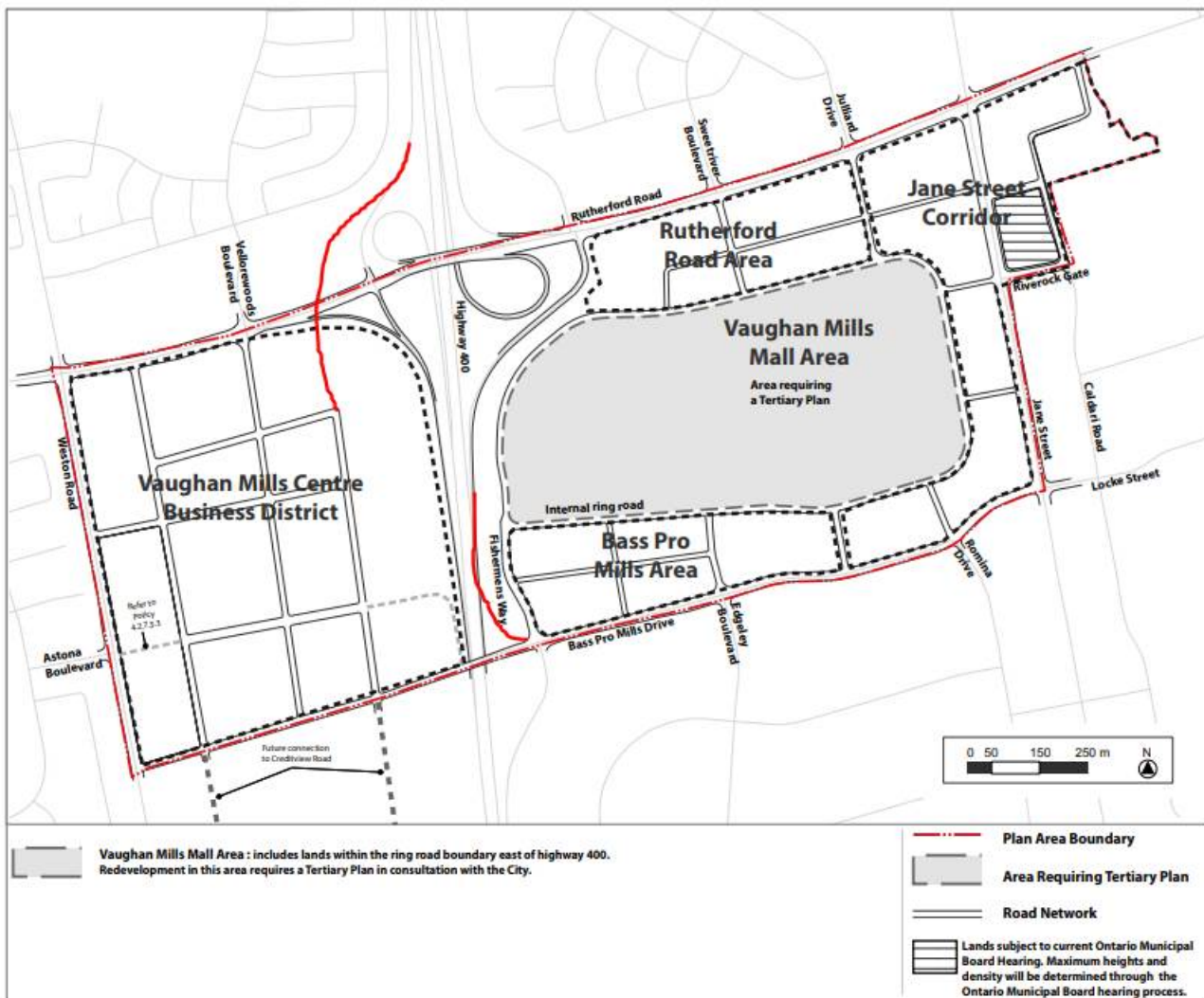
To reiterate, York Region has plans to widen Rutherford Road and Weston Road by 2041 (see the [Transportation Master Plan | York Region](#)). York Region has already begun the Detailed Design project to widen Weston Road from Highway 407 to Fieldstone Drive/Chrislea Road (Phase 1) and between the future Bass Pro Mills Extension and Hawkview Boulevard (Phase 2), with construction anticipated within the next few years. The Region is also about to conclude the [Langstaff Road Widening Environmental Assessment Study](#) which includes widening Langstaff Road from four to six lanes, the missing link, and an initial review of a full moves interchange at Langstaff Road and Highway 400. Although these projects fall under York Region's jurisdiction, the City works with all tiers of government to preserve the interest of citizens as best as possible, while reviewing multiple solutions to address growth and transportation needs. For more information regarding Regional projects, you may email [roads@york.ca](mailto:roads@york.ca) or visit the project websites. I believe York Region staff for the current Weston Road Widening project have already met with the Weston Downs Rate Payers Association (WDRA). Please note that the intersection at Bass Pro Mills Drive and Weston Road is proposed to be three-legged.



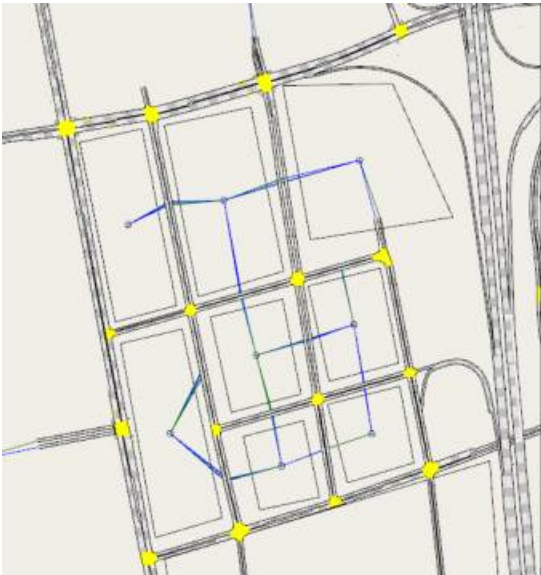
Direct Highway 400 connections to Wonderland and Vaughan Mills Center are outside the scope of the Bass Pro Mills Drive EA Study. Ramp connections to Highway 400 fall mainly under the Ministry of Transportation Ontario's (MTO's)

jurisdiction and would require compliance with MTO’s design standards including spacing, proximity to other interchanges (Major Mackenzie Drive, Rutherford Road and Langstaff Road), adequate lengths to accommodate speed changes and change in grades, property allowances, crossing bridge structures etc. This highway corridor currently accommodates many movements and creating adequate spacing for more movements while meeting MTO’s standards may be a challenge. For example, with my experience in Highway Design, and from looking into similar comments from the public for the Bass Pro Mills EA Study, the connections you have suggested under item 7 (both illustrated in red below) are likely not feasible; 1-because there isn’t much room to accommodate the elevation changes from Highway 400 southbound, then go above grade over Rutherford Road to come back down to connect to Bass Pro Mills Drive via the NW VMCSP lands. With the proposed VMCSP road network, vehicles from Highway 400 southbound can exit at Rutherford Road and turn left on Valeria Woods Blvd to get to Bass Pro Mills Drive. The proposed VMCSP road network will offer alternatives for traffic to get from Rutherford Road to the VMCSP area and Highway 400 without infiltrating adjacent neighbourhoods. 2- There isn’t much room to accommodate a direct Highway 400 northbound on ramp from the south-east side of Bass Pro Mills Drive while satisfying standards as noted above.

For more information, you may email MTO Corridor Management at [corridor-cr@ontario.ca](mailto:corridor-cr@ontario.ca) or visit the MTO website.



The project team is currently looking into a request by the WDRA for the Traffic Analysis information used for the Bass Pro Mills EA study. The road network shown in the approved [VMCSP 2014 document](#) was used for the traffic analysis. This excludes an Astona Blvd. east link.



Please be advised that Traffic Analysis Reports are typically included as part of the Environmental Study Report (ESR) for public review prior to the completion of the study. You are part of the study contact list so you will receive a notification at that time.

Regards.

**Hilda Esedebe**, P.Eng., MBA, M.Sc.  
**Transportation Project Manager**  
**Infrastructure Planning and Corporate Asset Management**  
905-832-8585, ext. 8484 | [hilda.esedebe@vaughan.ca](mailto:hilda.esedebe@vaughan.ca)

**City of Vaughan | Infrastructure Development**  
2141 Major Mackenzie Dr., Vaughan, ON L6A 1T1  
[vaughan.ca](http://vaughan.ca)



---

**From:** [REDACTED] >  
**Sent:** Friday, September 17, 2021 12:30 AM  
**To:** Hilda Esedebe <Hilda.Esedebe@vaughan.ca>; Diana.Addley@stantec.com  
**Cc:** [REDACTED]

**Subject:** [External] Bass Pro Mills Drive Municipal Class Environmental Assessment (EA) Study - Phase 3 recommendations and comments

## Bass Pro Mills Drive MCEA Study

**My recommendations and comments** for the "Bass Pro Mills Drive Municipal Class Environmental Assessment (EA) Study - Phase 3 are as follows:

1. **I do support** the extension of Bass Pro Mills directly west to Weston Road to better access the VMC complex.
2. **I do not support** the extension of Astona Blvd directly east into the VMC north west section. This road extension is not justified and will only create traffic infiltration problems into the Weston Downs community.
3. **I do not support** the multi modal connections from Bass Pro Mills Drive and adjacent proposed roads into the existing southbound ramp to the 400 Highway. This will not improve traffic on Weston Road or on Rutherford Road. See my recommendations below.
4. **I do not support** redistributing traffic from Rutherford Rd into the existing well established Weston Downs neighborhood. Installing a direct connection into the existing Bass Pro Mills 400 south bound onramp will create cut through traffic infiltration into the Weston Downs neighborhood. Please learn and apply the lessons at Pharmacy Ave. and Sheppard Ave. East I have already sent to you.
5. The existing Bass Pro Drive connection to the south bound 400 highway on ramp was installed to allow shoppers and visitors to enter the 400 Southbound highway directly from Vaughan Mills without using Rutherford Road or Weston Rd. This on ramp should be maintained for this sole purpose.
6. Any road connection proposal, directly opposite from Astona Blvd. and any new connections to the 400 South bound on ramp at Bass Pro Mills Drive is of critical importance to the residents of Weston Downs because if they go ahead and either is constructed, it will significantly increase the cut through traffic into the neighborhood of Weston Downs. This cut through traffic will significantly reduce the internal road safety for residents and for their children walking, biking and driving to and from their homes in Weston Downs. These road connection would create a major safety hazard to the residents of Weston Downs.

**A priority needs to put on immediately by Vaughan City and York Region on improving the following existing road infrastructure to properly relieve traffic around the Rutherford Road and Weston Road area without diverting it into the Weston Downs neighborhood.**

1. Increase Langstaff Road from Islington Ave. to Jane St from 4 lanes to 6 full lanes. Install full North bound on ramps to 400 highway north from for both east and west bound traffic on Langstaff Rd.
2. Install direct northbound 400 highway onramp directly from Wonderland at the southwest corner of Wonderland. Do not utilize Rutherford Rd for Wonderland visitors wanting to access 400 north bound when leaving.
3. Install direct southbound 400 highway onramp directly from Wonderland at northwest corner of Wonderland property. Do not utilize Major Mackenzie Rd for Wonderland visitors wanting to access 400 south bound when leaving.
4. Widen Rutherford Rd from 4 lanes to full 6 lanes from Islington to Jane Street.
5. Expand Weston Road and install 2 left turn lanes on Weston Rd southbound at Rutherford Rd. This will allow for rapid access to existing 400 South and North bound onramps on Rutherford

Rd. There is absolutely no need to route this traffic to a new onramp connection at Bass pro Mills to the 400 south.

6. Install 2 right turn lanes on Weston Rd northbound at Rutherford Rd. This will allow for rapid access to existing 400 South and North bound onramps on Rutherford Rd. Again there is absolutely no need to route this traffic to a new onramp connection at Bass pro Mills to the 400 south.
7. North bound 400 offramps to Vaughan Mills are well designed as is the exit from Vaughan Mills to south bound 400 via Bass Pro Mills Road. However there is no adequate direct access to 400 highway north bound for exiting from Vaughan Mills and there is no direct access to Vaughan Mills from the southbound 400. The following road infrastructure needs to be improved;
  1. The south bound off ramp onto Rutherford Rd. needs to be extended southbound into the NW VMC lands and connected to Bass Pro Mills for direct access to Vaughan Mills Centre.
  2. A direct 400 north bound on ramp from Vaughan Mills Center needs to be installed from the south east side of Bass Pro Mills for Vaughan Mills Center existing visitors wanting to access the north bound 400 highway directly

**Questions I have:**

1. Have the options I outlined above been evaluated? Why are these options not the preferred options? Why have these options not already been installed to relieve traffic in the Weston Road and Rutherford Road areas?
2. Why is widening of Weston Road, north of Bass Pro Mills Drive to Hawkview Boulevard being planned for? Is it to allow 2 lanes of left turn traffic from North of Weston road to go to the new Bass Pro Mills Road at Weston Road?
3. What is the basis for the study carried out for Vehicular trips associated with the 2014 Vaughan Mills Centre Secondary Plan (VMSCP) road network, this Bass Pro Mills Drive Municipal Class Environmental Assessment and Option B (Future Conditions with Bass Pro Mills Drive extension and VMCP (2031 and 2041 horizons)? Can I get a copy of this full report? Does this study include an Astona Blvd east link into VMC and a Bass Pro Mills Road with a multi-modal road connections to the south bound on ramp to the 400 highway?

If you have any questions regarding my input above, please contact me by email or by phone [REDACTED]

Thank you and Regards, [REDACTED]